

# Infrastructure Projects

## Northern Programmes



## The Network Rail (Huddersfield to Westtown (Dewsbury) Improvements) Order

### Occupation Underbridge (MDL1/10) – Conservation Implementation Management Plan

Network Rail

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## 1. INTRODUCTION

### 1.1 Overview

1.1.1 Network Rail has been granted a Transport and Works Act Order (TWAO) to authorise the Huddersfield to Westtown (Dewsbury) Improvements of the Trans-Pennine Route Upgrade (TRU) programme (Planning Direction for the Order was received from the Department for Transport, referenced TWA/21/APP/03, dated 13 October 2022). The works granted in the Network Rail (Huddersfield to Westtown (Dewsbury) Improvements) Order are referred to as 'the Order Scheme'. The Order Scheme forms part of a wider TRU programme will improve the Trans-Pennine railway between Manchester, Huddersfield, Leeds and York and improve connections between key towns and cities across the north of England. The Order Scheme will contribute to the overall TRU Programme aims of increasing service capacity and offering journey time benefits through:

- Four tracking and upgrading of the existing railway line including track realignment (currently the majority of the railway in the Scheme area has two tracks);
- Electrification of the line;
- Increase in line speeds;
- Provision of sections of new railway;
- Provision of new grade-separated junction within the Ravensthorpe area;
- Remodelling of stations including platform extension works at Deighton, Mirfield and Huddersfield; and
- Provision of replacement station at Ravensthorpe.

1.1.2 The application under the Transport and Works Act 1992 (TWAO) has been determined by the Secretary of State for Transport. As the proposed works affect a number of heritage assets and pass through a number of conservation areas a series of separate Listed Building Consent applications were submitted to Kirklees Council. Under the TWA (Listed Buildings Conservation Area and Ancient Monuments Procedure) Regulations 1992 these applications were automatically called in and considered at the Huddersfield to Westtown (Dewsbury) public inquiry and were also granted permission by the Secretary of State.

1.1.3 Listed Building Consent for the proposed Order Scheme works at Occupation Underbridge (MDL1/10) was granted on 27 June 2022 (Ref: 2021/65/91334/E). A plan showing the location of Occupation Underbridge (MDL1/10) is included in Appendix A.

1.1.4 The consent for the Order Scheme requires the discharge of various conditions attached to the Deemed Planning Permission and Listed Building Consent (refer to Appendix B for the list of agreed Listed Building Consent conditions). Condition 4 of the Listed Building Consent for the works to Occupation Underbridge (MDL 1/10) requires the production of a Conservation Implementation Management Plan (CIMP) before any works to the structure can commence.

### 1.2 Purpose of the Document

1.2.1 A Conservation Implementation Management Plan (CIMP) is a comprehensive document based on a detailed understanding of an historic asset and its significance. It sets out a conservation framework in the context of which the work for the Order Scheme is undertaken. This includes policies to ensure an asset's significance is retained in any future use, repair, alteration, development or management. Where it is not possible to retain an asset's significance, such as through partial or total loss, then the purpose of the CIMP is to set out best practice methodologies for demolition and removal which will be adhered to during the construction works.

1.2.2 It was acknowledged by way of the LBC conditions that, at the time of making the Order and associated Listed Building Consent applications, elements of detailed design and specifics of construction methodology were not fully determined. The CIMP was agreed to provide a suitable framework to capture, review and approve such details, within the context of the heritage significance of the Trans-Pennine Route and the individual Listed Buildings affected.

1.2.3 This CIMP has been produced to satisfy the requirements of Condition 4 of the Listed Building Consent, the wording of which states as follows:

**4. (Conservation Implementation Management Plan)** *No works shall commence until a Conservation Implementation Management Plan (CIMP) has been submitted to and approved in writing by the local planning authority. The approved CIMP will include as a minimum requirement contents based on the model template CIMP structure attached to this list of conditions. The CIMP will specifically include methodologies for:*

*i) fabric removal, masonry repairs, vegetation removal, repointing, metalwork repairs and application of protective paint systems as appropriate;*

*ii) the identification of historic elements of the fabric which once removed may be reused or preserved, and a strategy for their storage or reuse where appropriate;*

*iii) any improvements to the setting to sustain, enhance and better reveal the heritage asset affected;*

*iv) details of any maintenance access regime required (if any);*

*v) provision of heritage interpretation boards during construction works; and*

*vi) dissemination of “toolbox talks” to personnel involved in demolition and construction works.*

*The works must be carried out in accordance with the approved CIMP unless otherwise agreed in writing with the local planning authority.*

## 1.3 Other Supporting Information

1.3.1 The CIMP should be read in conjunction with the specification of materials and finishes, produced to satisfy Condition 2 attached to the Listed Building Consent. The CIMP should also be read in conjunction with the TWA Order and LBC Application documents including the Heritage Assessment<sup>1</sup> and Environmental Statement (ES) chapters<sup>2 3</sup> and the approved design drawings submitted with the Listed Building Consent Application.

<sup>1</sup> Network Rail, 2021. *The Network Rail (Huddersfield to Westtown (Dewsbury) Improvements) Order: Occupation Underbridge (MDL1/10) Heritage Assessment.*

<sup>2</sup> Network Rail, 2020. *Network Rail (Huddersfield to Westtown (Dewsbury) Improvements) Order application Environmental Statement, Volume 2i – Scheme-wide Assessment, Chapter 6 – Historic environment.*

<sup>3</sup> Network Rail, 2020. *Network Rail (Huddersfield to Westtown (Dewsbury) Improvements) Order application Environmental Statement, Volume 2ii – Route Section Assessment, Route Section 6 Ravensthorpe and Westtown, Chapter 6 – Historic environment.*

