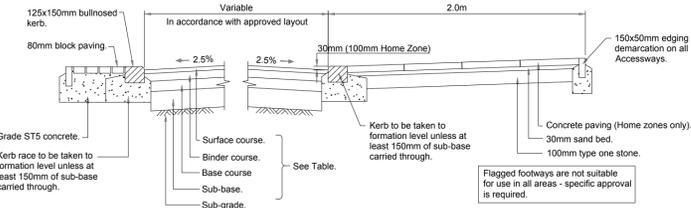


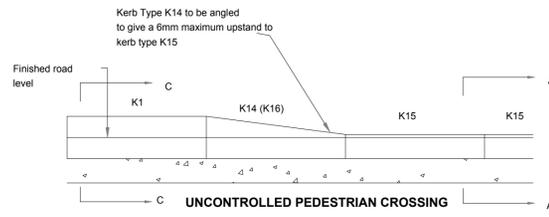
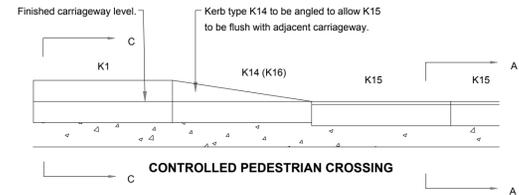
LOCAL RESIDENTIAL STREET, CONNECTOR STREETS AND INDUSTRIAL ROADS. TYPICAL CROSS SECTION.



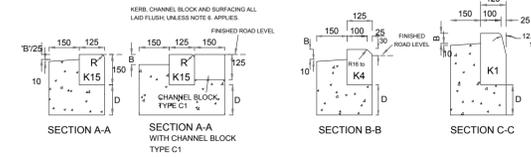
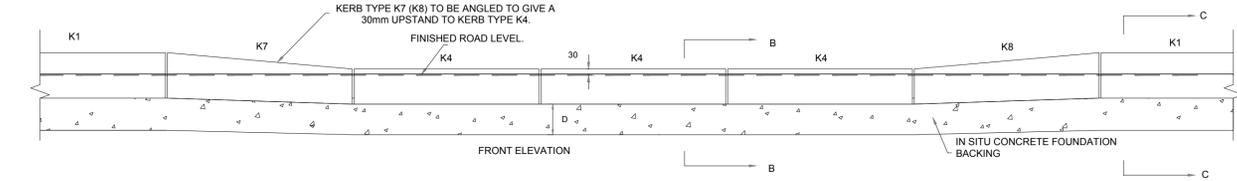
SHADED SURFACE AND HOME ZONE STANDARD TYPICAL DETAIL

Alternative footway construction - Home Zones and Conservation Areas.
Standard 80mm concrete block pavours.
Flags or small unit paving - (Not suitable for footway vehicular crossings or other area vulnerable to vehicular traffic).

PEDESTRIAN DROPPED CROSSING (1 DROPPER + N CENTRES = PC/N)



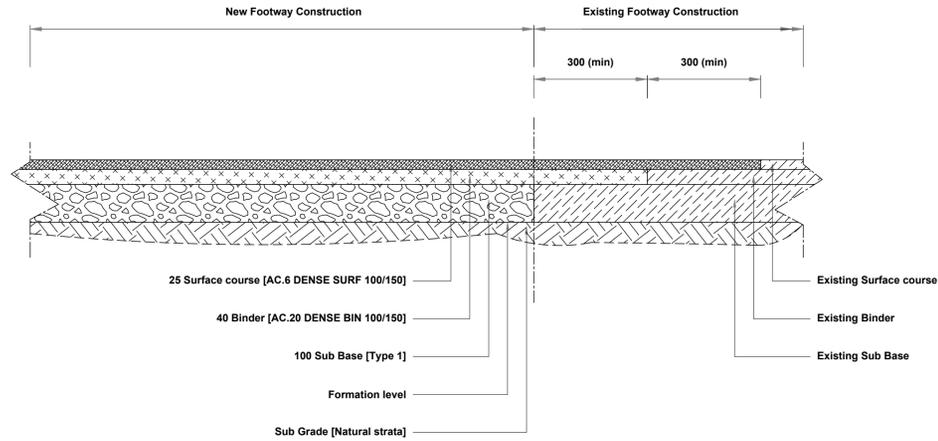
VEHICULAR DROPPED CROSSING (1 DROPPER + N CENTRES = VC/N)



