

**Transpennine Route Upgrade  
Hillhouse Construction Compound  
– s73 application**

**Amended Planning Statement**

**Document Ref: 151667-TSA-00-TRU-REP-W-EN-001974  
(TP/LNE/2025-016)**

**Network Rail**

**August 2025**



## Contents

<b>1. INTRODUCTION .....</b>	<b>4</b>
1.1 Background .....	4
1.2 Environmental Impact Assessment .....	4
<b>2. SCHEME DESCRIPTION .....</b>	<b>6</b>
2.1 The Site and Surrounding Area .....	6
<b>3. THE PROPOSED DEVELOPMENT .....</b>	<b>7</b>
3.1 Introduction .....	7
3.2 Phased development .....	7
3.3 Site Access .....	8
3.4 Parking Management .....	9
3.5 Site Layouts and Welfare Facilities .....	10
3.6 Railway sidings .....	11
3.7 Acoustic barrier .....	11
3.8 Retaining Wall .....	12
3.9 Operating hours .....	12
3.10 Vegetation Screening/Landscape proposals .....	12
<b>4. APPLICATION DRAWINGS .....</b>	<b>13</b>
<b>5. DESIGN CHANGES .....</b>	<b>14</b>
<b>6. PROPOSED CHANGES TO CONDITIONS ATTACHED TO PLANNING REF 2021/94337 16</b>	
6.2 Approved Conditions .....	16
6.3 Outstanding Conditions to be removed/varied .....	16
6.4 Outstanding Conditions to be attached to any new permission .....	18
<b>7. PLANNING CONTEXT .....</b>	<b>19</b>
7.1 Traffic .....	19
7.2 Landscape and Visual Impact .....	19
7.3 Water and Flood Risk .....	19
7.4 Noise and Vibration .....	19
7.5 Air Quality .....	20

7.6 Ecology .....	20
7.7 Cultural heritage.....	20
7.8 Land contamination.....	21
<b>8. CONCLUSIONS .....</b>	<b>22</b>
<b>APPENDIX A – SITE LAYOUTS AND WELFARE FACILITY DETAILS .....</b>	<b>24</b>
<b>APPENDIX A.1 PHASE 1 SITE LAYOUT.....</b>	<b>25</b>
<b>APPENDIX A.1 PHASE 1 WELFARE DETAILS .....</b>	<b>26</b>
<b>APPENDIX A.2 PHASE 2 SITE LAYOUT.....</b>	<b>28</b>
<b>APPENDIX A.2 PHASE 2 WELFARE DETAILS .....</b>	<b>30</b>
<b>APPENDIX B – ACOUSTIC BARRIER DETAILS .....</b>	<b>32</b>
<b>APPENDIX C – CONSTRUCTION TRAFFIC MANAGEMENT PLAN (CTMP) .....</b>	<b>33</b>
<b>APPENDIX D – DRAINAGE STRATEGY .....</b>	<b>34</b>
<b>APPENDIX E – CONSTRUCTION ENVIRONMENTAL MANAGEMENT PLAN .....</b>	<b>35</b>
<b>APPENDIX F – NOISE ASSESSMENT .....</b>	<b>36</b>
<b>APPENDIX G - CONTAMINATED LAND .....</b>	<b>37</b>
<b>APPENDIX G1 - PHASE II INVESTIGATION .....</b>	<b>38</b>
<b>APPENDIX G2 - REMEDIATION STRATEGY.....</b>	<b>39</b>
<b>APPENDIX H – ARCHAEOLOGY .....</b>	<b>40</b>
<b>APPENDIX H1 – ARCHAEOLOGICAL REPORT.....</b>	<b>40</b>

### Inserts

Insert 1-1 Site location and planning application boundary .....	5
Insert 3-1 Existing Site entrance to be retained during enabling works .....	8
Insert 3-2 Phase 2 welfare cabins.....	11

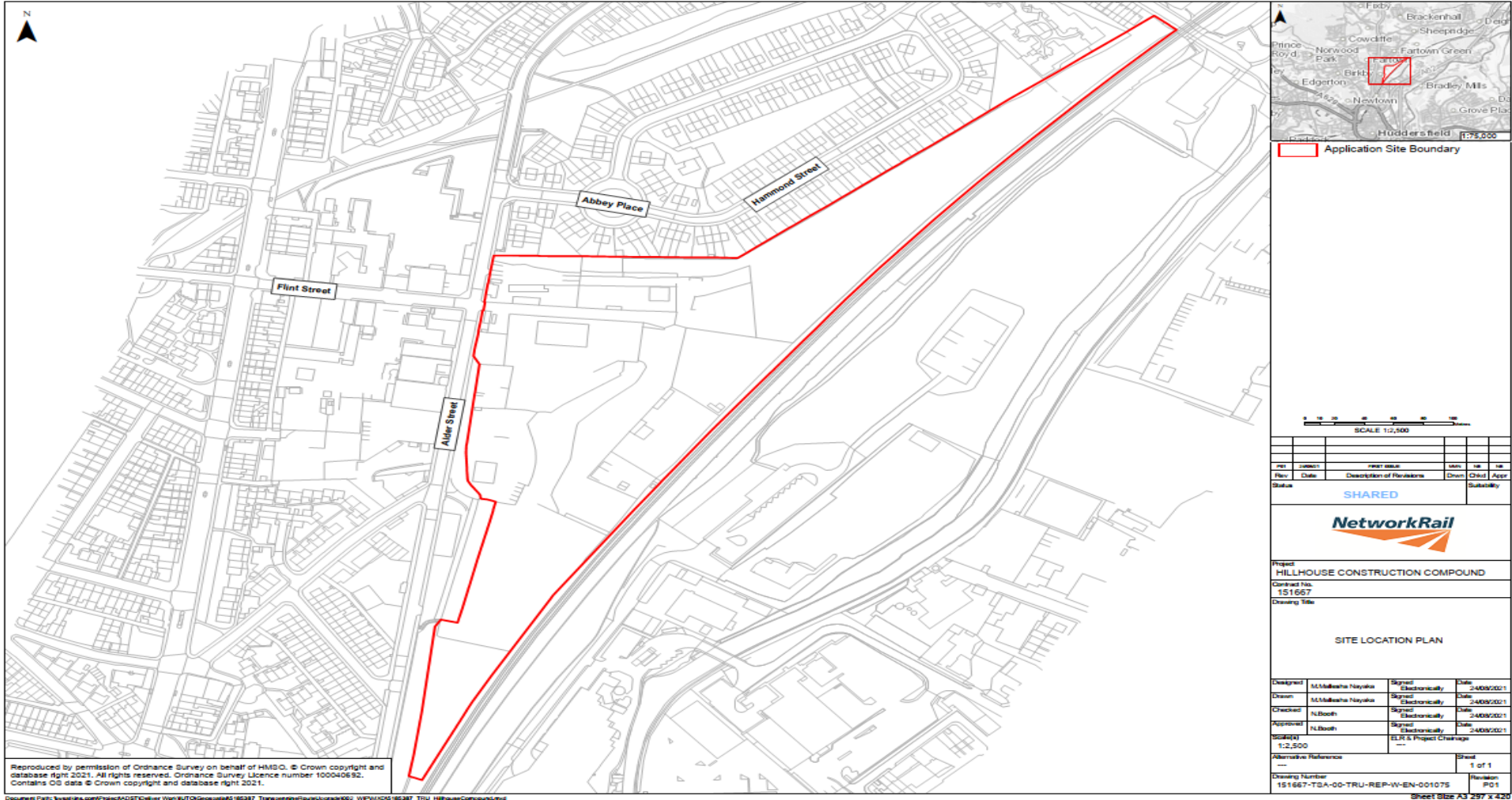
## 1. INTRODUCTION

### 1.1 Background

- 1.1.1 Planning Permission was granted for a temporary strategic construction facility with retaining wall, environmental mitigation measures and a temporary railway platform facilitate the construction works for the section of the Trans-Pennine Route Upgrade (TRU) between Huddersfield and Westtown (Dewsbury) ('the Scheme') at a site of Alder Street, Huddersfield namely the Hillhouse Yard.
- 1.1.2 This Planning, Design & Access Statement supports the submission under section 73 of the Town and Country Planning Act 1990 made on behalf of Network Rail and seeks to vary details of the development set out under paragraph 1.1.1 and consented under planning reference 2021/94337 dated 30 March 2022.
- 1.1.3 Drawing 151667-TSA-00-TRU-REP-W-EN-001075 (also see insert 1) shows the location of the land to which the planning application relates, edged in red (hereafter referred to as 'the Site'). The Site lies within the administrative area of Kirklees Council, and is not altered from the approved scheme.
- 1.1.4 It should be noted that certain elements of the proposed development fall under NR's Permitted Development privileges. These did not form part of the approved scheme and were supplied for information only; these include provision of the stabling sidings in the Yard for the train operating company, and the provision of a signing-on /mess facility to serve the sidings in question, along with associated internal access and car parking. It should also be noted that the existing GSM-R mast will remain in situ as part of this temporary works application. There is no change to these elements as part of this submission.