## 2. STRATEGIC OVERVIEW

### 2.1 Strategic Approach of the CIMPs within the TRU Project

- 2.1.1 As outlined above in Section 1.2, this CIMP has been produced as a requirement to discharge Condition 4 of the Listed Building Consent for Occupation Underbridge (MDL 1/10), for the works undertaken for the Network Rail (Huddersfield to Westtown (Dewsbury) Improvements) Order affecting the Listed Building. As such, the CIMP sits alongside other documents produced for the Listed Building Consent application and the wider Order Scheme.
- 2.1.2 The production of CIMPs for the works where Listed Building Consent was required as part of the Order Scheme was agreed between Network Rail and Kirklees Council (with the support of Historic England) in order to provide further design and construction detail pertinent to the impact on the significance of such Listed Buildings. It was acknowledged that, at the time of making the Order and associated Listed Building Consent applications, elements of detailed design and specifics of construction methodology were not fully determined. The CIMP was agreed to provide a suitable framework to capture, review and approve such details, within the context of the heritage significance of the Trans-Pennine Route and the individual Listed Buildings affected.
- 2.1.3 The content of the CIMP will define how the construction of the Order Scheme at Occupation Underbridge (MDL 1/10) is undertaken with respect to the sensitivity of the historic fabric, character and setting of the Listed Building. The critical elements of this CIMP are in setting out the detailed approaches to be undertaken in respect of: the infilling of the structure; identification of historic elements suitable for re-use; and the provision of temporary interpretation. The CIMP provides a conservation framework for the duration of the construction of the Order Scheme.
- 2.1.4 The methodologies and approaches discussed in this CIMP are conversant of the significance of Occupation Underbridge (MDL 1/10). The significance of Occupation Underbridge (MDL 1/10) is outlined below in Section 3, with further information on its historic context and significance detailed within the Heritage Assessment<sup>4</sup> produced to support the Listed Building Consent application 2021/65/91334/E and in the Environmental Statement produced for the Scheme<sup>5 6</sup>.
- 2.1.5 The heritage significance of the Trans-Pennine Route as a whole, of which Occupation Underbridge (MDL 1/10) is a part, has already been analysed and outlined in the Trans-Pennine Route Upgrade Route-Wide Statement of Significance, produced by Alan Baxter Associates<sup>7</sup> (submitted as part of the Order submission). As well as being shaped by the significance of Occupation Underbridge (MDL 1/10), production of the contents of this CIMP has taken into consideration the wider significance of the Route as a whole, including group value associated with this and other historic structures along it.
- 2.1.6 The design development of the proposals at Occupation Underbridge (MDL1/10) was conversant of group value and the significance derived from relationships between the

<sup>4</sup> Network Rail, 2021. *Network Rail (Huddersfield to Westtown (Dewsbury) Improvements) Order Occupation Underbridge (MDL1/10) – Heritage Assessment*.

<sup>5</sup> Network Rail, 2020. *Network Rail (Huddersfield to Westtown (Dewsbury) Improvements) Order application Environmental Statement, Volume 2i – Scheme-wide Assessment, Chapter 6 – Historic environment*.

<sup>6</sup> Network Rail, 2020. *Network Rail (Huddersfield to Westtown (Dewsbury) Improvements) Order application Environmental Statement, Volume 2ii: Ravensthorpe and Westtown, Chapter 6 – Historic Environment*.

<sup>7</sup> Alan Baxter Associates, 2019. *TransPennine Route Upgrade Route-wide Statement of Significance*.

structure and other similar and related bridges elsewhere on the Trans-Pennine Route. This was particularly the case in relation to the relationship between the bridge and the nearby Toad Holes Underbridge (MDL1/12) and Ming Hill Underbridge (MDL1/14). The methodologies detailed within this CIMP have also been developed in the context and understanding of the significance the bridge derives from this group value. The group value associated with the bridge, and its contribution to the bridge's overall significance, is explored in more detail in Section 3.5 below.

## **2.2 Methodological Approach – Discharge of Condition**

- 2.2.1 For a number of listed structures subject to works under the Order, the CIMP will be submitted in a number of phases for partial discharge, to allow elements of works to commence prior to the full availability of information covering all works proposed to the structure. However, in the case of Occupation Underbridge (MDL 1/10) it is anticipated that this CIMP will contain sufficient information to fully discharge the relevant condition.
- 2.2.2 We do not anticipate any need to depart from the information provided, however, there may be unforeseen circumstances which require us to urgently revise proposals. In such circumstances Kirklees Council will be notified as soon as possible, and their agreement sought on the proposals through the established TRU Heritage Working Group.

### 3. UNDERSTANDING THE SITE

#### 3.1 Heritage Context Overview: History of the Trans-Pennine Route

3.1.1 The Trans-Pennine Route between Huddersfield and Westtown (Dewsbury) was constructed and opened between 1836 and 1849. The route today comprises sections of rail line developed by different railway companies, characteristic of the wider Trans-Pennine Route between York, Selby and Manchester. The complex chain of companies and projects is a typical product of the “Railway Mania” of the mid-1840s, the height of a period of commercial confidence and expansion in the railways<sup>8</sup>.

3.1.2 Between Huddersfield and Westtown (Dewsbury), the Trans-Pennine Route is made up of sections of:

- The Manchester & Leeds Railway, constructed 1836-39, between Ravensthorpe and Heaton Lodge;
- The Leeds, Dewsbury & Manchester Railway, constructed 1845-47, between Westtown (Dewsbury) and Ravensthorpe; and
- The Manchester & Huddersfield Railway, constructed 1846-49, between Heaton Lodge and Huddersfield.

3.1.3 The line formed a new, more direct route to the West Riding from Manchester, in competition to the earlier Manchester & Leeds Railway which had been constructed through the Calder Valley in the late 1830s. The more direct route was enabled partly through the advances in tunnel construction and large-scale engineering technology, notably realised through the construction of the 3-mile Standedge Tunnel under the Pennine watershed to connect the line between the Upper Thame and Colne Valleys. Between Huddersfield and Westtown (Dewsbury), the line is partly characterised by such examples of large scale and/or pioneering engineering structures, including tunnels, viaducts and both masonry and cast iron bridges.

3.1.4 The development and expansion of the railways and their associated infrastructure during the first half of the 19th century, was characterised by the considerable influence on those towns which experienced the development of this new mode of transport. The railways resulted in place-making and industrial growth, as towns benefited from the connections and influences which they brought with them. The Trans-Pennine Route between Huddersfield and Westtown (Dewsbury) certainly had an influence on towns, forming an additional infrastructure element of the expansion of settlements such as Dewsbury, already underway as a result of the growth of textile, mining and maltings industries.

3.1.5 Occupation Underbridge (MDL1/10) is located on the section of the Trans-Pennine Route constructed by the Leeds, Dewsbury & Manchester Railway between 1845 and 1847. This line was constructed during the Heroic Age of railway building (1841-50). Opening in stages between 1846 and 1849, when railway mania was at its height, the Leeds, Dewsbury & Manchester Railway was constructed under the oversight of the principal engineer Thomas Grainger. Grainger was one of the leading railway engineers in Scotland at this time, working on Pioneering Age (1825-41) railways such as the Monkland and Kirkintilloch Railway (1824-1826) and the Glasgow and Garnick Railway (1826-1831), which he delivered in conjunction with the engineer John Miller. He is best known in England for his work on lines including the Leeds, Dewsbury & Manchester Railway (1845-1848), the East and West Yorkshire Junction Railway (1846); and the Leeds & Thirsk Railway (1845-1852).

