

# Japanese Knotweed Solutions Ltd

## METHOD STATEMENT

**SITE:** Land off New Hey Road, Huddersfield, HD3 4GP

**CLIENT:** Lidl Great Britian Limited

**ACTIVITY:** Non-native invasive plant remediation method statement

### **Introduction**

This document has been created to assist our site teams on site undertaking the works and also to outline to our client or any third parties what is planned to take place.

In addition to this it to assist our client in discharging the following planning condition:

#### 'Condition 28 – Invasive non-native species

*No development shall take place until details of a scheme to eradicate invasive non-native species within the site has been submitted to and approved in writing by the Local Planning Authority. All works which form part of the approved scheme shall be completed prior to the development first being occupied.'*

JKSL has attended site 09.08.2024 to assess the extents of Japanese knotweed (JK), Giant hogweed (GH) and Himalayan balsam (HB) on site along with any other plants listed on Schedule 9 of the Wildlife and Countryside Act or The invasive species order.

No JK or GH or other plants of concerned were recorded. However HB was noted to be fairly extensive on the north western part of the site along with a single Montbretia (MB) plant.

Multiple areas were recorded on site within the footprint of the proposed development plans. JKSL have therefore recommended a combined mechanical and herbicide remediation strategy to be implemented.

This document should be read in conjunction with the following drawings:

- Drawing JK24-9990-01 which highlights the areas of HB and MB) noted on site
- Drawing JK24-9990-02 which highlights the areas of HB and MB noted on site

### **Planning**

JKSL resource is scheduled to meet the need of the various activities detailed below.

### **Welfare**

Welfare arrangements and site fencing is still to be arranged but it is assumed that they will be provided by either the client or JKSL.

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## Access

Access to the site will be via the existing entranceway of New Hey Road.

## Parking

Our staff vehicles will be parked on the hardstanding on site at all times, not on the road outside.

## Site hours

We have assumed that the site working hours are 08:00 – 18:00 Monday to Friday. We do not anticipate that any works on Saturday or Sunday will be required.

We assume that no machinery work is to commence before 08:00 due to the adjacent residential dwellings.

## Staffing levels

JKSL anticipate having a maximum number of 2-3 employees on site during the mechanical removal works.

## JKSL key staff members

- Surveyor: Alexander Dayes – 07795 386 418 – [alex.dayes@sltd.co.uk](mailto:alex.dayes@sltd.co.uk)
- Contracts manager: Philip Whelan – 07876 558 164 – [phil.whelan@sltd.co.uk](mailto:phil.whelan@sltd.co.uk)
- Site foreman: Peter Whelan – 07796 836 745 – [pete.whelan@sltd.co.uk](mailto:pete.whelan@sltd.co.uk)
- Health and safety: Chris Oliver – 0161 723 2000 – [chris.oliver@sltd.co.uk](mailto:chris.oliver@sltd.co.uk)
- Emergency out of hours contact: Alexander Dayes – 07795 386 418

## Security

The site has an extensive boundary which is currently securely fenced.

Welfare will be provided by either JKSL, the main contractor or Lidl.

Site security for the excavation works (to ensure machinery isn't damaged) has not been included at this stage if needed this will be arranged between JKSL and our client.

## Deliveries

JKSL will be arranging for items of plant to be delivered to site in order to undertake these works.

All deliveries will be arranged with JKSL's contracts manager Phil Whelan, or the relevant foreman, to escort them safely into the site.

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## Fuel Security

If used, we propose to store the fuel bowser/drums next to the compound area during the day. This will be fitted with a padlock, whereby our site operatives hold the key. JKSL propose to refill all machinery at the end of each working day or when necessary. The fuel will be removed from site each night.

When re-fuelling the machines a 'plant nappy' will be used to contain any potential diesel spills.

## Substances Harmful to Health and the Environment

The substances harmful to health and/or the environment which JKSL plan to use on site are as below. These are dealt with in their own COSHH Assessment, attached separately.

- Diesel oil
- Petrol
- Prosolve line marker spray

Additional measures which will be undertaken for the use of these substances:

- When re-fuelling the machines a 'plant nappy' will be used to contain any potential diesel spills.
- Diesel on site will be kept in either a diesel bowser, or in the drums it is supplied in.
- Small amounts of diesel may be transported or supplied in legally approved colour-coded plastic (up to 10 litre) or metal (up to 20 litre) containers, which must be UN approved and marked UN 1203 (petrol) or UN 1202 (diesel). Containers must also have the correct "flammable" hazard symbol and should be stored securely inside the vehicle – ideally within the chemical "safe". A 2kg foam/dry powder / CO2 fire extinguisher should also be available in any van that transports petrol or diesel.