### 1.2 Environmental Impact Assessment

- 1.2.1 Schedule 2, 10(b) of the Town and Country Planning (Environmental Impact Assessment) Regulations 2017 Statutory Instrument 2017 No. 571 ('the Regulations') indicates the proposed Development is deemed to fall within Schedule 2; (10) Infrastructure Projects for which the following threshold currently applies:
- The overall area of the development exceeds 5 hectares.
- 1.2.2 As the site is approximately 5 hectares, consideration was given under the original submission as to whether the proposed works may give rise to significant environmental effects. It was concluded that the development did not fall as EIA Development under Town and Country Planning (Environmental Impact Assessment) Regulations 2017.
- 1.2.3 Given the changes proposed; it is not considered that this would materially affect the outcome of previous assessments and the Scheme remains a non-EIA Development.



Insert 1-1 Site location and planning application boundary

## **2. SCHEME DESCRIPTION**

### **2.1 The Site and Surrounding Area**

- 2.1.1 The Site is located at Hillhouse railway yard , Alder Street, Fartown is located wholly within the administrative authority of Kirklees Council.
- 2.1.2 The Site is approximately 5 hectares in area and is entirely within the ownership of NR. It is operational railway land within the meaning of Section 263 of the Town and Country Planning Act 1990 and is also designated by Network Rail (NR) as a Strategic Freight Site.
- 2.1.3 The Site is bounded by residential properties to the north, the operational railway to the south and Alder Street to the north-west. The north-western boundary of the Site is located within 20 metres of residential properties on Abbey Place and Hammond Street. Vehicle access is directly off Alder Street.
- 2.1.4 There is a Housing Allocation of 1.29 hectares, with an indicative capacity for 45 dwellings, adjacent to the western boundary of the Site. A Class 2 Archaeological Site is also adjacent to the western boundary, towards the south of the Site.
- 2.1.5 The Kirklees Wildlife Habitat Network runs north-south along the eastern boundary of the Site adjacent to the railway, and north-south adjacent to the western boundary at the southern end of the Site only. The Strategic Green Infrastructure Network runs east-west adjacent to the southern boundary of the Site.

### 3. THE PROPOSED DEVELOPMENT

#### 3.1 Introduction

3.1.1 The proposed Development is a construction facility to enable the construction works for the section of the TRU between Huddersfield and Westtown (Dewsbury). The proposed Development will comprise:

- Use of Hillhouse Railway Yard as a temporary strategic construction compound to serve the Trans-Pennine Route Upgrade works including:
  - Provision of strategic construction compound including open storage for civils and rail systems including ballast and track, Overhead Line Equipment (OLE) assembly and associated welfare facilities;
  - Construction of a retaining wall;
  - Construction of environmental mitigation measures (noise attenuation); and
  - Access; utilities and drainage works.

3.1.2 The proposed Scheme to date has generally been delivered in line with the approved planning permission (see Section 5 for further details). Site works commenced in January 2023.

#### 3.2 Phased development

3.2.1 The construction site is being used by the civils and rail systems engineers and includes laydown areas for the receipt and storage of the overhead line equipment (OLE) and trackwork. The laydown area for OLE is located in the northern part of the compound. A laydown area is also provided for permanent way works including an area where the trackwork will be fabricated. The Site is also used for storage of waste and excavated material. Active plant and machinery are present on site.

3.2.2 Construction and project management activities to service the wider Scheme are also undertaken from the Site. The Site has made provision for welfare cabins for construction staff (see Section 2.5).

3.2.3 Works associated with the Proposed Development will be undertaken in two stages, comprising a number of elements within each stage:

- Stage 1:
  - Use of the Site for construction of Advanced Works to the Order;
  - Construction of retaining wall (works now complete);
  - Construction of environmental mitigation measures (noise attenuation);
  - Construction of railway sidings (works now complete);
  - Provision of welfare for civils and rail systems staff (works now complete);
  - Material Storage (works now complete);
  - Limited staff parking; and
  - Associated utilities/drainage work (now complete).
- Stage 2:
  - Use of the Site as a strategic construction compound to facilitate the TRU works between Huddersfield and Westtown (Dewsbury);

- Material Storage; and
- Provision of welfare for civils and rail systems staff.

### 3.3 Site Access

- 3.3.1 Access to the Site is via the existing point on Alder Street. This existing junction between Alder Street, Flint Street and the Site is a four-arm priority-controlled junction with raised ‘table’ carriageway to control speeds.
- 3.3.2 The entrance width is 7m and the existing fencing / stonewall, along the front face of the Site (onto Alder Road).
- 3.3.3 No changes are proposed to the site entrance under the approved planning permission or as part of this submission. Where works are required to alter the access point from Alder Street this will be delivered under Article 17 of the Transport and Works Act Order (The Network Rail (Huddersfield to Westtown (Dewsbury) Improvements) Order 2022 and do not require further consent under the Town and Country Planning Act regime.
- 3.3.4 Flint Street connects to the A641 Bradford Road, a major north-south distributor road to the north of Huddersfield. Alder Street connects to both the A641 Bradford Road and A62 Leeds Road. The Site is therefore easily accessible from the major road network and near to an urban area.



**Insert 3-1 Existing Site entrance to be retained during enabling works**

- 3.3.5 Existing palisade fencing has been retained within the Site, with additional fencing installed along the northern boundary, at the Site entrance. A palisade security gate has been installed 30m back from the entrance along with a self-contained welfare

unit, with manned security.

### 3.4 Parking Management

3.4.1 Parking within the Site is strictly controlled, and requirements and restrictions are included as part of the Site induction.

#### Phase 1

3.4.2 In total, 23 associated parking spaces have been provided adjacent to the welfare cabins for staff, of which four will be accessible parking bays. An additional bay provides motorcycle parking for four motorcycles. A cycle shelter provides space for six bicycles adjacent to the accessible bays. Car parking provision is shown on the Site layout drawing in Appendix A.

3.4.3 As part of the Site induction, arrangements re parking are communicated to new staff, operatives and subcontractors. This includes arrangements for parking where applicable or alternative available e.g. off-site parking or public transport. All staff who arrive on site are met and are signed in by the site security. A shuttle service is used to bring staff to/from the station as necessary and will be used to bring staff to site when an offsite parking facility has been agreed.

3.4.4 All parking on-site is per parking signs and rules and avoid creating any form of safety hazard when parking or parked within the Site.

3.4.5 The site also accommodates circa 180 operatives at peak periods. Operatives working at the Site are transported to the Site by minibus from the station. Minibuses will also transport staff utilising public car parking sites in Huddersfield Town Centre. Some staff may travel to Site via public transport, or walk or cycle to work. No parking provision is made for operative staff at the Site.

3.4.6 No parking by contractors, site operatives, visitors or delivery vehicles is permitted on the local highway network at any time during the use of the Site.

3.4.7 Operatives are required to clock in and out at the start and finish of each shift and it is assumed that no operatives will work outside of the Site working hours.

3.4.8 Staff time is more variable; support and admin staff will keep more traditional office hours while delivery staff (engineers and construction managers) will be closer to the operative hours.

3.4.9 There is no requirement for temporary traffic regulation orders during this phase..

3.4.10 Regarding deliveries, a site delivery plan is in place and this is issued to all suppliers which details vehicle routes, limits to delivery times and general site rules. In addition to this it is made clear to all suppliers that there is to be no waiting on the existing road network and vehicles are to enter straight into site.

#### Phase 2

3.4.11 Staff parking will be provided at the TRU Flint Street site (subject to separate planning approval ref 2025/90618). From August 2025, no staff parking is proposed at the Hillhouse site).

- 3.4.12 No parking by contractors, site operatives, visitors or delivery vehicles will be permitted on the local highway network at any time during the use of the Site.
- 3.4.13 As there is now no requirement for a temporary platform, there is also no corresponding requirement for temporary traffic regulation orders during this phase.
- 3.4.14 Regarding deliveries, a site delivery plan is in place and this is issued to all suppliers which details vehicle routes, limits to delivery times and general site rules. In addition to this it is made clear to all suppliers that there is to be no waiting on the existing road network and vehicles are to enter straight into site.