<sup>8</sup> Alan Baxter Associates, 2019. TransPennine Route Upgrade Route-wide Statement of Significance. 14

Grainger's work is notable for the imaginative way in which he tailored these lines to the difficult surrounding terrain and his bold masonry and iron bridge designs<sup>9</sup>.

- 3.1.6 In 1847, the Leeds, Dewsbury & Manchester Railway along with the Huddersfield and Manchester Railway were absorbed into the London and North Western Railway (LNWR), providing a more direct route from Manchester to the West Riding and enabling the LNWR to access the textile and coal industries of West Yorkshire. By 1851, the LNWR was the most prominent railway company of the period, with over 800 miles of track and was the largest joint-stock concern of its time, capitalised at £29 million<sup>10</sup>.
- 3.1.7 The history and significance of the Trans-Pennine Route is discussed at more length in the Route-Wide Statement of Significance (Alan Baxter, 2019). This was produced to characterise the overall heritage significance of the Trans-Pennine Route as a whole, and is included in Appendix 6.1 of the ES for the Scheme.

### 3.2 Historic Development of Occupation Underbridge (MDL1/10)

- 3.2.1 Occupation Underbridge (MDL1/10) is a single-span accommodation underbridge, located a short distance off Thornhill Road, approximately 1km to the south-west of the centre of Dewsbury, in Kirklees, West Yorkshire. The bridge accommodates a driveway providing access to a property located to the south-east of the railway; this private access is the only access under the bridge, which does not accommodate any public rights of way.
- 3.2.2 The underbridge was constructed between 1845 and 1847, as part of the Leeds, Dewsbury & Manchester Railway, under the oversight of engineer Thomas Grainger. The bridge forms one of a number of Grainger-engineered accommodation underbridges on the railway between the River Calder and Dewsbury Station, and was constructed to provide access under the railway between Fall Lane and the properties and fields to the south-east; originally, the domestic building to the south-east of the line comprised a pair of semi-detached cottages. Though the other underbridges on this section of the railway were engineered with similar cast iron decks and architectural detailing, Occupation Underbridge (MDL1/10) was constructed differently, with the entire construction of masonry.
- 3.2.3 Constructed of quarry-faced sandstone, the bridge survives in largely its original form, with only minor additions (see below), and currently appears in good condition. The underbridge comprises a semi-circular arch flanked by curved wing walls. The underbridge is notable for the detail of its construction, with elements of design which lift it above the purely functional, such as rusticated voussoirs and impost bands.
- 3.2.4 It appears that the underbridge has undergone few alterations since its construction. Unlike other sections of the Trans-Pennine Route, the section between Ravensthorpe and Leeds was not widened with additional tracks, and consequently the structure has never been widened or reconstructed. Historic mapping indicates that the access driveway underneath the structure has been realigned on the north-western side, with its current arrangement likely dating from the 1970s. There have been a small number of minor alterations and additions around the structure in recent years relating to its use as access to the adjacent dwelling; these include the construction of an electronic access gate on the south-eastern side of the underbridge, as well as security lights and associated cabling.

### 3.3 Heritage Context: Other Designated Heritage Assets

- 3.3.1 There are no other designated heritage assets located within the immediate vicinity of

<sup>9</sup> Alan Baxter Associates, 2017. MDL1/6 & MDL 1/8 Bridges Statement of Significance. 13.

<sup>10</sup> Alan Baxter Associates, 2017. Transpennine Route Statement of History and Significance: West of Leeds. 5.

Occupation Underbridge (MDL1/10). The closest other Listed Building is the Grade II Listed Church of St Matthew (NHLE 1313641), located approximately 100m north-west between the A644 Huddersfield Road and Cemetery Road. Three other Grade II Listed Buildings are located within 250m of the bridge: 120 Huddersfield Road (NHLE 1134695) and the Church of St Paulinus (NHLE 1134723), both located approximately 150m north of the bridge; and Toad Holes Underbridge (MDL 1/12) (NHLE 1450704), located approximately 200m further north-east along the railway line.

- 3.3.2 Of these other Listed Buildings, only Toad Holes Underbridge (MDL 1/12) has any relationship with Occupation Underbridge (MDL1/10) (see the discussion of group value below in paragraphs 3.5.7 to 3.5.9).

### 3.4 Heritage Context: Non-Designated Heritage Assets

- 3.4.1 The ES (Chapter 6 in Volumes 2i and 2ii) produced for the Scheme has identified one non-designated heritage asset located in close proximity to Occupation Underbridge (MDL1/10). This comprises the Former St Matthew's Infants School, located approximately 50m west of the bridge. The former school dates to the second half of the 19th century and consists of the former infants' school to the north-west, and girls and boys' school to the south-east. Both are constructed of stone and designed in a Gothic style.

### 3.5 Significance of Heritage Assets: Occupation Underbridge (MDL1/10)

- 3.5.1 Occupation Underbridge (MDL1/10) is of significance as a largely unaltered example of an 1840s accommodation underbridge, dating from the Heroic Age of railway building. The bridge derives significance from its association with the historic railway, and engineering of Thomas Grainger, as well as the quality of architectural expression in its design.

- 3.5.2 The Historic England list description<sup>11</sup> identifies the following elements of significance from which the structure is considered to have special interest:

- Historic interest:
  - constructed during the Heroic Age (1841-50) of railway building and a little altered example of an 1840s accommodation underbridge on what is now one of the main railway lines in northern England;
  - designed by the notable Scottish railway engineer Thomas Grainger.
- Architectural interest:
  - although a simple, small accommodation bridge, the use of rusticated voussoirs, impost bands and a moulded ashlar string course lift its design above the purely functional.

- 3.5.3 The overall significance of the bridge is largely derived from its historical value as an element of historic railway infrastructure from the period of great railway construction during the 1840s, as well as its historic association with the engineering of Grainger, and the Leeds, Dewsbury & Manchester Railway and LNWR. As noted by Historic England, the bridge dates to the Heroic Age (1841-50) of railway building, associating it with the rapid expansion of the network during the years of so-called 'railway mania'. Its association with Grainger also contributes to its significance, considering his own stature and historic importance, though, being wholly of masonry construction (with no cast iron elements), the bridge does not sit in as clear a group of similar historic Grainger structures along the route

<sup>11</sup> Historic England List Description, 'Railway underbridge MDL1/10, Thornhill Lane' . <https://historicengland.org.uk/listing/the-list/list-entry/1450702>. Accessed 11 July 2022.

