

PLANNING STATEMENT

PROPOSED HV SUB-STATION

For

CUMMINS

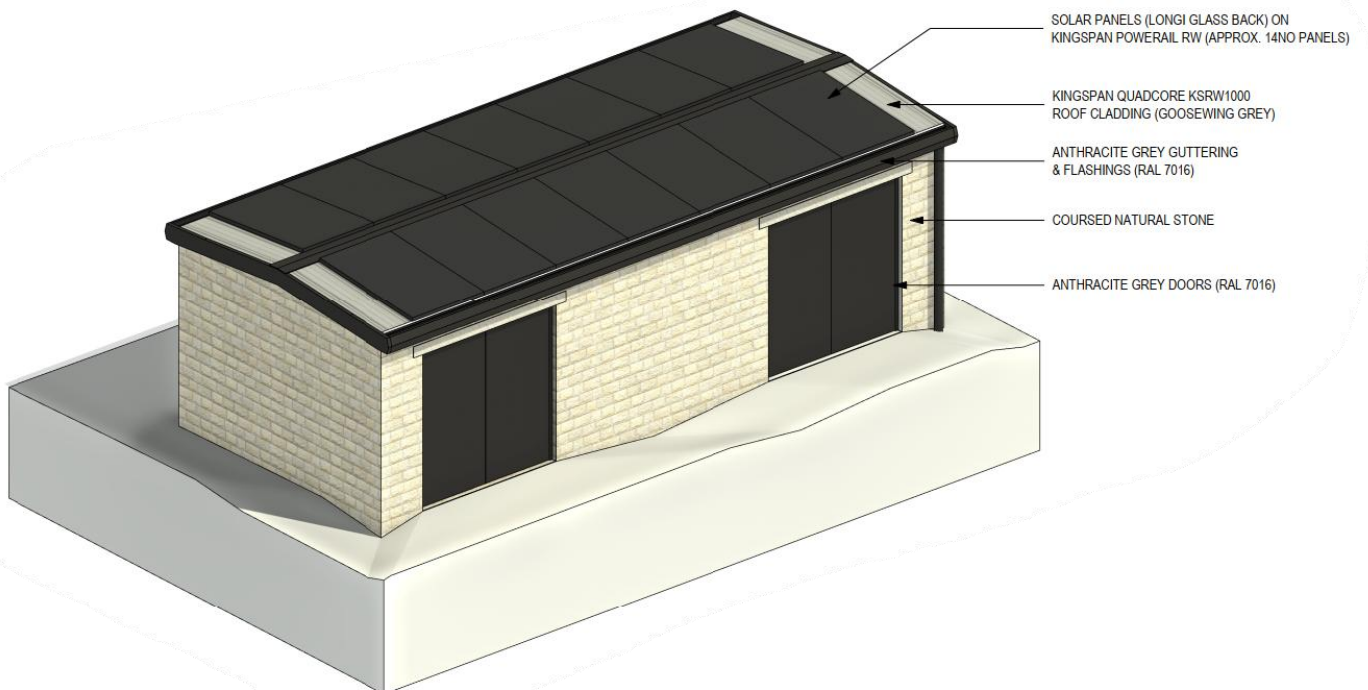
At

ST. ANDREW'S ROAD, HUDDERSFIELD, HD1 6RA

1.0 Introduction

- 1.1 This supporting planning statement aims to provide context, justification, and an overview of the proposed new HV sub-station at Cummins Turbo Technologies, Huddersfield.
- 1.2 This application is for a single storey high voltage sub-station. The sub-station will be located within an existing staff car parking area as indicated on the supporting drawings.
- 1.3 This planning statement should be read in conjunction with all supporting drawings and documentation.