### 3.5 Site Layouts and Welfare Facilities

#### Phase 1

- 3.5.1 Appendix A1 provides details of the Phase 1 site layout and welfare facilities. Activities undertaken during the Phase 1 have included storage of plant and equipment, earthworks and environmental mitigation. Storage materials include ballast, waste and excavated material. The layout details of this stage of development are provided in planning drawing (Drawing ref 151667-TSA-31-MVL3-DRG-T-LP-162886).
- 3.5.2 Welfare cabins were installed to the east of the Site to accommodate circa 30 staff. 14No. cabins are required to meet welfare requirements. The cabins measure 9.75m in length, are 3.0m wide and 2.6m high. They have been double stacked so the overall height on the welfare area will be circa 5.2m.
- 3.5.3 Details of the welfare facilities are provided on the layout drawing; details of the specification are provided in Document Ref - SA25 Ver 4.0. Please note these cabins will be removed from site during summer 2025.

#### Phase 2

- 3.5.4 Rail systems staff welfare cabins (3-storey) were installed close to the centre of the site during Phase 1 works. Details of the cabins locations as well as floor plans and elevation details are provided in Appendix A2 (Drawing ref 112347 - D-001F- and 112347 - D-001E). These cabins will remain on site until the end of 2028 (i.e. during Phase 2).
- 3.5.5 Information is provided below in terms of colour details for the modular units. See insert 3-2 for details.

Modular fascia's	Modular Unit
<p><b>Reliable Blue</b> RAL 5022 Hex: #00266F RGB: 0, 38, 111 CMYK: 100, 90, 5, 25</p>	<p><b>Pebble Gray</b> RAL 7032 Hex: #b5b0a1 RGB: 181, 176, 161 CMYK: 15, 10, 25, 20</p>

- 3.5.6 Doors, door frames and window frames are white.



**Insert 3-2 Phase 2 welfare cabins**

3.5.7 Activities undertaken during the Sites use as a strategic construction compound (Phase 2) will include storage of plant and equipment and areas for trackwork and overhead line equipment (OLE) assembly. The Site will continue to be used for the storage of ballast, waste and excavated material. The layout details of this stage of development are provided in planning drawing (Appendix A2) (Drawing ref 151667-TSA-31-MVL3-DRG-T-LP-162881).

### **3.6 Railway sidings**

3.6.1 Railway sidings have been constructed for the stabling of trains, to replace the loss of sidings provision at Huddersfield Station. The compound would provide stabling to accommodate a maximum of 16 carriages.

3.6.2 The sidings have been constructed under Network Rail's Permitted Development Rights through Part 18 of the Town and Country Planning General Development Order.

### **3.7 Acoustic barrier**

3.7.1 Details of the acoustic barrier including its location and elevation details are provided in drawing 151667-TSA-31-MVL3-DRG-T-LP-162888 (see Appendix B).

3.7.2 The main function of the barrier is to address operational effects, i.e. during the Site's use as a sidings site, installation of noise attenuation will take place following construction of the retaining wall) to reduce the effects on local noise sensitive receptors during construction.

3.7.3 The acoustic barrier is a reflective barrier and therefore is to be close boarded with minimum mass of 20kg/m<sup>2</sup> and a DLSI value of at least 25db, in accordance with BS EN 16272. It will be 2m in height.

### **3.8 Retaining Wall**

- 3.8.1 A retaining wall has been constructed to the north of the compound site. Earthworks have been regraded at 1:2 to tie in with the top of the proposed retaining wall. Details of the retaining wall are shown on 'Sections and Elevations sheets 1 and sheet 2', drawing reference: 151667-TSA-31-MVL3-DRG-T-LP-162884 and 151667-TSA-31-MVL3-DRG-T-LP-162884.

### **3.9 Operating hours**

- 3.9.1 There are no proposed changes to the approved operating hours.

### **3.10 Vegetation Screening/Landscape proposals**

- 3.10.1 The amended proposal (subject of this submission) does not require any additional removal of planting within the site other than previously identified in the approved permission.

#### 4. APPLICATION DRAWINGS

- 4.1.1 The following plans and documents are submitted in support of this application (see Table 4-1).

**Table 4-1: Submitted Planning Drawings**

Title	Reference
Site Location Plan	151667-TSA-00-TRU-REP-W-EN-001075
Site Plan Sheet 1 of 3	151667-TSA-31-MVL3-DRG-T-LP-162880
Site Plan Sheet 2 of 3	151667-TSA-31-MVL3-DRG-T-LP-162881
Site Plan Sheet 3 of 3	151667-TSA-31-MVL3-DRG-T-LP-162882
Sections and Elevations Sheet 1	151667-TSA-31-MVL3-DRG-T-LP-162883
Sections and Elevations Sheet 2	151667-TSA-31-MVL3-DRG-T-LP-162884
Sections and Elevations Sheet 3	151667-TSA-31-MVL3-DRG-T-LP-162885
Vehicle Tracking	151667-TSA-31-MVL3-DRG-T-LP-162886
Site Layout – Phase 1	151667-TSA-31-MVL3-DRG-T-LP-162886
Site Layout – Phase 2	151667-TSA-31-MVL3-DRG-T-LP-162881
Welfare Building Details (Phase 1)	SA25 Ver 4.0
Welfare Buildings Elevations (Phase 2) – Proposed Floor Plans and Elevations	112347 - D-001F
Welfare Buildings Elevations (Phase 2) - Proposed Floor Plans Elevations	112347 - D-001E
Acoustic barrier Details	151667-TSA-31-MVL3-DRG-T-LP-162888

- 4.1.2 The following drawings/documents are not amended from the approved scheme and therefore do not form part of this submission.

Outline Landscaping Plan	151667-TSA-00-TRU-REP-W-EN-001074
Arboricultural Impact Assessment	151667-TSA-00-TRU-REP-W-EN-001078
Transport Statement	151667-TSA-00-TRU-REP-W-EN-001070
Phase 1 Desk Study (Land Contamination)	151667-TSA-00-TRU-REP-W-EN-001072
Noise and Vibration Assessment Report	151667-TSA-00-TRU-REP-W-EN-001071
Flood Risk Assessment (FRA)	151667-TSA-00-TRU-REP-W-EN-001067
Ecological Impact Assessment (EclA)	151667-TSA-00-TRU-REP-W-EN-001068
Code of Construction Practice Part A	151667-TSA-00-TRU-REP-W-EN-001079

## 5. DESIGN CHANGES

5.1.1 The main design change that has led to this submission is a change in the scope of the TRU programme to remove the requirement for a temporary platform at this location. The train operating companies have implemented an alternative strategy for rail users and therefore this infrastructure is no longer required.

5.1.2 There are a number of minor changes that have been realised due to design development. The table below shows provides details of all changes included in this submission.

**Table 5-1: Design Changes**

Title	Reference	Planned changes
Site Location Plan	151667-TSA-00-TRU-REP-W-EN-001075	<ul style="list-style-type: none"> <li>No changes to site location plan or planning application boundary</li> </ul>
Site Plan Sheet 1 of 3	151667-TSA-31-MVL3-DRG-T-LP-162880	<ul style="list-style-type: none"> <li>No changes to works approved under approved permission.</li> <li>Change to sidings area (works are being delivered under Network Rail's PDR) details are included for information only</li> </ul>
Site Plan Sheet 2 of 3	151667-TSA-31-MVL3-DRG-T-LP-162881	<ul style="list-style-type: none"> <li>Removal of parking bays associated with welfare cabins</li> <li>Further details provided for DNO facility</li> <li>Highway / access to temporary platform area removed</li> <li>Change to sidings area (works are being delivered under Network Rail's PDR) details are included for information only</li> </ul>
Site Plan Sheet 3 of 3	151667-TSA-31-MVL3-DRG-T-LP-162882	<ul style="list-style-type: none"> <li>Highway / access to temporary platform area removed</li> <li>Removal of temporary platform and associated access infrastructure</li> <li>Inclusion of access road and gate (providing access to eastern section of site)</li> </ul>
Sections and Elevations Sheet 1	151667-TSA-31-MVL3-DRG-T-LP-162883	<ul style="list-style-type: none"> <li>Footway between access road and retaining wall has been removed and replaced by grass verge</li> <li>Change in detail on retaining wall from masonry cladding to concrete cladding</li> <li>Details of acoustic barrier now known - drawing updated to reference detailed drawing</li> </ul>
Sections and Elevations Sheet 2	151667-TSA-31-MVL3-DRG-T-LP-162884	<ul style="list-style-type: none"> <li>Footway between access road and retaining wall has been removed and replaced by grass verge</li> <li>Change in detail on retaining wall from masonry cladding to concrete cladding</li> <li>Details of acoustic barrier now known - drawing updated to reference detailed drawing</li> </ul>
Sections and Elevations Sheet 3	151667-TSA-31-MVL3-DRG-T-LP-162885	<ul style="list-style-type: none"> <li>Change to sidings area (works are being delivered under Network Rail's PDR) details are included for information only</li> </ul>



## 6. PROPOSED CHANGES TO CONDITIONS ATTACHED TO PLANNING REF 2021/94337

6.1.1 The approved permission for the Hillhouse Compound site (ref 2021/94337) was subject to the discharge of numerous conditions. To date a number of these conditions remain outstanding and where available detail has been included within this submission. The following section details the status of the planning conditions under planning ref 2021/94337 and the applicant's proposal for where detail is now submitted that will enable removal of these conditions from any subsequent permission. For completeness the applicant has also identified those conditions that will remain outstanding and should be attached to any subsequent permission.

