



# Bromley Farm Quarry, Upper Cumberworth

Access Management Plan

May 2026

Project number: 2074

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## Quality Management

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Remarks	Final	A	B	
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Appendix A 2074 AMP 01A (Site Plan)

Appendix B 2074 AMP 02A (Designated Delivery Route & Signage)

## **1.0 Proposed Development and Site Access**

1.1 The proposals relate to an extension of Bromley Farm Quarry, Upper Cumberworth. The current site is exhausted of mineral reserves; however, additional land adjacent to the existing site is proposed to be quarried. As part of the development, improvements will be made to the existing access road, along with the creation of a new site access.

1.2 This Access Management Plan has been prepared in response to Condition 22 of the draft planning conditions received from the Council. The report outlines how vehicles will access the site, including parking and loading arrangements, and explains how access will be maintained to visitors, and emergency services.

1.3 Access to the site will be taken from the existing arrangement off the A635 Barnsley Road. HGVs and construction traffic can approach from either the northwest or southeast via the A635. After turning off the A635 onto the unnamed access road, vehicles will travel north towards the proposed site access. The access road will be widened in places as part of the works. HGV's will be directed by temporary signage and will enter via new access proposals on the west side of the unnamed access road.

1.4 Upon exiting the site, vehicles will turn right onto the unnamed access road and proceed south to the A635 Barnsley Road junction. From there, they will either:

- Turn northwest towards the A629 for access to Huddersfield and Sheffield, and the wider strategic road network; or
- Turn southeast along the A635 towards the M1 motorway and Barnsley town centre.

All vehicle movements will follow the designated delivery route shown in Appendix B (Drawing 2074 AMP 02 – Designated Delivery Route & Signage).

1.5 The access design accommodates delivery vehicles up to 10.0m rigid trucks. Larger vehicles may be allowed on an ad-hoc basis, subject to prior approval from site management. Swept path analysis is provided in Appendix A.

- 1.6 All personnel, subcontractors, suppliers and HGV drivers will be briefed on the approved traffic route and delivery management procedures prior to attending site. The approved route and operational requirements will be issued as part of the driver booking and induction process and must be adhered to at all times. No alternative access route will be permitted unless agreed in advance with site management.
- 1.7 The internal layout will provide for designated operative and visitor parking, materials storage, a site office, and a wheel bath. The compound will be securely fenced and monitored to ensure safety and security. Details are shown in Appendix A (Drawing 2074 AMP 01A – Site Plan).
- 1.8 Uninterrupted access for emergency services will be maintained at all times during quarry operations and any construction activity. The site entrance will remain unobstructed, and site operatives will ensure that gates can be opened immediately for emergency vehicles. A clear route from the public highway to the main working areas will be identified and kept free of parked vehicles, plant, or materials. All site staff will be briefed on the emergency access protocol and will be trained to stop all vehicle movements and guide emergency services directly to the incident location. Contact details for the site manager and deputy will be available in the site office to enable rapid coordination with the fire, ambulance, or police services.

### **Parking & Trip Generation**

- 1.9 The site will have approximately 5 personnel on site. Most are expected to arrive in individual vehicles or shared vans. The site will provide parking for:

- 4 vehicles for site operatives.

This allows for some degree of car sharing or van pooling, but ensures there is sufficient capacity if most staff members arrive individually.

- 1.10 All contractor parking will be accommodated within the site boundary, and no parking will occur on surrounding public roads. Parking arrangements will be managed on-site by the site supervisor to ensure efficient use of space and prevent congestion or obstruction.

1.11 In addition to staff, it has been advised that 4 vehicles are expected to visit the site during peak times. These typically include:

- Material deliveries (HGVs and rigid lorries)
- Subcontractor vans
- Plant and equipment deliveries

These visits will be managed and staggered where possible to avoid conflict with staff arrival/departure times and to maintain clear access routes within and around the site.

1.12 A delivery management system will be implemented and managed by the Site Manager, or in their absence the nominated Deputy Site Manager. All HGV deliveries and collections will be pre-booked in advance through the site office to ensure vehicle arrivals are controlled and staggered throughout the working day. Delivery times will be coordinated to avoid multiple HGVs arriving simultaneously and to ensure sufficient space is available within the site for vehicles to enter, manoeuvre, load/unload and exit in a forward gear.