(see below, 3.5.7 to 3.5.9).

- 3.5.4 The underbridge also derives some significance from the aesthetic value of its architectural quality; Grainger's approach to constructing a masonry accommodation underbridge which incorporated styled elements such as the rusticated voussoirs have the effect of elevating the architectural expression of the structure. The understanding of the quality of this design and the craftsmanship of its execution, which is evident today when approaching or passing through the bridge, contributes to its overall significance.
- 3.5.5 The significance of the underbridge is enhanced by the fact that the bridge has undergone little alteration since its construction, ensuring the continued legibility of its historic design; this is particularly the case for those elements of notable aesthetic value such as the voussoirs and wing walls. That the bridge continues to form part of the operational railway, providing access to the property to the south-east of the railway, also contributes to its significance.
- 3.5.6 Occupation Underbridge (MDL1/10) does not derive particular significance from its setting. The structure is located in a relatively secluded position, with very limited visibility from public roads or footpaths. Currently and historically the structure is only experienced by those passing underneath it along the entrance driveway to the private property. Similarly, though it has a relationship with the railway, the legibility of this cannot currently be widely understood, either from within the surrounding landscape, nor for those traveling by train over it, and this doesn't contribute to the underbridge's overall significance.

#### Group value

- 3.5.7 Masonry railway bridges across the Trans-Pennine Route to the west of Leeds have been characterised as being of local or regional interest, depending on their architectural quality, contribution to wider historical interest of the route, and their group value; it has been noted however that, as a group, they are not of substantial national interest due to their dating from a period of railway development when thousands of similar structures were erected
- 3.5.8 Occupation Underbridge (MDL1/10) is one of a number of underbridges constructed by Grainger for the Leeds, Dewsbury & Manchester Railway which share some common design language. Grainger's bridge engineering on the line more commonly revolved around the use of cast iron, as opposed to the masonry construction of Occupation Underbridge (MDL1/10). Examples of this can be seen a short distance down the line towards Dewsbury, in the similarly-styled construction of Toad Holes Underbridge (MDL 1/12) (Grade II Listed, NHLE 1450704), MDL 1/14 Ming Hill (Grade II Listed, NHLE 1451887) and George Street (MDL 1/16) (Grade II Listed, NHLE 1451888). Though Occupation Underbridge (MDL1/10) does derive some significance from group value with these structures, the most notable comparator examples on the route are Howley Mill Lane Underbridge (MDL 1/35) (Grade II Listed, NHLE 1452199) and MDL 1/39 Churwell Underbridge (MDL1/39) (Grade II Listed, NHLE 1451051). These examples, also of Grainger design, are similar masonry underbridges which incorporate the same elements of architectural expression, such as rusticated voussoirs and striking curved wingwalls.
- 3.5.9 The group value of Occupation Underbridge (MDL1/10) makes some contribution to its overall significance. The bridge comprises one element of the wider surviving group of structures associated with Grainger on the railway between Leeds and Ravensthorpe.

## 3.6 Significance of Other Designated Heritage Assets

- 3.6.1 As identified above in paragraph 3.3.1, of the four Grade II listed buildings within 250m of Occupation Underbridge (MDL 1/10) only Toad Holes Underbridge (MDL 1/12) has any relationship with Occupation Underbridge (MDL1/10).

- 3.6.2 Toad Holes Underbridge (MDL1/12) largely derives significance from its historical value as a rare surviving, albeit altered, example of a cast iron level beam bridge. Historic value is also derives from their association with the Leeds, Dewsbury & Manchester Railway (1845-7) and its railway engineer Thomas Grainger (see 3.5.8 above). The structure also possesses evidential value given its representation of 1840s engineering designs as well as subsequent strengthening techniques, sourcing of material and management of repairs, contributing to its overall significance. The underbridge also derives significance from the aesthetical value of its architectural quality and design interest, which is a characteristic feature of Grainger's railway bridges.
- 3.6.3 Although Toad Holes Underbridge (MDL1/12) continues to carry the railway and is a rare example of a surviving cast iron level beam bridge, a modern deck replacement was added in the 1990s and in the 1970s it was partially infilled in its north western approach. Such changes have diminished the bridge's authenticity and legibility of its historic use and form as well as reduced its historic significance.
- 3.6.4 A thorough assessment of the significance of Toad Holes Underbridge (MDL 1/12) can be read in the Heritage Assessment<sup>12</sup> submitted in support of the Listed Building Consent application in relation to the Scheme (application number 2021/91335).

### 3.7 Significance of Non-Designated Heritage Assets

- 3.7.1 The Former St Matthew's Infants School is considered to be of local importance, due to its historic interest as a mid-19th century school and its architectural interest as a mid-19th century gothic revival structure. The setting of the school is dominated by Huddersfield Road which, following modern widening, rises directly above the buildings and dominates its setting. Views of the buildings are now visible only from the south-west and make a limited contribution to the asset's significance. Though there is some limited inter-visibility between Occupation Underbridge (MDL1/10) and the former school, the bridge does not contribute to the setting of the non-designated asset, and neither asset derives significance from proximity to, or relationships with, the other.

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<sup>12</sup> Network Rail, 2021. *The Network Rail (Huddersfield to Westtown (Dewsbury) Improvements) Order: Toad Holes Underbridge (MDL1/12) Heritage Assessment*.

## 4. OCCUPATION UNDERBRIDGE (MDL1/10) – METHODOLOGIES

### 4.1 Summary

4.1.1 As outlined above in Section 1.2, this CIMP responds to the requirements of Condition 4 attached to the Listed Building Consent for Occupation Underbridge (MDL1/10), which states:

*No works shall commence until a Conservation Implementation Management Plan (CIMP) has been submitted to and approved in writing by the local planning authority. The approved CIMP will include as a minimum requirement contents based on the model template CIMP structure attached to this list of conditions. The CIMP will specifically include methodologies for:*

- i) fabric removal, masonry repairs, vegetation removal, repointing, metalwork repairs and application of protective paint systems as appropriate;*
- ii) the identification of historic elements of the fabric which once removed may be reused or preserved, and a strategy for their storage or reuse where appropriate;*
- iii) any improvements to the setting to sustain, enhance and better reveal the heritage asset affected;*
- iv) details of any maintenance access regime required (if any);*
- v) provision of heritage interpretation boards during construction works; and*
- vi) dissemination of “toolbox talks” to personnel involved in demolition and construction works.*

*The works must be carried out in accordance with the approved CIMP unless otherwise agreed in writing with the local planning authority.*

4.1.2 The following sections consider and outline the key principles and approaches which are to be applied in relation to the methodologies i) to vi) as identified in the Condition wording, as well as those items identified in Section 4 of the model template CIMP structure attached to the list of conditions (see Appendix B).