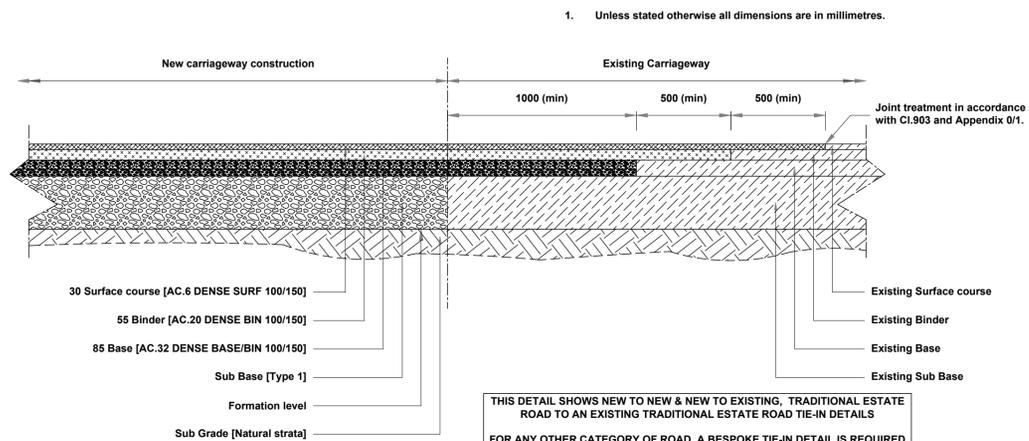
NOTE

- ALL DIMENSIONS ARE IN MILLIMETRES.
- WHERE THE SUFFIX 'V' IS INCLUDED GRADE C25/30 CONCRETE SHALL BE USED IN FOUNDATIONS AND BACKING. ALL OTHER INSITU CONCRETE SHALL BE CLASS C8/10 OR ST2
- CENTRE KERBS TO BE HALF KERB LENGTH WHEN LAID TO RADII LESS THAN 15 METRES.
- NUMBER OF CENTRE KERBS WILL VARY.
- K15 KERBS TO BE SET FLUSH TO CHANNEL BLOCKS EXCEPT WHERE BACKFALL TO FOOTWAY IS UNAVOIDABLE IF SO SET K15 KERBS 6mm ABOVE CHANNEL BLOCKS FOR DRAINAGE PURPOSES
- CHANNEL BLOCKS SHALL NOT BE PROVIDED IF CARRIAGEWAY SURFACING IS NOT INCLUDED IN THE WORKS.

FOOTWAY TIE-IN DETAIL



CARRIAGEWAY TIE-IN DETAIL



THIS DETAIL SHOWS NEW TO NEW & NEW TO EXISTING, TRADITIONAL ESTATE ROAD TO AN EXISTING TRADITIONAL ESTATE ROAD TIE-IN DETAILS
FOR ANY OTHER CATEGORY OF ROAD, A BESPOKE TIE-IN DETAIL IS REQUIRED

PAVEMENT DESIGN

| ROAD LAYER | MATERIAL | MATERIAL CHOICES | LOCAL RESIDENTIAL STREET (max 3) in mm | CONNECTOR STREET (max 4.5) in mm | INDUSTRIAL ROAD (max 11) in mm |
|----------------|--|---|--|----------------------------------|--------------------------------|
| SURFACE COURSE | 10mm stone mastic asphalt surface course SMA 10 surf 40/60 | SMA, Thin Surface Course Systems, AC, HRA | 50 | 50 | 50 |
| BINDER COURSE | 0/20, AC 20 HDM bin 40/60 | AC (DBM 50, HDM 50), HRA | 60 | 60 | 60 |
| BASE COURSE | 0/32, AC 32 HDM base 40/60 | AC | 130 | 150 | 170 |

- msa - Traffic in Million Standard Axes for 40 year period.
- Total Asphalt thickness depends on the traffic (msa)
 - The total Asphalt thickness for Key routes and New roads shall be designed using actual / forecast traffic (msa) using DMRB Standards
 - All longitudinal joints in all layers shall be situated outside wheel track zones, refer MCHW C1 903.21.
 - The joints in different pavement layers should not coincide. Transverse joints shall have an overlap not wider than 300mm. The longitudinal joint overlap width may be extended to a maximum of 300mm. Refer MCHW C1 903.10.