1.13 A banksman or trained traffic marshal will be present during periods of significant HGV activity and will coordinate vehicle movements into and out of the site. Drivers arriving early will be instructed not to wait on the unnamed access road or surrounding highway network and instead will be directed to return at their allocated booking time. All suppliers, subcontractors and visiting drivers will receive written delivery instructions advising them of the approved access route, booking requirements, site speed limits and the strict no-waiting policy on the shared access road and public highway.

1.14 The site entrance gates will remain set back sufficiently to allow vehicles to pull clear of the shared access road whilst access arrangements are confirmed and gates opened. Should an unforeseen issue arise resulting in a temporary delay to access, site staff will prioritise clearing the access immediately to ensure the shared access road remains unobstructed at all times.

- 1.15 To accommodate contractors and visitors, 4 contractor parking bays have been provided as well as 1 HGV bay should they be required. In busy phases there is scope to increase parking on site should it be needed. Full parking details can be seen in Appendix A (Drawing 2074 AMP 01A Site Plan).

## 2.0 Traffic Management and Control

- 2.1 Temporary traffic signage will be erected in the form of "WORKS TRAFFIC" directional signs, "CAUTION WORKS TRAFFIC" and "SITE TRAFFIC ONLY" warning signs to alert other road users. The locations and diagram numbers for the proposed signage can be seen at Appendix B.



- 2.2 The signage scheme and associated traffic controls will be in place before the start of works and maintained throughout the construction period. All signs will be removed promptly at the end of the works.
- 2.3 All traffic marshals and operatives involved in traffic control will be appropriately trained and experienced. Safety procedures and guidance will be strictly followed throughout the construction phase.
- 2.4 All traffic marshals and operatives involved in traffic control will be existing members of the site team who have completed accredited traffic-marshall training. Their primary role is to manage the safe movement of construction vehicles and pedestrians at all site access points and within the site boundary. Traffic marshals will be present whenever there are significant vehicle movements taking place.
- 2.5 They will wear appropriate PPE at all times, including high-visibility clothing, safety footwear, and hard hats, and will use agreed hand signals and two-

way radios to communicate with drivers and site personnel. Their responsibilities include monitoring and controlling vehicle entry and exit, ensuring gates remain closed when not in use, keeping public highways clear, and coordinating with the site manager to schedule deliveries and prevent vehicle queuing on the public highway. Traffic marshals will also have responsibility for enforcing the site no waiting policy and reporting any noncompliance to the Site Manager.

- 2.6 To minimise disruption, HGV movements will be limited to between 07:00 and 19:00 hours Monday to Friday, between 08:00 and 13:00 Saturdays, and there will be no Sunday working. Deliveries will be scheduled to avoid peak traffic times when possible. Vehicle arrivals outside these hours will not be permitted unless previously agreed. A banksman will supervise all HGVs navigating within the site.
- 2.7 Construction and operatives' vehicles will not be permitted to wait or park on the unnamed access road or surrounding highway. All vehicles must use the designated on-site areas.
- 2.8 All suppliers and contractors will be informed of approved delivery times and routes prior to attending site. Deliveries will be coordinated by the Site Manager or nominated deputy to ensure safe unloading, efficient site operations and to prevent vehicles arriving before their allocated booking slot. Site staff will maintain communication with drivers where required to minimise the risk of vehicle queuing or obstruction on the shared access road or surrounding highway network.
- 2.9 To prevent mud or debris from being tracked onto the highway, wheel bath will be installed within the site boundary. If debris is deposited on the access road or A635, a road sweeper will be deployed as required. The wheel bath location is shown at Appendix A.
- 2.10 Responsibility for monitoring mud and debris on vehicle wheels and the surrounding highway network will rest with the Site Manager and appointed traffic marshals. All outbound HGVs will pass through the wheel bath facility prior to leaving the site and will be visually inspected by site operatives before entering the shared access road. Should any mud, stone, debris or material be identified on the access road or adopted highway, the Site Manager will arrange for immediate cleansing measures, including

deployment of a road sweeper where necessary. Regular inspections of the access road and site exit will be undertaken throughout the operational day, particularly during wet weather conditions.

- 2.11 Dust suppression methods including vehicle wheel damping and use of water sprays will be implemented to minimise emissions during dry weather or intensive construction phases. Additional measures include using dust sheets to contain airborne particles within specific work areas and skip covers to prevent dust dispersion in windy conditions.
  