### 4.2 (a) Historic Building Recording and Monitoring before and after the works

#### Historic Building Recording

4.2.1 As secured by Condition 3, historic building recording will be undertaken in accordance with a WSI submitted to Kirklees Council and approved by West Yorkshire Archaeological Advisory Service (WYAAS). A separate application to Kirklees Council to discharge Condition 3 will be made following the production of the report. In accordance with the WSI, monitoring visits may also occur should WYAAS or Kirklees Council wish to visit site.

### 4.3 (b) Methodology for Fabric Removal

4.3.1 To allow the installation of required engineered fill and casting of reinforced strip footing, the existing ground beneath the arch of the bridge will be suitably prepared. Using an excavator, of an appropriate height to minimise the risk of causing damage to the structure, the ground below the overbridge will be excavated and compacted to the level required to enable the formation of the ground condition needed to support the infill (see paragraph 4.4.3 for further

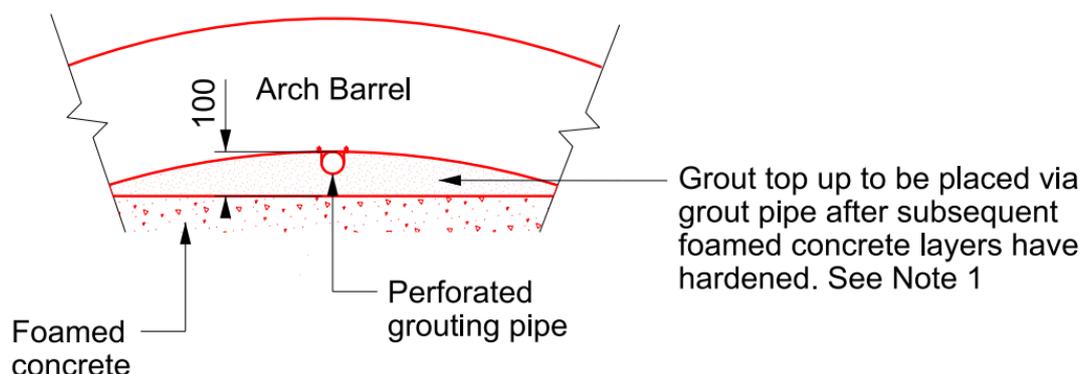
information). The existing utilities will be temporarily supported. The excavated material will be removed and suitably disposed in line with best practice guidance.

- 4.3.2 Modern gates affixed to the structure for use by the occupant of the property from which Occupation Underbridge (MDL1/10) provides access, will be removed and the bolt holes patched with mortar to match the existing. These gates are modern additions and their removal will not impact on the significance of the Listed Building.

#### 4.4 (c) Introduction of New Fabric

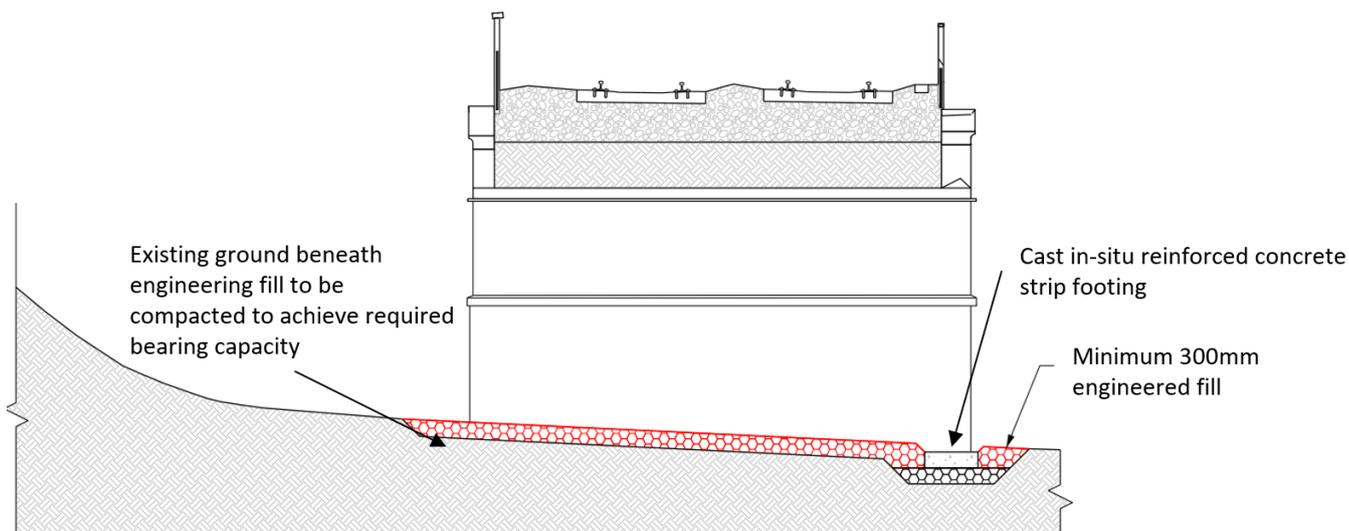
##### Design specification for infilling

- 4.4.1 Utility diversion works will take place prior to commencement of the infill works. Diverted utilities will be buried at a depth of at least 900mm below existing ground level and will therefore not be disturbed during requirements for excavation as part of the infill works.
- 4.4.2 A 50mm diameter grout pipe will be fixed to the underside of the barrel arch prior to any infilling. This perforated grout pipe will extend the width of the arch and be used to ensure the very top of the arch will be completely infilled with grout reducing the risk of voids and ensuring a complete infill is achieved (see Insert 4-1 below, described further in paragraph 4.4.5). Once the grouting has been completed the excess length of pipe will be cut back and obscured behind the masonry facing wall.