## Preparation

Before attending site all staff will have read and signed the method statement and risk assessment.

If necessary all JKSL site staff will be inducted by the client before works commence. The wagon drivers are not to be inducted as they will technically not be working on the site, merely driving on and off the site.

JKSL's Contracts Manager, Phil Whelan, (or the senior foreman) will conduct a tool box talk on the works after this induction. Phil will also conduct short morning briefings on the day's tasks so that all staff are aware of the tasks ahead.

## PPE

Full personal protective equipment will be worn by our operatives at all times. This includes the following:

- High visibility vest or jacket
- Hard Hat (this can be taken off when operating an excavator)

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- Steel toe capped boots
- Gloves
- Safety Glasses (only if the work activity requires them)
- High visibility trousers may also be worn at times, although this is not compulsory

## **Qualifications**

Our operatives will be appropriately qualified to undertake the task at hand and will have their qualifications on their person at all times.

Our SSSTS qualified person on site will be Richard Heyliger, Alan Brown or Paul Cox.

Our excavator operators are Asbestos Aware trained.

## **Environmental issues**

All works will be undertaken in a controlled manner, as set out in this document, to ensure that they do not disperse Japanese knotweed rhizomes throughout the rest of the site.

As our operatives will be working within areas of infestation, an inspection point will be created within close proximity. This will be clearly marked out and lined with geotextile for the inspection and cleaning of all hand tools and equipment.

A foot wash facility will also be installed and used by each operative every time they leave the contaminated area to ensure that no plant propagules are dispersed via muddy boots.

This will ensure that cross-contamination doesn't occur and the Wildlife & Countryside Act (1981) isn't compromised.

Full details of JKSL's Clean Site Policy can be made available on request, prior to the commencement of works; however measures taken on site will depend on site conditions, and will be implemented by JKSL Site Foreman.

## **Phase 1 – Initial application of herbicide**

### **General**

An application of herbicide will be made in April-May and June-July prior to mechanical works commencing.

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## **Phase 2 – Himalayan balsam and Montbretia mechanical remediation works**

*Duration: Approximately 5 days*

### **General**

It is proposed that the mechanical excavation and removal of HB & MB will take place before construction works commence on site.

However before this can start samples will be need to be taken from the various areas in order to determine the appropriate waste disposal classification. These works will either be implemented by JKSL or they will form part of the clients ground investigation contractor's remit. If it is the latter then JKSL will provide an appropriate clerk of works to ensure that biosecurity measures are implemented and the HB & MB is not accidentally dispersed around the site.

### **Preparation**

Full PPE will be worn at all times.

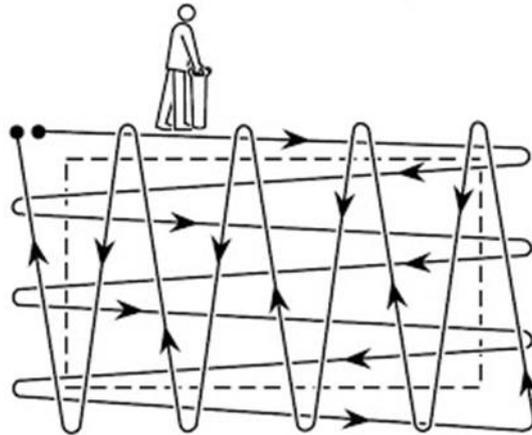
All machinery used on site will be hired in and have all relevant paperwork within the cab or in the possession of JKSL's senior site foreman. All operatives will have relevant qualification details on their persons at all times. All items of plant will be checked daily by JKSL.

The actual excavation area and loading areas (to be arranged with client) will be fenced off and appropriately signed with orange plastic mesh fencing to prevent access by other contractors (where necessary). This will be done first before any other works commence.

At the time of writing the initial document JKSL had not been provided with service drawings – these will need to be provided and consulted before works commence.

A CAT scan sweep will be carried out by a suitably-competent operative. Service drawings should be provided by the client and will be consulted where these are available. The practical inspection will then start with a visual inspection of the site to identify any indications of services (e.g. lamp-posts, manhole covers, drains or similar). The excavation area will then be marked out, and the operative will sweep backwards and forwards across the area in a regular pattern, (e.g north-south and vice versa) and then once this sweep is complete, in the same pattern, at 90° to the first sweep (e.g. east-west and vice versa).

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An appropriate CAT scan sweep pattern.

Suspected services should then be marked, ideally using small flags or line marker spray, or (where appropriate), pegs or road pins, taking due care to avoid any possible contact between pegs or road pins and the services they are marking. Care should still be taken as some kinds of services cannot be detected using this equipment.