### 6.2 Approved Conditions

6.2.1 The following conditions have been discharged by the LPA:

- Condition 19 – BNG (Planning Ref: 2022/92438)
- Condition 22 – Protection of coal chutes (Planning Ref: 2022/92438)
- Condition 23 – Drainage Design (Planning Ref: 2023/91827)
- Condition 24 – Storm event assessment (Planning Ref: 2023/91827)
- Condition 25 – Temporary Surface Water Drainage (Planning Ref: 2022/92438)
- Condition 26 – YW Asset Protection Strip (Planning Ref: 2023/91827)

6.2.2 The proposed design changes do not affect the submissions to date and therefore the information previously submitted and the DoC of such remains valid.

### 6.3 Outstanding Conditions to be removed/varied

6.3.1 Additional details have been included within this submission. This application seeks to vary or remove a number of planning conditions from planning ref 2021/94337. Details of the proposed action and justification is provided in the table below.

**Table 6-1: Removal/Variation of Conditions**

Condition No.	Condition details	Variation/removal request	Details of where considered in this submission
Condition 3	Temporary Platform Details	The proposal for a temporary platform as part of the development has been removed, therefore this condition is no longer required. Request removal of Condition 3 under any subsequent approval	There is no longer a requirement for a temporary platform at the Hillhouse site, the submitted planning drawings have been updated to reflect this. Therefore this condition is no longer appropriate/required and should be removed from any subsequent permission.

Condition 4	Site compound offices and storage area details	Request consideration of submitted information and removal of Condition 4 under any subsequent approval	Details of the are included in this submission see section 3.6
Condition 5	CTMP	Request consideration of submitted information and removal of Condition 5 under any subsequent approval	The required details are included in this submission see Appendix C
Condition 6	Parking on the highway	Request consideration of submitted information and removal of Condition 6 under any subsequent approval	There is no longer a requirement for a temporary platform at the Hillhouse site, the submitted planning drawings have been updated to reflect this. There is therefore no parking associated with the temporary platform and queuing of rail replacement services. Details relating to parking of other vehicles is included in section 3.4.
Condition 7 -	Commuter Travel Plan	The proposal for a temporary platform as part of the development has been removed, therefore this condition is no longer required. Request removal of Condition 7 under any subsequent approval	There is no longer a requirement for a temporary platform at the Hillhouse site, the submitted planning drawings have been updated to reflect this. This condition was directly related to that requirement and therefore this condition is no longer appropriate/required and should be removed from any subsequent permission.
Condition 8 -	Drainage details -	Request consideration of submitted information and removal of Condition 8 under any subsequent approval	The required details are included in this submission see Appendix D
Condition 10	CMP	Request consideration of submitted information and removal of Condition 10 under any subsequent approval	The required details are included in this submission see Appendix E
Condition 12 -	Noise Assessment	Request consideration of submitted information and removal of Condition 12 under any subsequent approval	The required details are included in this submission see Appendix F

Condition 14	Noise barrier -	Request consideration of submitted information and removal of Condition 14 under any subsequent approval	The required details are included in this submission see Appendix B.
Condition 15	Contaminated Land	Request consideration of submitted information and removal of Condition 15(a) Phase II investigation and 15(b) Remediation Strategy under any subsequent approval	The required details are included in this submission see Appendix G1 and G2
Condition 21	Archaeology -	Request consideration of submitted information and removal of Condition 21 under any subsequent approval	The required details are included in this submission see Appendix H

#### 6.4 Outstanding Conditions to be attached to any new permission

6.4.1 The following information is still outstanding and therefore the applicant proposes that these form planning conditions to any new permission:

- Condition 16 – Validation Report
- Condition 18 – LEMP
- Condition 20 - Biodiversity Net Gain - monitoring, maintenance, management and reporting element only (Strategy previously approved)

6.4.2 The following are compliance conditions and we assume that they also be attached to any new permission:

- Condition 9 – Details of entrance gate
- Condition 11 – Measures to control dust
- Condition 13 – Noise from fixed plant and equipment
- Condition 17 – Unexpected contamination
- Condition 19 – Timing of vegetation removal
- Condition 27 – Establishment of risks posed by coal mining
- Condition 28 – Site is suitable/stable for development

## 7. PLANNING CONTEXT

### 7.1 Traffic

- 7.1.1 The TS submitted under the approved scheme identified potential impacts on the highway network from the use of the Site as a construction compound, along with mitigation measures to address these impacts. It is anticipated that there will be limited transport impacts associated with the proposed Development.
- 7.1.2 The TS states that any impact on the road network would be temporary, short-term and insignificant. Once the works are complete there would be no residual effects. The report concludes that there are no operational or safety impacts on the road network from trips generated by the proposed Development, and that the impact of the proposed Development on the road network would not be severe.
- 7.1.3 The proposed changes will not result in any significant changes to this assessment. Future use of the site from summer 2025 will lead to a reduction in vehicles accessing the site (particularly in the context of the removal of the temporary platform), and these will be limited to delivery vehicles in the main. The proposed use of Flint Street to access/egress the compound is considered appropriate.
- 7.1.4 A construction traffic management plan is included at Appendix C of this submission.

### 7.2 Landscape and Visual Impact

- 7.2.1 An Outline Landscape Plan was submitted and approved through application 2021/94337.
- 7.2.2 Further details will be submitted, once available which will provide the following level of information:
- A plan of ecological mitigation details including areas of new plantings and details of any habitats created or enhanced.
  - Details of hard and soft landscaping works, including the following details:
    - full detailed landscape plans indicating full planting specification, including
    - layout, species, number, density and size of trees, shrubs, plants, hedgerows
    - and/or seed mixes and sowing rates, including extensive use of native
    - species and the avoidance of invasive species;
    - any areas of grass turfing or seeding and depth of topsoil to be provided
- 7.2.3 Consideration has been given to the above design changes, with the primary focus on the visual impact changes. The overall scale of the proposed development has not changed, and therefore the changes will not result in any significant change to the impacts on visual amenity

### 7.3 Water and Flood Risk

- 7.3.1 No material changes are proposed which will impact on the potential impacts to water quality or flood risk. A drainage strategy has been submitted with this application (see Appendix D). The overall scale of the proposed development has not changed, and therefore the changes will not result in any significant change to the impacts on Water and Flood Risk.

### 7.4 Noise and Vibration

- 7.4.1 The results indicate that potential significant adverse impacts have been identified at NSR to the

north of the Site due to night-time construction works while the Site is being used as a construction compound during the construction of the wider TRU Scheme.

- 7.4.2 An assessment of potential noise impacts was undertaken prior to the development being brought into use and this is included in Appendix F.
- 7.4.3 The TRU Alliance undertake screening assessments of the works at the site to ensure that noise and vibration impacts are minimised. This screening assessment tool identifies where works are of a nature that a s.61 application is required.
- 7.4.4 Best Practicable Means (BPM) will be employed to reduce potential noise impacts from the construction works. In addition, noise impacts will be mitigated by the installation of a noise barrier at the north eastern end of the Site, adjacent to the inside of the boundary fence (details provided in Appendix B). It is not considered that the works will trigger any further action under the Noise Insulation Regulations.
- 7.4.5 No further mitigation is required as a result of the changes identified in this application.

## **7.5 Air Quality**

- 7.5.1 The original assessment approved under the existing permission concludes that the overall operational air quality impacts of the proposed Development are considered to be insignificant. This conclusion is not affected by the changes identified in this application.