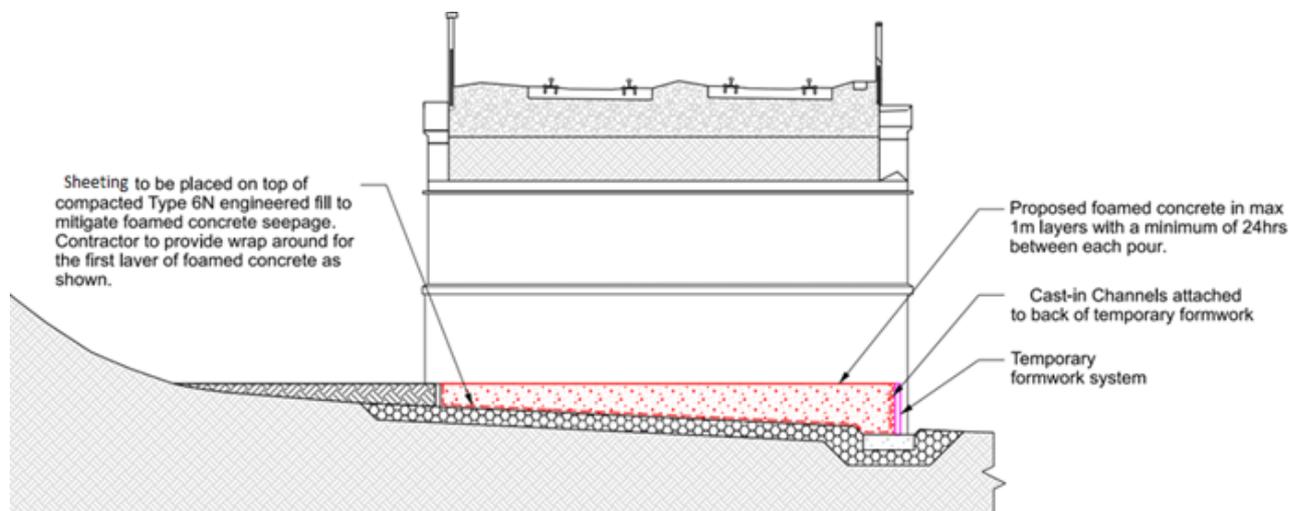
**Insert 4-1 Section showing grout pipe arrangement.**

- 4.4.3 Prior to commencement of the infilling, the ground surface underneath the arch and on its north-western approach will be suitably prepared. The ground will be excavated down to the required formation level and ground compacted to achieve a suitable bearing capacity. Soft spots will be identified and treated to ensure structural integrity of the infill is maintained. A minimum depth of 300mm of engineered fill will be installed at the eastern opening where the proposed masonry retaining wall will be situated. Within this fill, a reinforced concrete strip footing will be cast *in-situ* to support the masonry facing wall (see Insert 4-2 below). Any utilities which may clash with location of the strip footing will need to be adjusted and/or moved ahead of the casting. The voussoir will be used to set out the location of the strip footing, with a 50mm recess from its edge maintained in order to preserve legibility of the structure’s historic design. The level of the adjacent ground will be built up using a minimum of 300mm engineered fill to form a granular filter layer.



**Insert 4-2 Typical section showing formation of suitable ground conditions and cast in-situ strip footing.**

4.4.4 The infilling will commence with the formwork on either side of the arch and material carefully inserted both on the north-western side and under the arch itself. Compacted embankment fill material will be established in 1m lifts extending across to the embankment at the north-western end of the arch. A permanent PVC formwork system will be propped by this embankment fill. A temporary formwork system will be installed at the south-eastern end of the arch. Cast-in Channels will be attached to the back of the formwork to facilitate tying the masonry facing to the foamed concrete infill. To mitigate the risk of foamed concrete seepage, polythene sheeting will be laid on top of the compacted engineered fill and will wrap up the formwork for the first layer of foamed concrete (see Insert 4-3 below).

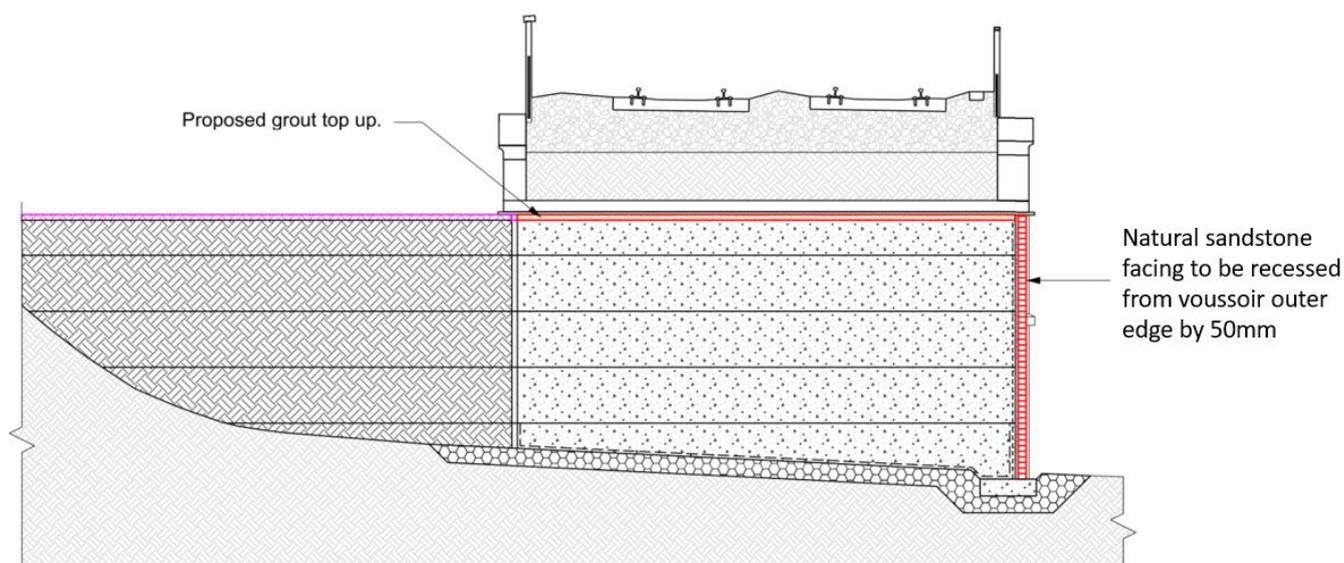


**Insert 4-3 Typical section showing formation of initial foamed concrete infill pour.**

4.4.5 The infilling will be undertaken in stages to ensure the material has time to settle and set appropriately. The compacted embankment fill material, temporary and permanent formwork and foamed concrete will be built up in 1m segments, with a minimum of 24 hours between each pour to allow sufficient time for curing. Grout poured via the previously affixed 50mm diameter pipe will be used to fill the final void beneath the arch (discussed above in paragraph 4.4.2). Once the foamed concrete is cured, the temporary formwork will be stripped off the face of the south-eastern end of the infill revealing the new concrete infill complete with cast-in channels. Where there will be an interface between the existing