- 2.12 Responsibility for ensuring all loads are safely secured prior to vehicles leaving the site will rest with the loading operatives and Site Manager. All HGVs transporting loose materials will be required to use appropriate sheeted covers, enclosed skips or other suitable containment methods before departing the site. Site staff will undertake visual checks prior to vehicles exiting to ensure there is no risk of material escaping onto the shared access road or adopted highway network. Any vehicle identified as inadequately loaded or uncovered will not be permitted to leave the site until corrective measures have been implemented.

### **3.0 Driver Communication and Route Management**

3.1 Responsibility for communicating approved delivery routes, delivery times and site access requirements to all HGV drivers will rest with the Site Manager, or in their absence the nominated Deputy Site Manager. No HGV driver will be permitted to attend site unless they have first received the approved routing and delivery information.

3.2 All haulage contractors, suppliers and subcontractors will receive written driver instructions at the point of booking and prior to attending the site. This information will be issued electronically via email or mobile communication and will include:

- The approved HGV delivery route
- Approved hours of operation
- Site access arrangements
- The requirement to avoid waiting on the shared access road or public highway
- Site speed limits
- Wheel washing and load security requirements; and
- Contact details for the Site Manager or traffic marshal.

3.3 Drivers will be informed that all HGV movements associated with the quarry must:

- Arrive only during approved operational hours
- Follow the approved delivery route shown within Appendix B
- Enter and exit the site in accordance with banksman instruction
- Avoid reversing movements on the shared access road

- Not park or wait on the shared access road or surrounding highway network; and
- Ensure all loads are safely sheeted or contained before leaving the site.

3.4 Delivery bookings will be managed through the site office. Drivers arriving significantly earlier than their allocated booking slot will be instructed not to wait on the shared access road and will instead be required to return at their designated time slot. Traffic marshals and site operatives will monitor compliance with these requirements throughout operational hours.

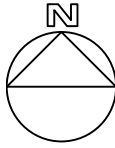
3.5 Clear signage will also be erected at the site entrance and along the approved delivery route reminding drivers of the designated route and site traffic requirements. Temporary traffic management signage is shown on Drawing 2074 AMP 02A contained within Appendix B.

3.6 An example driver instruction notice is provided below:

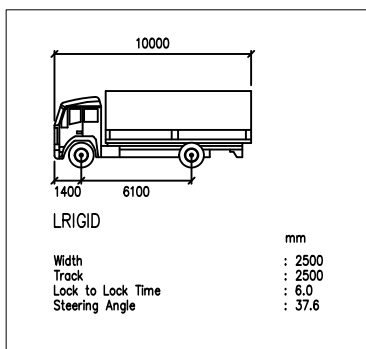
"All quarry related HGV traffic must approach and depart the site via the approved route shown within the site delivery plan. Vehicles must not wait or park on the shared access road or public highway. Drivers arriving early must contact the Site Manager and return at their allocated booking time if access is unavailable. All loads must be securely sheeted prior to departure and all vehicles must pass through the wheel wash facility before leaving site. Failure to comply with site traffic management requirements may result in refusal of entry to the site."

# Appendix A

## Construction Site Plan



- General Notes
- This drawing should not be scaled for setting out purposes.
  - This drawing shows the provisional design only and is subject to Local Authority approval.
  - This drawing is based upon a topographical / ordnance survey provided by others.



- Wheel bath location
- Large rigid truck tracking
- Compound for store of plant and materials used throughout the construction phase
- HGV parking area for visitors/contractors
- Parking area for visitors/contractors
- Parking area for site operatives
- Site cabin