## **7.6 Ecology**

- 7.6.1 An Ecological Impact Assessment (EclA) was submitted in support of the approved application.
- 7.6.2 All works were undertaken in line with the approved method statements and required mitigation. Pre-construction surveys were carried out in relation to badgers and bats, no evidence of protected species were identified within the site boundary and no specific mitigation was deemed necessary.
- 7.6.3 There are no further anticipated impacts on biodiversity assets as a result of the proposed changes identified in this submission.
- 7.6.4 NR is committed to achieving 10% biodiversity net gain to compensate for the proposed Development. A BNG delivery strategy has now been approved and the details submitted are not affected by the changes identified in this submission. BNG for the proposal is non-statutory and a report will be issued to the Council, as set out in the strategy, which demonstrates that 10% net gain has been achieved.

## **7.7 Cultural heritage**

- 7.7.1 There will be no physical change to the historic fabric of the Grade II Listed Railway Coal Chutes and Tramway with Walls and gates structure as a result of the proposed Development..
- 7.7.2 The Proposed Development will continue to result in a permanent adverse impact to the sites of Hillhouse Sidings and Engine Sheds (ATK59) and the White Stone Engine Shed (HER 18375), Details of the works carried out are provided in Appendix H1. This approach has been discussed with WYAAS and the submitted information has been reviewed and agreed as appropriate prior to submission.
- 7.7.3 There are no further anticipated impacts on cultural heritage assets as a result of the proposed changes identified in this submission.

## 7.8 Land contamination

- 7.8.1 There is potential for land contamination impacts associated with human activity on the Site (including commercial premises) and from Secondary A and undifferentiated aquifers. Potentially significant adverse effects are anticipated in localised areas of the proposed Development.
- 7.8.2 A Phase 1 Desk Study was submitted under the approved scheme. A Phase 2 Site Investigation Report is included at Appendix G1 and a Remediation Statement at Appendix G2. A validation report will be submitted to the LPA for approval at the appropriate time.
- 7.8.3 No further mitigation is required as a result of the changes identified in this application.

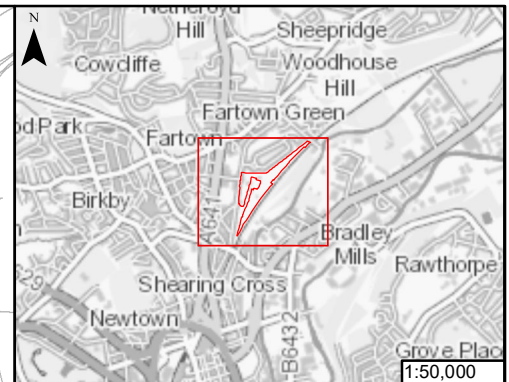
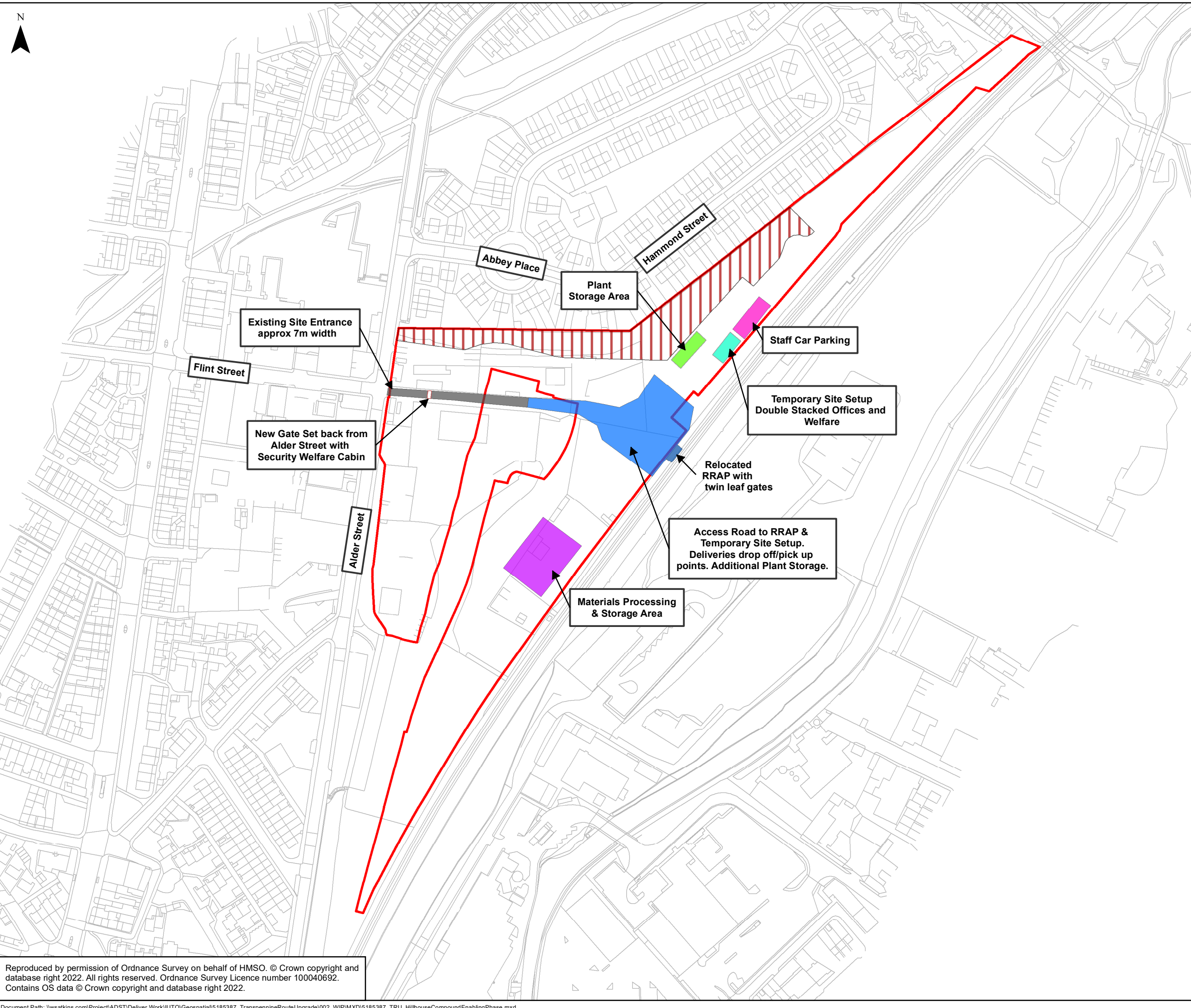
## 8. CONCLUSIONS

- 8.1.1 The planned changes to the approved permission do not alter the previous conclusion; that the use of this large brownfield site on a temporary basis to serve the TRU programme is a key component in delivering the Upgrade. It is fully supported by policy LP19 and allocation TS7 of the Local Plan in helping to deliver a step change in improving the Trans-Pennine rail corridor and the level of service capacity improvement and environmental benefit from electrification it will bring, and the subsequent positive effect it will have on the economic well-being of the District.
- 8.1.2 The changes proposed to the extant consent are of a relatively minor nature, the key one being the removal of the proposed temporary platform. There are no other material considerations which should preclude consent from being granted for the proposed Development.

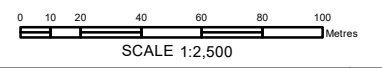
# APPENDICES

## APPENDIX A – SITE LAYOUTS AND WELFARE FACILITY DETAILS

## APPENDIX A.1 PHASE 1 SITE LAYOUT



- Application Site Boundary
- Access Road Extension to RRAP & Temporary Site Setup
- Car Parking
- Existing Access Cobble Sets Road
- New Gate
- Material Storage Area
- New RRAP with twin leaf gates
- Plant Storage Area
- Retaining Wall
- Temporary Site Setup Double Stacked Offices & Welfare



P01	11/04/22	FIRST ISSUE	MMN	NB	NB
Rev	Date	Description of Revisions	Drwn	Chkd	Appr
Status	SHARED				Suitability



Project  
**HILLHOUSE CONSTRUCTION COMPOUND**

Contract No.  
**151667**

Drawing Title

**HILLHOUSE CONSTRUCTION COMPOUND  
ENABLING PHASE**

Designed	M.Mallesha Nayaka	Signed Electronically	Date	11/04/2022
Drawn	M.Mallesha Nayaka	Signed Electronically	Date	11/04/2022
Checked	N.Booth	Signed Electronically	Date	11/04/2022
Approved	N.Booth	Signed Electronically	Date	11/04/2022