PAVEMENT FOUNDATION DESIGN

| CBR% | SUBBASE ON CAPPING | | SUBBASE ONLY | |
|------|---|------|---------------|--|
| | CAPPING (mm) + SUB-BASE (mm) | | SUB-BASE (mm) | |
| <2.5 | Ground improvement will need to be considered to improve the subgrade CBR | | | |
| 2.5 | 430 | 250 | 420 | |
| 3 | 380 | 230 | 370 | |
| 4 | 320 | 220 | 320 | |
| 5-15 | 260 - 160 | 200 | 280 - 210 | |
| >15 | 150 | 200* | 200* | |

* Minimum required type 1 subbase thickness to achieve 450mm (from the designed final surface) of non-frost susceptible material

FOOTWAY AND CYCLEWAY PAVEMENT DESIGN

| TYPE | CBR | SUB-BASE (mm) | BASE (mm) | | SURFACE COURSE (mm) | |
|--|---------------|---------------|----------------------------|-------------------------|--|--------------------|
| | | | AC 32 dense base 100 / 150 | AC 20 dense bin 100/150 | AC 6 dense surf 100/150 or 15/10P HRA Surf 100/150 | BINDER COURSE (mm) |
| Pedestrian only Footway / Cycleway or Footpath | ≥ 2.5% | 100 | 0 | 50 | 20 | |
| Vehicular Footway Crossing or Light Vehicle footways / cycleways | ≥ 2.5% | 225 | 0 | 50 | 20 | |
| Heavy-vehicle footways / cycleways | ≥ 2.5% | 320 | 90 | 0 | 25 | |
| | ≥ 2.5% & ≤ 4% | 210 | 90 | 0 | 25 | |

General Notes

- This drawing should not be scaled for setting out purposes.
- This drawing shows the detailed design only and is subject to Local Authority approval.
- This drawing is based upon a topographical / ordnance survey provided by others.
- This drawing is to be read in conjunction with all other relevant drawings.
- Any conflict between the details shown on this drawing and those of any other related drawings should be noted to the engineer prior to construction on site.
- All dimensions are in millimetres unless otherwise stated.

Construction Notes

- All levels to be confirmed on site prior to commencement of any works.
- Any existing utility covers retained are to be adjusted to suit proposed levels – cover class to be upgraded if required.
- Any existing utility covers that are damaged will require renewing and resetting.
- Gullies located in shared use surfaces must have suitable pedestrian and cyclist friendly covers. The openings in gully gratings must not align with cyclists anticipated wheel tracks.
- Any soft areas will require excavating until firm ground is found and backfilling with a 6F2 material and to be compacted in 150mm layers.
- Proposals assume a CBR of 2% – CBR tests to be undertaken by others for actual values and construction altered to suit.

Kerbing Notes

- Suitable transition kerbs shall be used at all changes in kerb face.
- For radii of 12m or less – kerbs of the appropriate radius shall be used. For radii 12m and above – straight kerbs 600mm long shall be used.
- The length of any kerb shall not be less than 450mm.



PROJECT TITLE
FORMER ROSE & CROWN INN, KNOWL ROAD, GOLCAR

DRAWING TITLE
TYPICAL SECTIONS

| ORIGINATOR | PROJECT | VOL. | TYPE | ROLE | NUMBER |
|------------|---------|------|------|------|--------|
| PRGN | 2376 | HGN | DR | CH | 0005 |

CLIENT
SOVEREIGN DEVELOPMENT GROUP LTD

| SCALE | SIZE | DRAWN | CHECKED | AUTHORISED | DATE |
|-------|------|-------|---------|------------|--------|
| NTS | A1 | AH | LO | AH | MAY 25 |

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