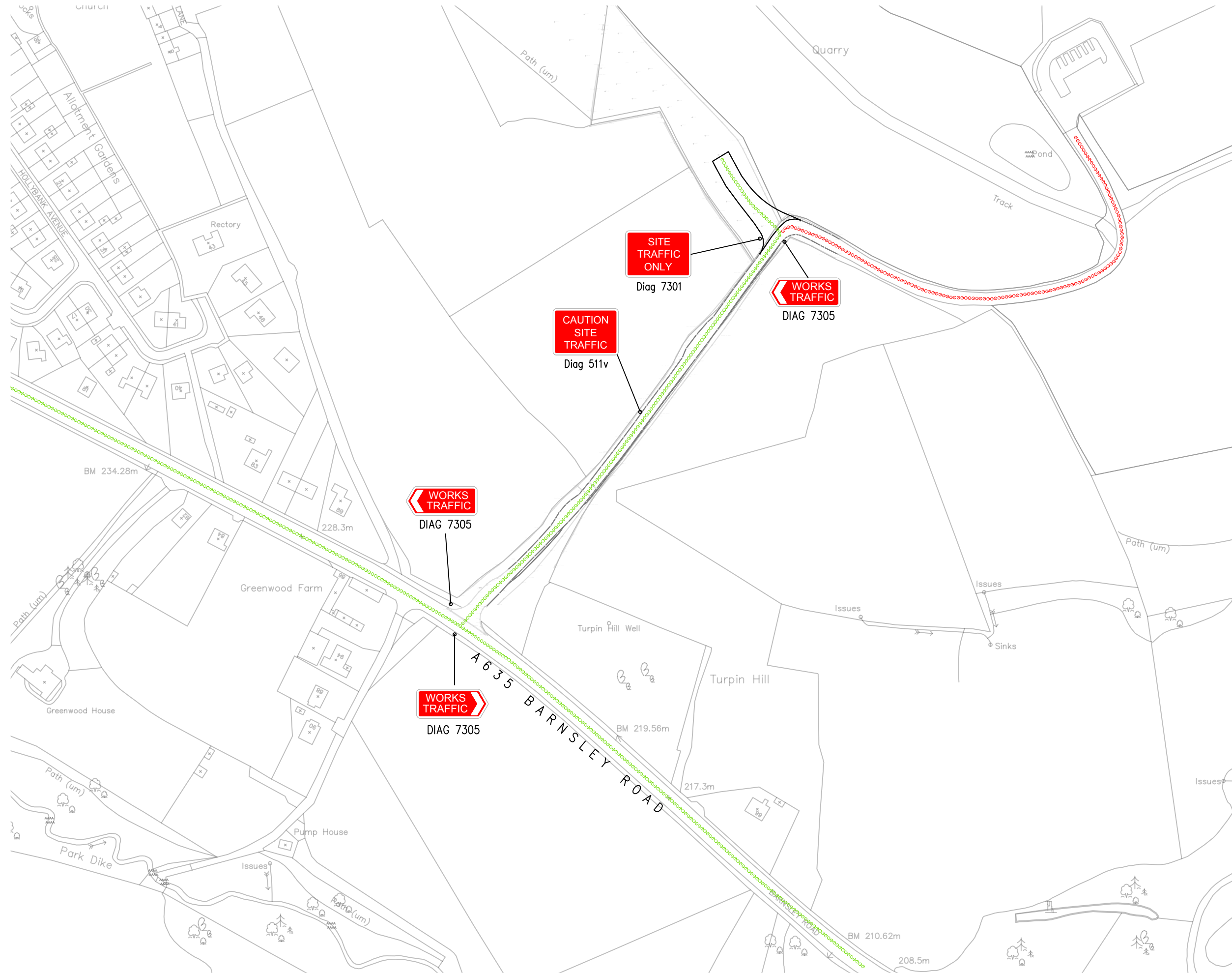
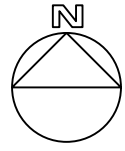


PROJECT TITLE						
BROMLEY FARM QUARRY, UPPER CUMBERWORTH						
DRAWING TITLE						
CONSTRUCTION SITE PLAN						
DRAWING NUMBER						
ORIGINATOR	PROJECT	VOL.	TYPE	ROLE	NUMBER	
PRGN	- 2074 -	HGN	- DR -	CH -	AMP	01A
CLIENT						
SILKSTONE ENVIRONMENTAL LTD						
SCALE						
1:500	SIZE	A3	DRAWN	CHECKED	AUTHORISED	DATE
			JJH	LJO	LJO	SEPT 25

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# Appendix B

## Designated Delivery Route & Signage



- General Notes**
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ROUTING KEY	
	ROUTE TO BE USED BY HOV'S & DELIVERIES
	ROUTE NOT TO BE USED BY HOV'S & DELIVERIES



<b>PROJECT TITLE</b>						
BROMLEY FARM QUARRY, UPPER CUMBERWORTH						
<b>DRAWING TITLE</b>						
DESIGNATED DELIVERY ROUTE						
<b>DRAWING NUMBER</b>						
ORIGINATOR	PROJECT	VOL.	TYPE	ROLE	NUMBER	
PRGN	2074	HGN	DR	CH	AMP	O2A
<b>CLIENT</b>						
SILKSTONE ENVIRONMENTAL LTD						
<b>SCALE</b>						
SCALE	SIZE	DRAWN	CHECKED	AUTHORISED	DATE	
1:500	A3	JJH	LJO	LJO	SEPT 25	

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