Scale(s)  
1:2,500

ELR & Project Chainage  
---

Alternative Reference  
---

Sheet  
1 of 1

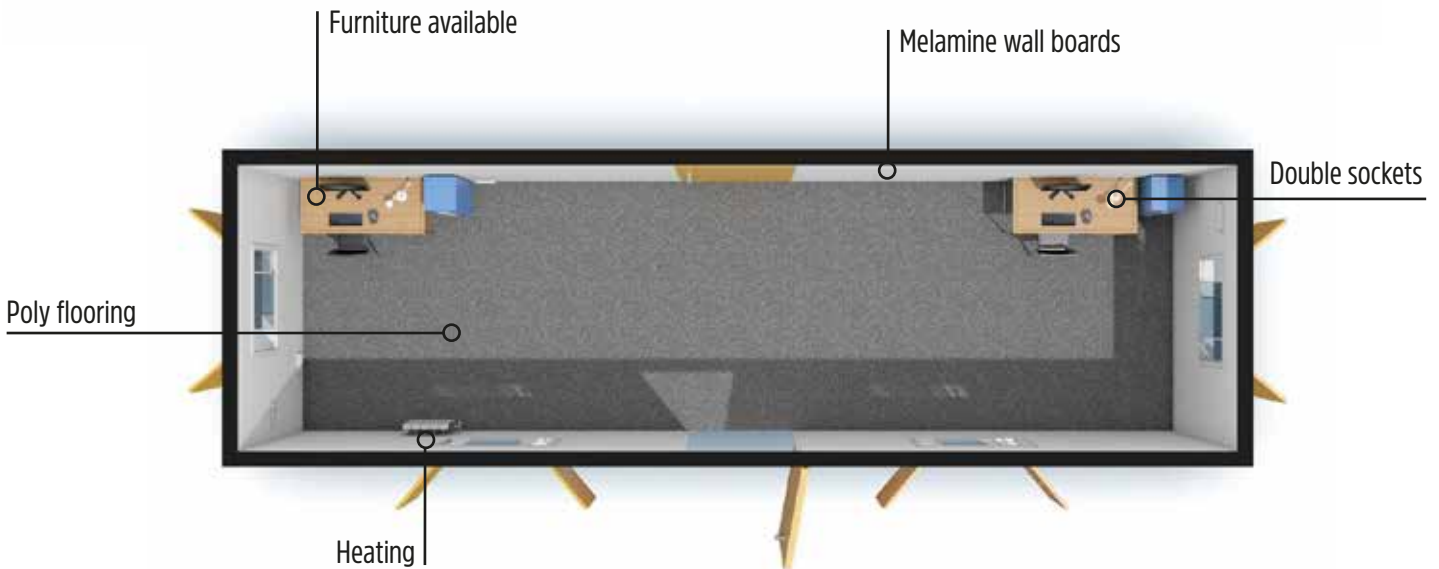
Drawing Number  
151667-TSA-31-MVL3-DRG-T-LP-162886

Revision  
P01

Reproduced by permission of Ordnance Survey on behalf of HMSO. © Crown copyright and database right 2022. All rights reserved. Ordnance Survey Licence number 100040692. Contains OS data © Crown copyright and database right 2022.

## APPENDIX A.1 PHASE 1 WELFARE DETAILS

# 32FT OFFICE



## Dimensions

- > Length: 9754 mm
- > Width: 3000 mm
- > Height: 2620 mm
- > Weight: 4000 kg

## Security

- > Anti-vandal steel shutters
- > Anti-jemmy Kirncroft door
- > UPVC windows

## Facilities

- > Double sockets
- > Light switches
- > Heating
- > Fused spur
- > Door retainer hook
- > Furniture available

## Sustainability

- > Insulation to IEE standards
- > LED, PIR lighting

## Design

- > Flat sided steel
- > Poly floor
- > Melamine wall boards

## Moveability

- > Forkli<sup>o</sup> pock ets
- > Corner li<sup>o</sup>ing points
- > ISO Fastening plates

**LED**  
LIGHTING

**LINKABLE**  
STACKABLE

HEATING

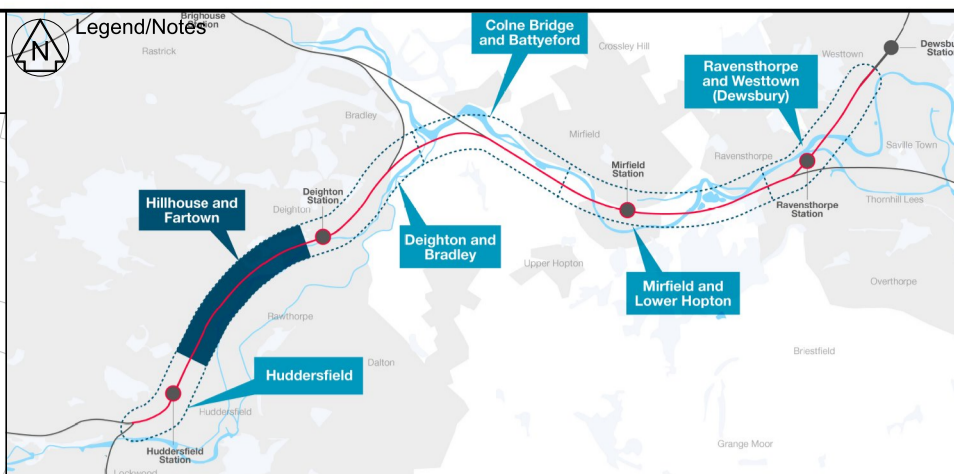
**HIGH**  
SECURITY

FLAT SIDED STEEL

FURNITURE AVAILABLE

DOUBLE SOCKETS

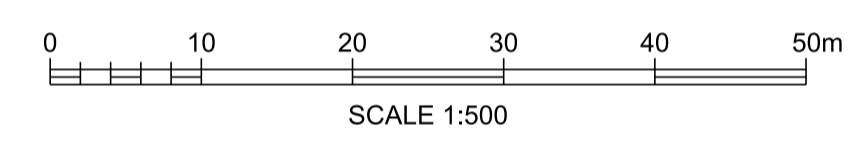
## APPENDIX A.2 PHASE 2 SITE LAYOUT



**Legend**

- Existing Network Rail Boundary
- Planning Application Boundary
- Proposed Infrastructure
- Visibility Splay

- Notes**
- All dimensions are in millimetres unless stated otherwise.
  - Do not scale from this drawing.
  - This drawing is to be read in conjunction with drawings:  
151667-TSA-31-MVL3-DRG-T-LP-162880  
151667-TSA-31-MVL3-DRG-T-LP-162882  
151667-TSA-31-MVL3-DRG-T-LP-162883  
151667-TSA-31-MVL3-DRG-T-LP-162884  
151667-TSA-31-MVL3-DRG-T-LP-162885  
151667-TSA-31-MVL3-DRG-T-LP-162886  
151667-TSA-31-MVL3-DRG-T-LP-162887.
  - Railway sidings and reception rails are to be provided under Part 8 of the General Permitted Development Order, and are shown here for information only.
  - Welfare Building to be delivered under Part 8 of the General Permitted Development Order, and is shown here for information only.



P01	12/11/21	For Planning Application	SM	DM	JH
A01	01/04/25	For Planning Application	SM	DM	JH
P02	27/06/25	Planning Issue	JB	RG	RG
Rev	Date	Description of Revisions	Drawn	Chkd	Appr
Status					Suitability
					<b>S2</b>



Authorised: \_\_\_\_\_ Signed: \_\_\_\_\_ Date: \_\_\_\_\_

Contractor(s)  
**TRU West Alliance**

Location	STALYBRIDGE STN - HEATON LODGE JN (HUDD-MANC LINE)	
Type	CAD Drawing	Sub-type: General Arrangement
Role	Town and Country Planner	Sub-Role: General
Zone	Hillhouse and Fartown	
Phasing	Proposed	Project Stage: GRIP 5