## **Excavation of HB and MB on drawing JK24-9990-02**

JKSL will be excavating the areas highlighted on drawing JK24-9990-02 and removing them from site.

Prior to commencing excavation works JKSL will use a cat-scan to detect any services that may be present onsite. No excavations will commence until a permit to dig has been issued by the client (depending on the sites working practises).

Our Site Operatives CAT-scan certificates (and other qualifications) are available on request from the Operations Department at JKSL.

The HB infestations have been individually highlighted on the drawing and the sequencing of the excavation will be determined on site by our contracts manager/senior foreman.

JKSL will prepare the excavation areas on the first day and clear on-site immediate vegetation to facilitate works. The removal of materials off site will then commence in the following days.

JKSL will then carryout removal of all HB plants and their potential seed dispersal zones from the contaminated area highlighted on the drawing.

Set depths will be excavated for both species to account for its root structure and seed dispersal zones.

JKSL follow stringent methods to avoid any spreading of contaminated material across the site during excavation and removal from site. Specifically the excavator used to remove the waste, remains in the marked area until completion, upon which the vehicle is fully decontaminated, as is any machinery used to move material or any vehicle that may enter a contaminated zone.

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Once each area is fully excavated and the materials have been removed the individual area will be signed off as remediated by Phil Whelan or our foreman.

## **Excavation restrictions**

It is assumed that our excavations will not be hampered by underground services, buried structures, buried contaminants or tree roots from trees which are to be retained.

## **Excavations near trees**

Before works commence on site JKSL will erect some temporary tree protection fencing (plastic mesh and road pins) and signage around the root protection zones of the trees in the north west corner.

JKSL have been made aware of the tree protection order/s on the adjacent trees however as our excavation is due to be fairly shallow we hope that no tree roots will be present.

We will however liaise with the local tree officer over excavations in this area in order to obtain approval for excavations to commence inside the tree root protection zone.

At the time of writing this document it was expected that the following works would need to take place in this area.

Prior to the excavations commencing a series of trenches 300mm deep will be excavated running east to west every 2m apart using a spade. If no tree roots are encountered then the excavator will be used to take off 150mm. This will hopefully continue until all the HB materials in the ground is removed or tree roots are encountered.

Trenches will start from the outer extents of the RPA working inwards.

If roots (less than 25mm diameter) are encountered then root pruning will be undertaken, and hessian membrane can be placed against these tree roots, and watered, where necessary.

After our works have taken place in this area permanent tree protection fencing is to be erected by others.

## **Loading area**

A loading area will be created on the in a suitable area near the entranceway as determined by our site foreman.

Due to the tarmac surface the loading area will note be lined with geotextile.

## **Manual Handling**

See separate Manual Handling Risk Assessment.

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Manual handling should always be carried out in a planned way, approved by the Site Foreman. If you do not feel confident or safe carrying out a manual handling task, you should stop work in a safe way and report it to your supervisor, who should re-evaluate the task, contacting senior management if required.

All manual handling activities should take account of "TILE":

**The Task** – which should avoid twisting, bending or lifting wherever possible and should minimise repetitive actions.

**The Individual** – you should take account of your normal ability to lift, as well as what you have been doing that day, whether you have warmed up and whether you may be suffering from fatigue.

**The Load** – you should account for the weight, but also the shape, the grip (or handles) and the weight distribution of the load, as well as the likelihood of the load falling over, rolling away or any other unpredictable or undesirable behaviour. It is generally considered that any load over 25kg (for an individual) or 50kg for a team of two is too heavy and should not be handled manually.

**The Environment** – you should consider the weather conditions, lighting, the conditions on the ground (is it slippy?), the loading and unloading areas and anything else that may affect the task.

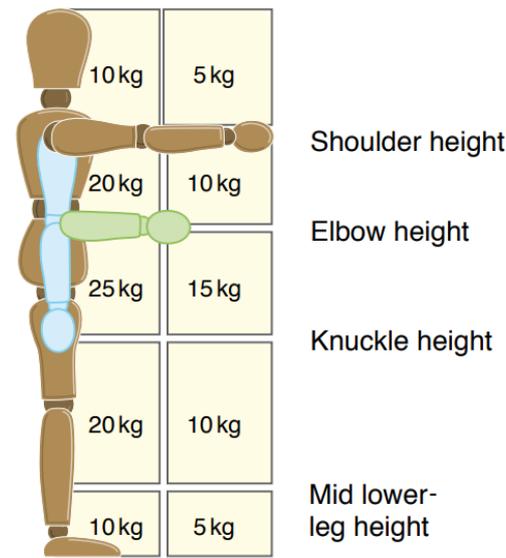


Diagram showing examples of recommended load limits for healthy adult males

## Transferral of Waste to Loading Area

Vehicles are to move and remain within the site boundary only - JKSL will ensure that the only time machinery leaves the site boundary is when it is to be removed from site or for security reasons.