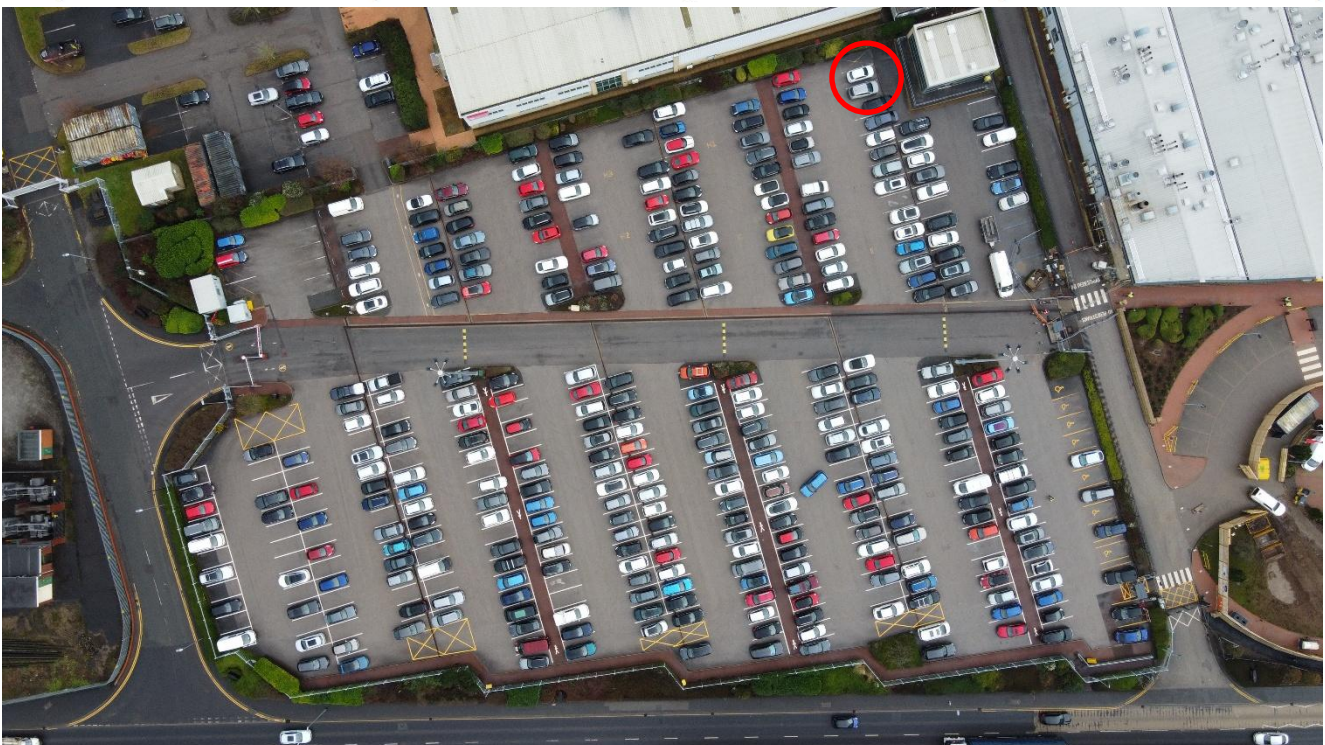
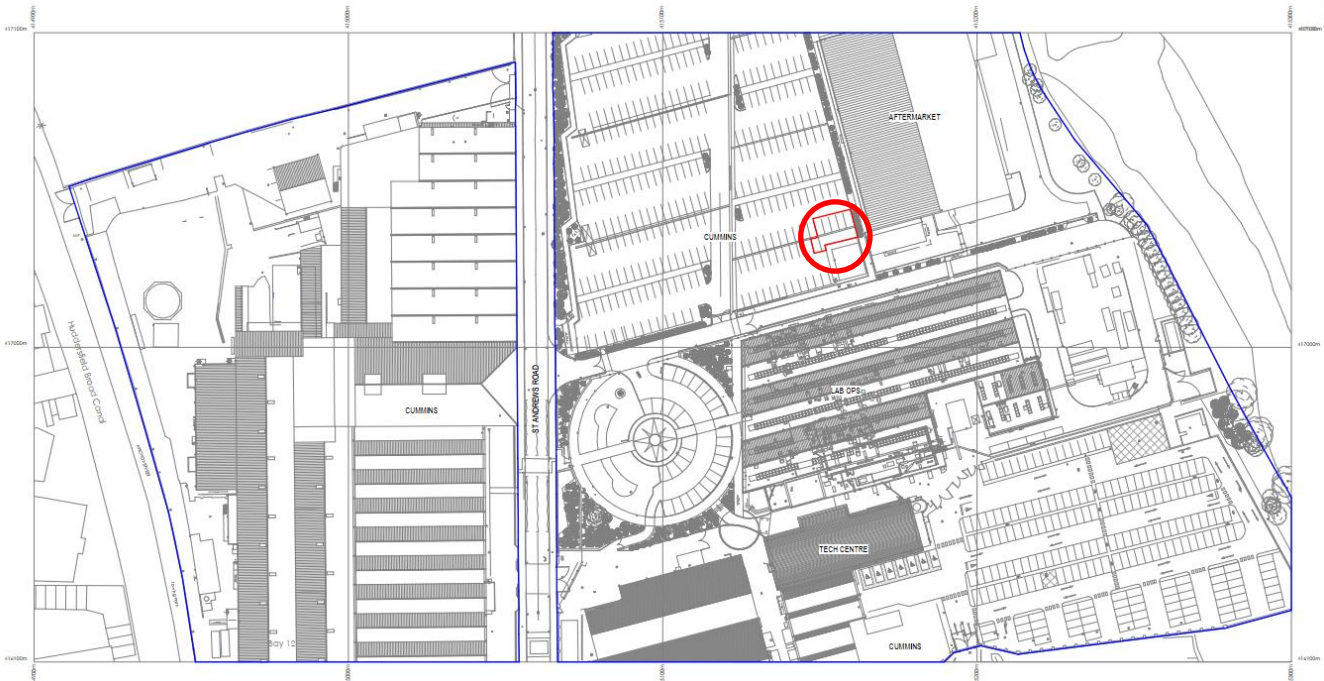
The below 3D image shows the proposed single storey sub-station.