Project  
**Transpennine Route Upgrade**

Contract No.  
**151667**

Contract Title  
**TRU - West of Leeds**

Drawing Title  
**Planning Drawing**

**Hillhouse Construction**

**Facility**

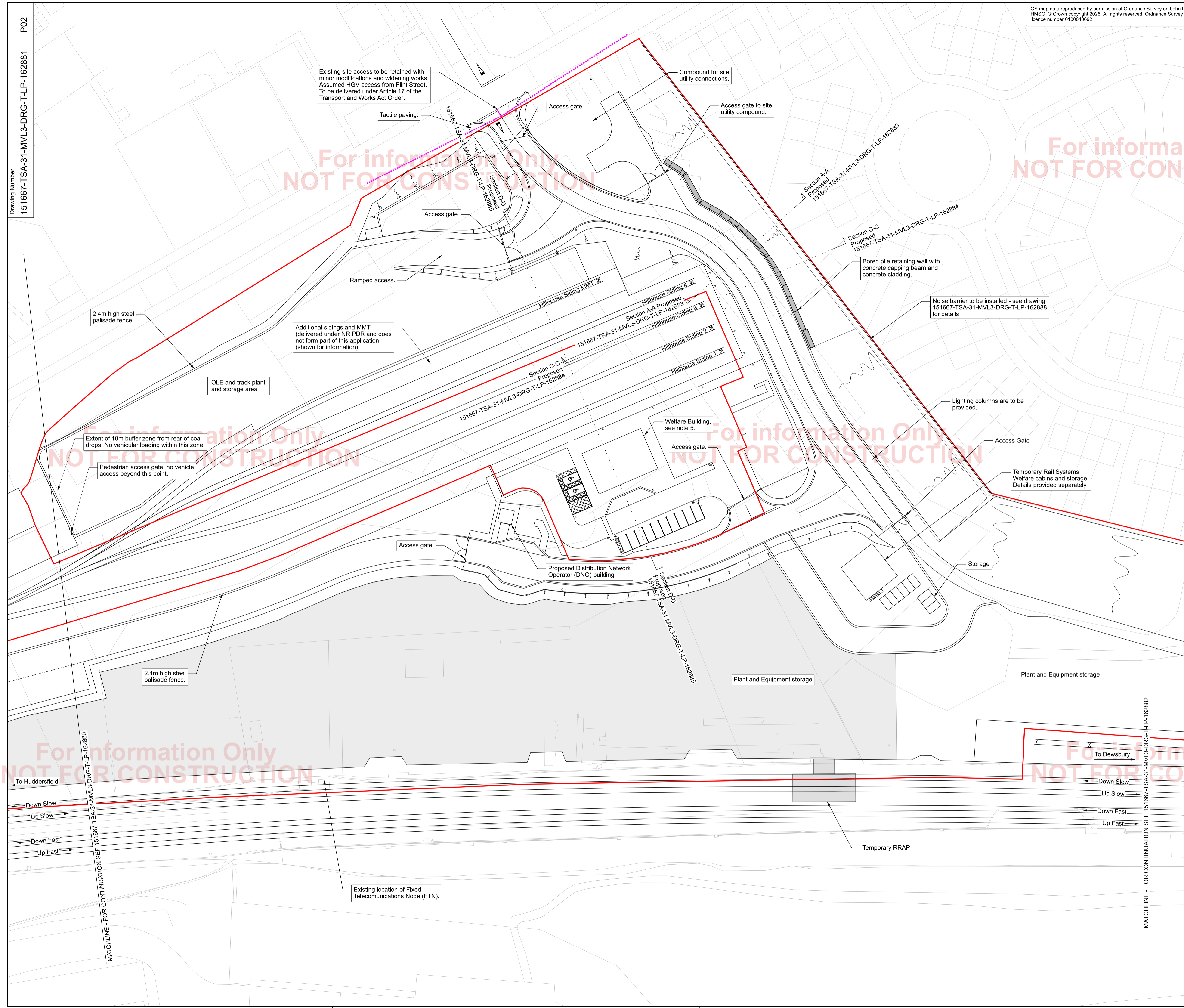
**Site Plan 2**

Designed	J.Bacinski	Signed: <b>Electronically Signed</b>	Date: 27/06/25
Drawn	J.Bacinski	Signed: <b>Electronically Signed</b>	Date: 27/06/25
Checked	R.Gee	Signed: <b>Electronically Signed</b>	Date: 27/06/25
Approved	R.Gee	Signed: <b>Electronically Signed</b>	Date: 27/06/25

Scale(s)  
**1:500** ELR & Mileage **026.0500** to **026.1484**

Alternative Reference  
Drawing Number: **151667-TSA-31-MVL3-DRG-T-LP-162881** Sheet **2** of **3**

Revision  
**P02**



NOT FOR CONSTRUCTION

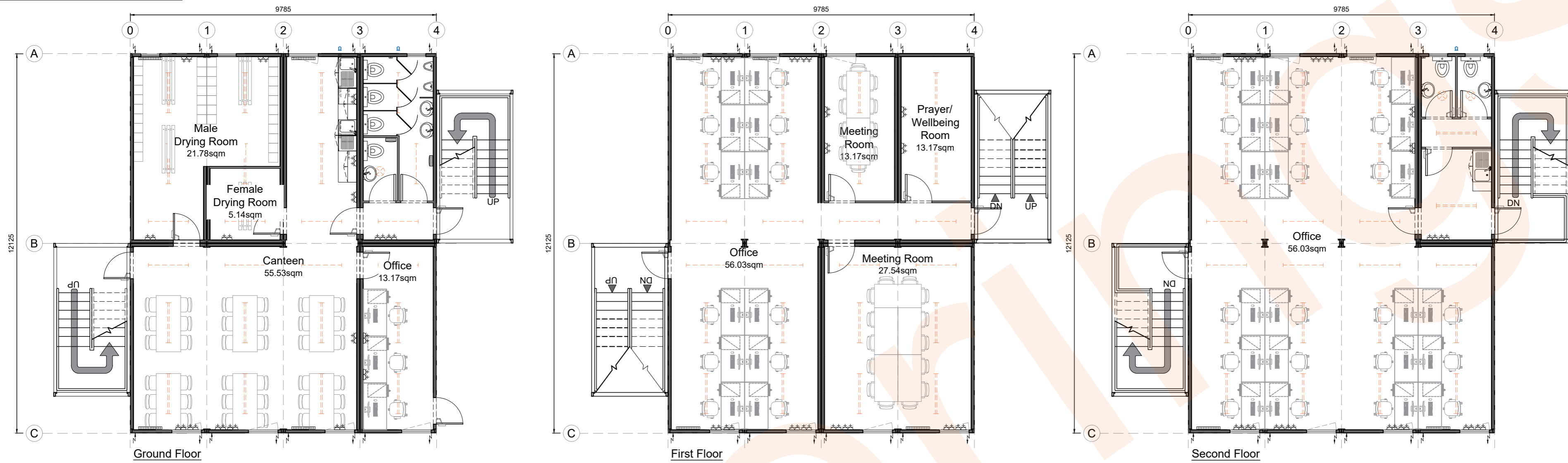
For information Only



## APPENDIX A.2 PHASE 2 WELFARE DETAILS

# Client Layout Approval

Signature: \_\_\_\_\_  
 Name: \_\_\_\_\_  
 Date: \_\_\_\_\_



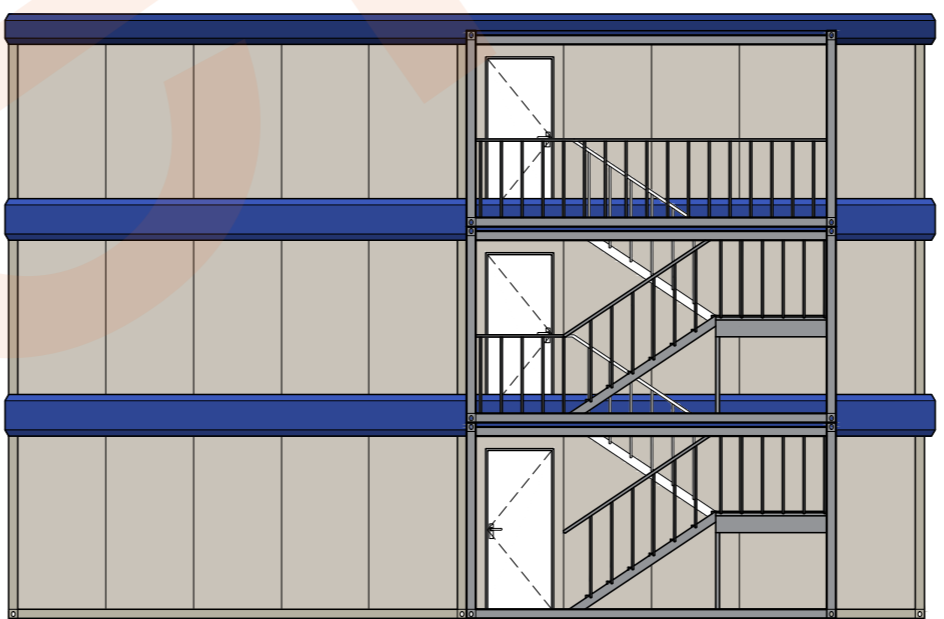
- Key**
- LED 66w/8800lm IP54
  - LED Sensor 66w/8000lm IP54
  - 25w Glass Lamp
  - Double Socket
  - 2 kW E-convector
  - 2 kW Fan heater
  - CEE Exterior Socket
  - Water Inlet
  - Extractor Fan Wall Mounted

- Notes:**
- All Measurements in mm unless stated otherwise.
  - Furniture shown is for illustrative purposes only. For furniture details look at the accompanying quote.
  - This and any accompanying drawings are for illustrative purposes only and as such, the building shown may vary from the actual finished building on site.
  - Lighting Layout Is Indicative and may not be the layout provided on the finished setup.
  - All external doors to have door Closers
  - Socket layout to be confirmed
  - Additional bracing for 3 storey (maximum room size therefore 3x2 cabins).