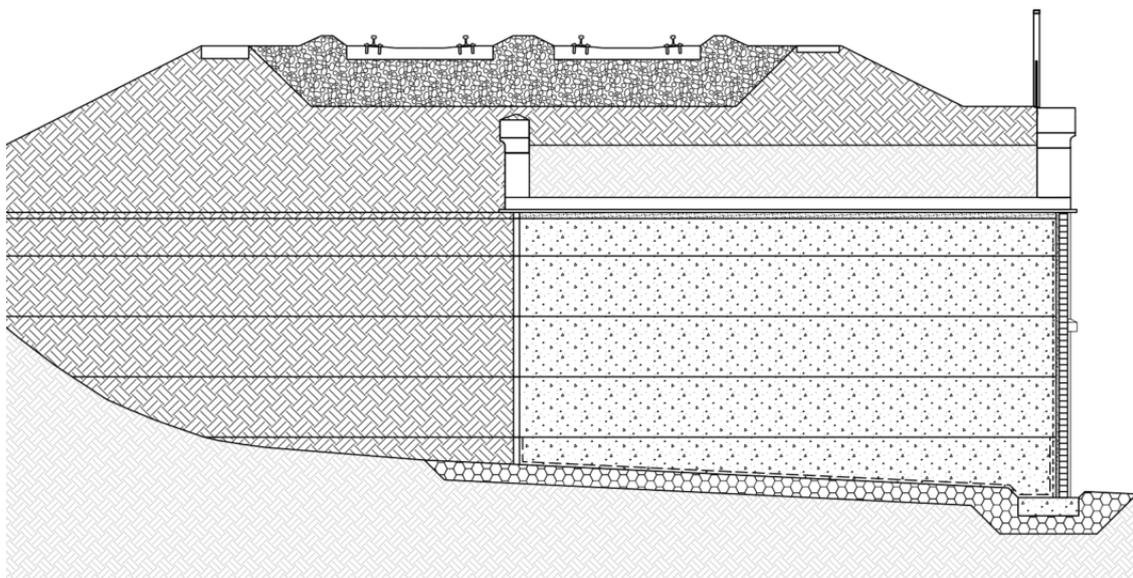
abutment and the new masonry wall, the sandstone will be scabbled to allow the flush positioning of the new wall at the junction with the existing.

- 4.4.6 The natural sandstone masonry facing wall will be built up and tied in using cast-in channels and movement ties. Once quarried, the blocks will be dressed to match the finish of the adjacent abutments. To ensure a high quality finish is achieved, the facing wall will be built by a specialist masonry stone sub-contractor, with each sandstone block manually handled into position. The cavity between the masonry wall and the foamed concrete infill will be filled using lime mortar. The natural sandstone masonry walling will be recessed at least 50mm from the face of the voussoir outer edge, to ensure the legibility of this work in the structure’s historic development.



**Insert 4-4 Typical section showing arrangement of masonry facing wall in front of completed foamed concrete infill.**

- 4.4.7 Once the infilling of the structure is complete, the alterations to the trackbed and completion of the embankment to the north-west of the infilled bridge will be undertaken. The existing tracks and ballast, as well as the north-western parapet, spandrel wall and wing walls will be removed to a minimum of 1m below the bottom of the proposed sleeper level. The reduction in height of the masonry elements to be covered by infill is required to mitigate the risk of a ‘hard spot’ remaining beneath the track bed, which could result in defects arising to the track above. The earthwork fill material will then be built up to 1m below the bottom of the proposed sleeper level. The remainder of the earthworks and track bed will then be built up, with tracks reinstated to the proposed new alignment (see Insert 4-5 below).



**Insert 4-5 Typical section showing track instated along new alignment.**

#### **4.5 (d) Repairs, Vegetation Removal etc.**

4.5.1 No other repairs have been identified as currently being required at this structure. Section 5 outlines the approach to the future regime for inspections which will form the basis of an ongoing maintenance plan. Should repairs be needed in the future, repairs will be undertaken with a matching material and undertaken following best conservation practice to maintain the special interest of Occupation Underbridge (MDL/10).

#### **4.6 (e) The identification of historic fabric which once removed may be reused or preserved, and a strategy for their storage or reuse**

4.6.1 A section of spandrel and wing wall are being reduced in height on north-eastern elevation of the bridge in order to accommodate the alignment of the new track. As further described in paragraph 4.4.7 the removal of this fabric is required in order to prevent the risk of differential track stiffness and resultant risk to passengers.

4.6.2 The masonry removed from the structure will not be reused in the works to this structure. The opportunity does, however, exist for the masonry to be used elsewhere on the scheme, should its condition allow. Following its removal, the condition of the stone will be cleaned and assessed to consider its feasibility for reuse. The findings of this assessment will be reported via the TRU Heritage Working Group. If reuse of the material is feasible, the stone will be taken to stockpile at the relevant construction compound.

#### **4.7 Improvements to the setting to sustain, enhance and better reveal the significance of the heritage assets affected**

##### Temporary Heritage Interpretation During Construction

4.7.1 The Environmental Statement (ES) submitted with the TWAO application recommended that appropriate mitigation should include the provision of temporary information panels or hoardings during the construction period which will depict the historic development of the heritage assets within their local and wider context.

4.7.2 This methodology is concerned with discharging Condition 4 in respect of:

v) any improvements to the setting to sustain, enhance and better reveal the heritage asset affected; and

viii) provision of heritage interpretation boards during construction works.

4.7.3 The wording of this CIMP condition was agreed via the Public Inquiry process regarding the Listed Building Consents and TWAO, with input from Kirklees Council and the Planning Inspector. Interpretation was included in the CIMP condition as a recognised way to capture the methodological approach to securing heritage interpretation for the scheme as a mitigation and compensation measure. This section of the CIMP responds to the condition wording and this intended purpose.

4.7.4 The heritage interpretation to be produced and installed will provide the opportunity for members of the public to engage with the heritage of the Trans-Pennine railway, better revealing the significance of Occupation Underbridge (MDL1/10).