2.0 Location

- 2.1 The full site address is:
Cummins, St Andrew's Road
Huddersfield, HD1 6RA

The below image is the location plan and aerial photograph of the site and the proposed new sub-station is within the area circled red.



3.0 Context

- 3.1 The proposed sub-station is related and associated with the recent solar panel additions to the buildings on the Cummins Huddersfield site and will provide greater infrastructure for future additions such as solar car ports to car parking areas.
- 3.2 Cummins in Huddersfield have a 'Planet 2050' renewable site goal, and their recent solar proposals will contribute significantly to this goal. Approximately 3,000 solar panels were installed which equates to 200,000 tonnes of Co2 eliminated from the atmosphere.

All elements of site wide solar panels have been covered by previous planning applications and prior approval notices.

The below image shows the extent of solar works on site to date



4.0 Proposal

- 4.1 The proposed single storey sub-station will be constructed in materials to match other adjacent buildings within the Cummins site boundary.

Natural coursed stone walls, dark grey steel doors and a grey metal clad roof.

There is approx. 14no. solar panels to be installed on the roof. The solar panels are to be LONGi pv modules to ensure FM and RoofNav approval when installed with Kingspan Quadcore KS1000RW and PowerRail RW.

- 4.2 The new sub-station will replace existing hardstanding areas therefore there will be no collection of additional surface water. The roof surface water will discharge into the existing surface water drainage system on site.

- 4.3 The proposed new sub-station will remove 4no. standard parking spaces.

Cummins have sufficient parking spaces in their 3no. car parking areas across their whole site and the removal of 4no. bays will not cause any effect on their operations.

As noted above, the proposed sub-station will be replacing existing hardstanding materials (tarmacadam) and there will be no loss of habitats.