A 9 ton forward tipping dumper truck will be used to move contaminated material across the site to the loading area. This will not be over-loaded (maximum 75% capacity used) to avoid any cross-contamination issues. This dumper will never drive on contaminated material within the excavation or loading area to avoid rhizomes being caught up on the wheels.

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The haul route will be marked out on site and should it deteriorate during the works an alternative route will be used once highlighted and agreed with all parties involved.

The machine loading the dumper will sit on contaminated material at the edge of the excavation area. Once this area has been remediated the dumper can enter the area travelling on remediated land working along the infestation. This will be controlled the excavator driver as well as the dumper driver to ensure that the dumper doesn't travel too far. JKSL will erect a small amount of orange barrier mesh fencing and signs to mark out the loading area/stopping area for the dumpers. This can be moved back across the excavation area as its remediated.

The dumper trucks will follow the designated haul routes marked out by our foreman. Due to the steep slope this is expected to be at the bottom of the old field but outside of the tree root protection area.

## **Removal of materials from site**

While the above operations are taking place JKSL will be removing the material from site to a landfill in tipper wagons.

The landfill licence and all waste carrier licences will be provided upon request. All waste transfer notes will be provided after the works once the paperwork has been received from the landfill.

The wagons will enter the site and position themselves adjacent to the stockpile of materials on the loading area without coming into contact with any contaminated materials. Using an excavator JKSL will then carefully load the waiting wagon.

The loaded wagon will leave site via the site entranceway.

Should any materials be dispersed onto the road via the wheels JKSL's will immediately scrape it up with shovels and brushes then jet wash the area. At the end of the working day this area will be jet washed down to ensure that it is left in the correct way. We have not allowed for a road sweeper, however one can be provided at an additional cost.

JKSL will ensure that the wagons are staggered where necessary thus ensuring that the site, side road and nearby roads does not become clogged up with traffic. Should for some reason such as traffic congestion all the wagons turn up at the same time then they will be asked to wait at a suitable location before being instructed to enter the loading area. However due to the fairly large hardstanding area at the eastern end of Regatta Street we are installing we anticipate that they can wait on site.

All wagons will be covered and will not leave site until the cover is secured in place.

Wagons may arrive on site at 07:30 some mornings but will be asked to wait on site with their engines off until work commences at 08:00.

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## **Asbestos**

Any Site Operative suspecting that they may have encountered asbestos must immediately inform the client's site manager. The site manager shall then investigate any potential discovery of asbestos using their Asbestos Aware training (or using an Asbestos Aware Operative to do so).

Works will be stopped immediately if the discovery of material which contains or is suspected to contain asbestos is confirmed. JKSL's Operations Department shall be informed without delay on 0161 723 2000.

Works shall not be re-commenced until an asbestos risk assessment has been undertaken, a method statement produced by a suitably-competent asbestos professional, and suitable PPE and control measures are in place.

## **Decontamination of machinery**

After each time a machine has been working with materials containing a non-native invasive plant it will be fully decontaminated.

All cleaning of machinery is undertaken by hand using shovels, spades and stiff brushes – a mobile jet wash is then used to clean off the remaining material. All debris are then collected by hand and disposed of in the receptor area. Care is taken to ensure that the angle of the water is facing downward so as not to disperse small fragments of material any distance.

The cleaning of machinery will also not take place near road gullies – only undertaken at location of excavation site.

## **Backfilling**

Due to the shallow depths of the excavations backfilling is not expected to be necessary at this stage.

## **Phase 3 – Ongoing monitoring**

*Duration: Typically a 3 years period*

## **General**

Large areas of HB plants appears to be located to the west of the site boundary within a woodland area and some to the north. Unfortunately access to this area was not obtained during the August 2024 survey. We would therefore recommend that access is obtained, the HB extents recorded at some point prior to the initial application of herbicide being made.

The adjacent landowners will need to be contacted with a view to their HB areas being managed in order to achieve a better level of control in the area.

JKSL will only be able to excavate so close up to the boundary fences/features and therefore an allowance for monitoring and treating the HB in future years should be made.

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At the time of writing this document JKSL have recommended that this will initially be April-May and June-July for 3 years before the sites progress is reviewed. If needed, particularly due to offsite HB plants being untreated then annual maintenance may well need to keep it from re-establishing itself on the site.

**Author: Alexander Dayes**

**Position: Managing Director – Japanese Knotweed Solutions Ltd**

**Signed:**

**Dated: 20.08.2024**

## **Declaration**

I have read and understood the above Method Statement:

Print Name	Date	Signature