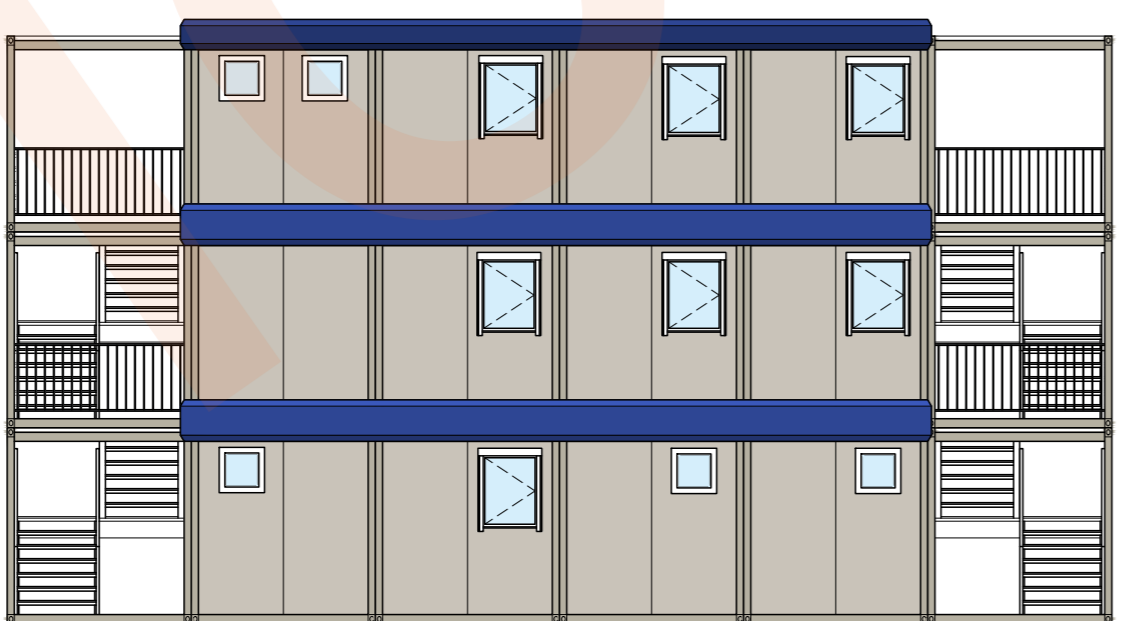
Rev.	Date	Amendment
F	20.05.24	Sockets
E	18.04.24	Layout amendments
D	16.04.24	Phases removed
C	07.03.24	Fascia added.
B	06.03.24	2F added.
A	05.03.24	Phases introduced.



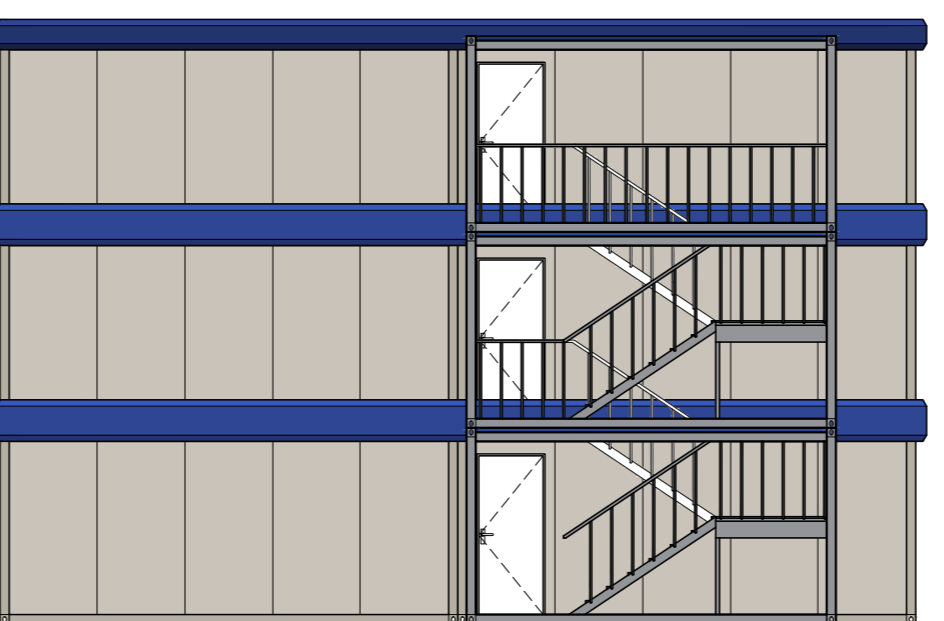
Elevation A



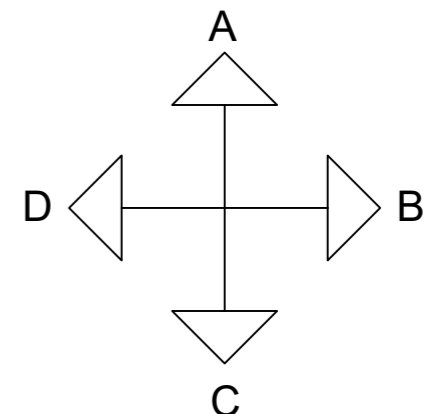
Elevation B



Elevation C



Elevation D



## Elevations



1:100@A2



Client:	HSS
Project:	Huddersfield
Title:	Proposed Floor Plans & Elevations
Unit:	Prima 8
Scale:	1:100@A2
Date:	20.05.2024
Drawn:	PF
Checked:	AT
Drg No:	112347-D-001F

THIS DRAWING IS THE PROPERTY OF PICKERINGS HIRE LTD. IT MAY NOT BE REPRODUCED IN ANY FORM OR BY ANY MEANS FOR ANY PURPOSE WITHOUT WRITTEN PERMISSION.  
 PLEASE NOTE THAT THIS AND ANY ACCOMPANYING DRAWINGS ARE FOR ILLUSTRATIVE PURPOSES ONLY AND AS SUCH, THE BUILDING SHOWN MAY VARY FROM THE ACTUAL FINISHED BUILDING ON SITE.

## **APPENDIX B – ACOUSTIC BARRIER DETAILS**

Submitted under separate cover (Document Ref: 151667-TSA-00-TRU-REP-W-EN-001961)

## **APPENDIX C – CONSTRUCTION TRAFFIC MANAGEMENT PLAN (CTMP)**

(submitted under separate cover – doc reference 151667-TSA-00-TRU-REP-W-EN-001960)

## **APPENDIX D – DRAINAGE STRATEGY**

(submitted under separate cover – doc reference 151667-TSA-00-TRU-REP-W-EN-001959)

## **APPENDIX E – CONSTRUCTION ENVIRONMENTAL MANAGEMENT PLAN**

(submitted under separate cover – doc reference 151667-TSA-00-TRU-REP-W-EN-001962)

## **APPENDIX F – NOISE ASSESSMENT**

(submitted under separate cover – doc reference 151667-TSA-00-TRU-REP-W-EN-001966)

## APPENDIX G - CONTAMINATED LAND

## **APPENDIX G1 - PHASE II INVESTIGATION**

(submitted under separate cover – doc reference 151667-TSA-00-TRU-REP-W-EN-001963)

## **APPENDIX G2 - REMEDIATION STRATEGY**

(submitted under separate cover – doc reference 151667-TSA-00-TRU-REP-W-EN-001965)

## **APPENDIX H – ARCHAEOLOGICAL REPORT**

(submitted under separate cover – doc reference 151667-TSA-00-TRU-REP-W-EN-001968)

Network Rail  
Waterloo General Offices  
London  
SE1 8SW  
[www.networkrail.co.uk](http://www.networkrail.co.uk) **uk**