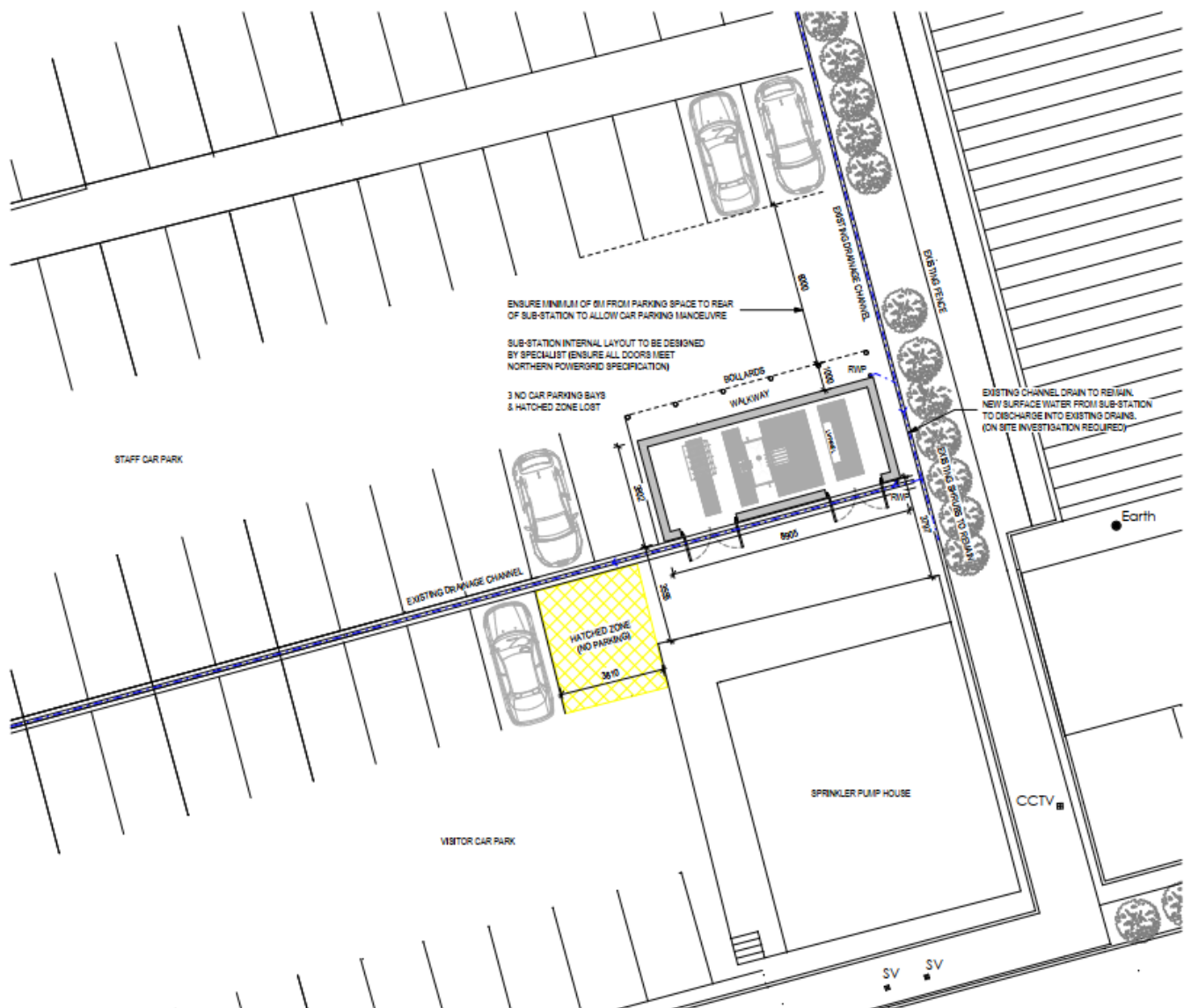
Bio-diversity measures are exempt for these proposals.

5.0 History

5.1 Cummins are continually developing their Huddersfield site to improve the performance and safety of their buildings.

A recent example of their improvement works is the roof replacement of the Laboratory Operations Building. These works involved the removal of the existing roof, removal of redundant mechanical and electrical equipment, the elimination of roof penetrations, and the installation of a full new roof system to achieve a U-value beyond the minimum requirement of the Building Regulations.

The below image shows the proposed sub-station in context to the existing car parking area, adjacent sprinkler pump house and aftermarket building.

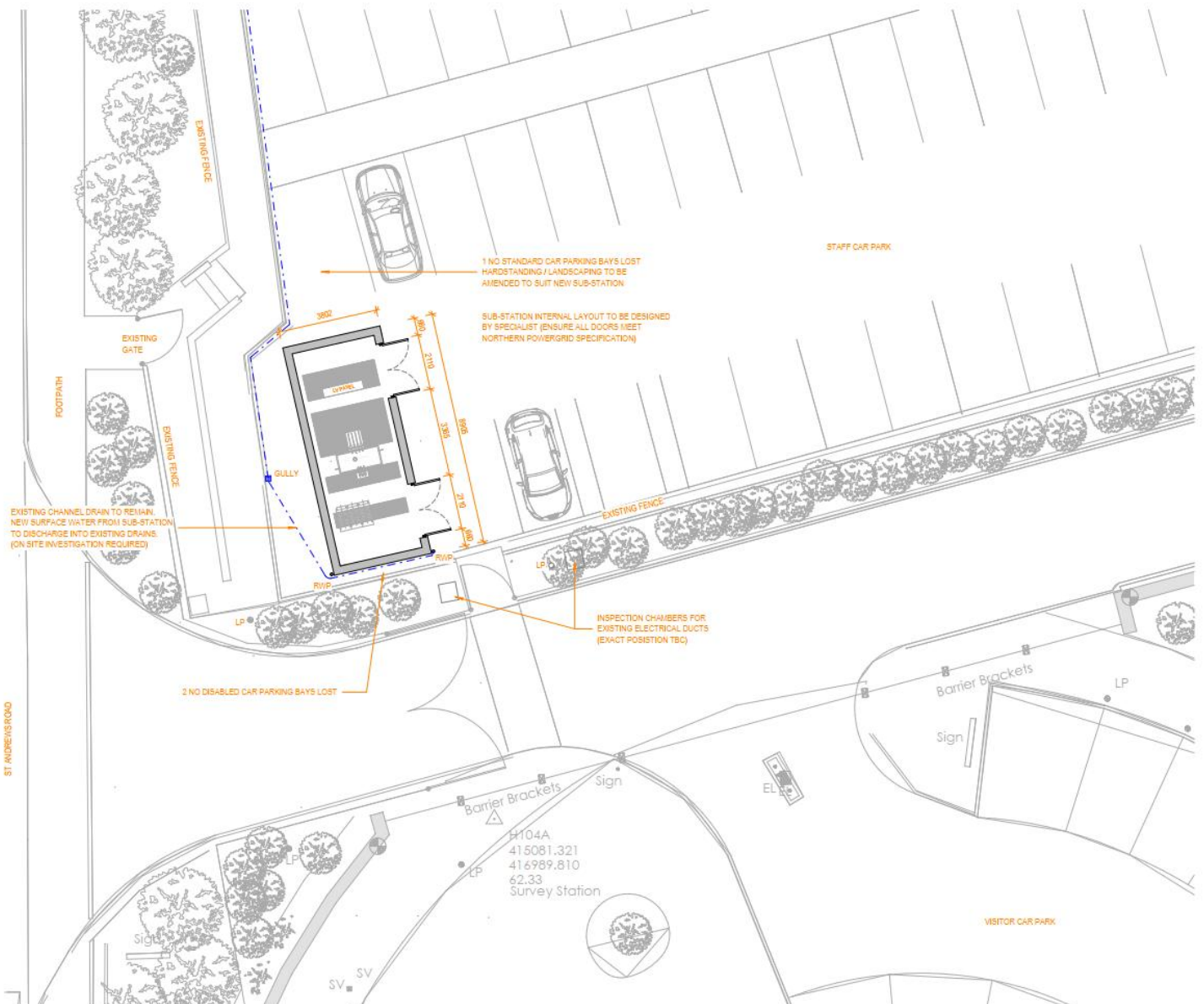


5.0 History

5.2 Cummins recently received planning approval for a similar sub-station proposal, application reference: 2024/92900

The sub-station that was approved is similar in appearance and size to the new proposal, but in a different location.

The below image shows the previously approved sub-station.



6.0 Flooding

7.1 The overall site is located in both Flood Zone 1 and Flood Zone 2, which means there is a medium probability of flooding from rivers and the sea.

The site is already served by extensive flood protection measures.

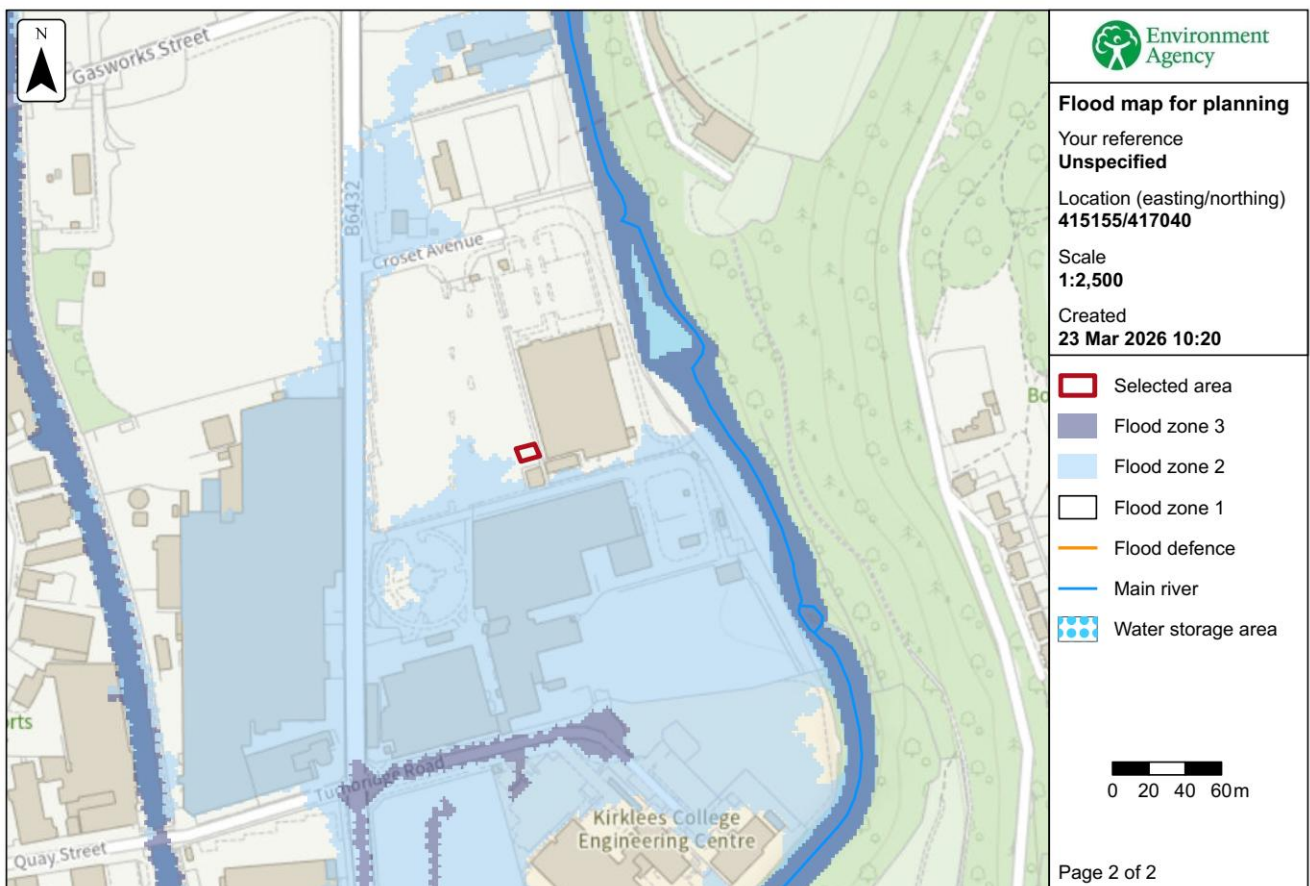
The proposed sub-station will be sited at a level similar to other existing buildings and sub-stations on the applicant's site.

Trenches for cables and ducts within substation will be adequately drained in accordance with Northern Powergrid specifications.

Refer to pages below for EA minor works Flood Risk Self-Assessment.

The below image is the flood map for the area of works at Cummins, Huddersfield. The works are located within Flood Zone 1 as shown with red boundary line.

(Map extracted from flood-map-for-planning.service.gov.uk)





Cumulative impact of minor extensions and the removal of Permitted Development rights.

PPS25 paragraph D16 highlights the potential for the cumulative impact of minor extensions to have a significant effect on flood risk. Where local knowledge (Strategic Flood Risk Assessment held by the LPA/Information provided by the parish council) suggests this is the case the guidance contained in FRA guidance note 2 should be applied². FRA guidance note 2 can also be applied where permitted development rights have been removed for flood risk reasons. The Environment Agency does not usually comment on minor development in this category.

Permeable paving and changes to permitted development rights for householders

On the 1st October 2008 the General Permitted Development Order (GPDO) in England was amended by the Government (Statutory Instrument 2008 No. 2362).

One of the changes introduced by the GPDO amendment is the removal of permitted development rights for householders wishing to install hard surfacing in front gardens which exceeds 5 square metres (i.e. 1m x 5 m) without making provision to ensure permeability. This means that use of traditional materials, such as impermeable concrete, where there is no facility in place to ensure permeability, requires an application for planning permission.

In order to help and advise householders of the options for achieving permeability and meeting the condition for permitted development status the Department for Communities and Local Government (CLG) has produced guidance on permeable paving which can be found on the following link <http://www.communities.gov.uk/publications/planningandbuilding/pavingfrontgardens>.

The Environment Agency support this change to the GPDO as it is in line with the recommendations of the Pitt Report regarding the need to better tackle the impact of surface water flooding. However, Local Planning Authorities should determine these applications in accordance with the CLG guidance **without** consulting the Environment Agency.

End of comment