4.7.5 The approach to meeting Condition 4 item v and viii is outlined in this methodology which covers:

- Part 1: Concept design
- Part 2: Research and content creation
- Part 3: Construction and implementation

#### Concept Design

4.7.6 At the concept design stage, a team of graphic designers worked on creating three concepts with different approaches as summarised below:

- Concept 1: Traditional Portrait Panel Layout - this concept makes use of informative bold key messaging with a playful typography which grabs attention and creates a memorable impression. Duotone photography in company branding is paired with spot illustrations to create a cohesive and engaging visual style.
- Concept 2: Modular Layout – this concept makes use of a playful annotation style applied over a traditional layout to create a concept with a dual personality which can appeal to different demographics. The complimentary palette is used to highlight key information and pairs with the annotated assets, which range from key messaging to photography and illustrations.
- Concept 3: Dynamic Layout – this concept differs from the standard panelled approach and allows for content to flow smoothly between sections. The concept includes large scale illustrations which are embedded with key messaging and information on a stone colour background to align with the surroundings, as well as allowing space for community engagement.

4.7.7 The concepts were further developed with discussions largely focused on social value and how that might be prioritised in the final design. The interpretation had to appeal to different communities and to a wide range of people, including residents, travellers, visitors, young people and the construction / work teams undertaking the TRU scheme. These concepts were thus developed with consideration of the above discussions and the need to comply with Network Rail brand guidelines and design standards.

4.7.8 Concept 2 was chosen as the agreed concept. This was due to the modular design which would enable ease of content and messaging; and the ability to section out more historical interpretative facts and information alongside sharing the TRU Programme concept and the ultimate benefits to the public from the new scheme. The concept design is aimed at meeting the social value principles of: identity, inclusion and wellbeing.

4.7.9 The Concept 2 design is shown in Insert 4-6 below:



**Insert 4-6 Chosen Concept 2 design for temporary heritage interpretation.**

4.7.10 The font and colour palette, whilst relating to the TRU Programme branding, is also designed to appeal to a wide spectrum of people and be user-friendly. It is styled to encourage engagement and discussion about the content. The design takes account of dual messaging with content aimed to accommodate different needs and perspectives. For instance, the content will be sectioned across panels and hoarding at different heights and with varying depth of detail. This is to purposefully aim particular content at:

- Children – Felix and Bolt the Huddersfield Station cats will provide a fun way to discover facts about the history of the railway,
- Visitors to the Recycling Centre – using waiting time to gain a quick insight into the works and history of the site,
- Residents / local people - who want to know more detailed information about the history of their local railway assets.

4.7.11 The benefits of this interpretive approach are:

- Increased engagement with local history and local stories.
- Increased sense of identity and community understanding of their past
- Greater perspective and appreciation of places and spaces within which people live and work.

#### Research and content creation

4.7.12 In order to produce the content for the Occupation Underbridge (MDL1/10) temporary heritage interpretation boards, a variety of documents were consulted. These include but are not limited to:

- TransPennine Route Statement of History and Significance: West of Leeds (Baxter, March 2017);
- TransPennine Route Upgrade Route-wide Statement of Significance (Baxter, August 2019); and
- The Network Rail (Huddersfield to Westtown (Dewsbury) Improvements) Order: Occupation Underbridge – Heritage Statement (Atkins, March 2021).

4.7.13 Key information was drawn out of these documents in order to produce a summary which sought to effectively communicate the historical significance of Occupation Underbridge (MDL1/10). This included:

- General scheme information and messaging;
- A history of the railway;
- A timeline on the coming of the railway;
- Information about architect Thomas Grainger;
- A history of the structure.

- 4.7.14 This information was then condensed in order to provide an effective amount of text and content on the interpretation panel. It was essential to achieve an appropriate balance between providing enough information and not over-burdening with text. It had to blend historical content and the importance of the Grade II Listed asset with messaging on the needs for the scheme and the benefits it would bring.
- 4.7.15 Each of the interpretative elements will feature general key messages about the works and the wider TRU scheme. In addition, there will be some historical background about the coming of the railway in Huddersfield and the significance of the railway assets. Each board or hoarding will then have specific information about the correlating location. This will consist of historic background, a summary of significance, information about the works being implemented and what is being done in order to protect the heritage assets during these works.

#### Construction and Implementation

- 4.7.16 The interpretative elements will not be fixed to historic fabric. Due to the access constraints at Occupation Underbridge, the interpretative elements will be located at the Weaving Lane Compound and fixed to the palisade fencing in proximity to the access route for the Recycling Centre Compound. To increase the reach of the temporary interpretation material, the opportunity to site smaller posters in Ravensthorpe and Dewsbury Stations is also being explored.
- 4.7.17 Further details will be shared once the location, sizing and materials for the interpretation elements have been agreed.

#### Changes within Setting

- 4.7.18 As outlined in paragraph 3.5.6, Occupation Underbridge (MDL1/10) does not derive particular significance from its setting. The structure is located in a relatively secluded position, with very limited visibility from public roads or footpaths. The formation of a new access track to the residential property will preserve an element of the contribution made to significance by the relationship between the bridge and the residential curtilage of the dwelling. The south-eastern elevation of the bridge will remain visible to those people accessing the land in this area via the new access track, with the design of the recessed infilling ensuring that the legibility of the historic development of the structure is maintained in views towards it within this element of its setting.

## 4.8 Toolbox Talks

- 4.8.1 Toolbox Talks were identified as a required mitigation measure which would be applied in relation to the historic environment in the ES for the Scheme<sup>13</sup>. Toolbox Talks aim to disseminate best practice guidance to the construction staff working on the Scheme, identifying the sensitivities of the historic environment with the objective of reducing impact on the historic environment as a result of construction of the Scheme.
- 4.8.2 The Toolbox Talks will outline the sensitivities of Occupation Underbridge (MDL1/10) and its environs, as well as the measures to be taken throughout the relevant construction sites to preserve the significance of the bridge, its setting, and the contribution the bridge makes to the setting of other heritage assets. The Toolbox Talks aim to address such best practice throughout the construction phase. The Toolbox Talk material for Occupation Underbridge (MDL1/10) is included as Appendix C.

<sup>13</sup> Network Rail, 2020. *Network Rail (Huddersfield to Westtown (Dewsbury) Improvements) Order application Environmental Statement, Volume 2i – Scheme-wide Assessment, Chapter 6 – Historic environment.*

- 4.8.3 Toolbox Talks will be delivered to the TRU construction contractors and teams working at Occupation Underbridge (MDL1/10). The dissemination of the Toolbox Talks will be secured via the embedding of environmental and consents leads within the TRU Alliance teams co-ordinating on-site works for the Scheme. The Toolbox Talk will be delivered as part of the site induction for all personnel working at the listed structure. A register will be kept of all personnel who have received the information contained within the Toolbox Talk, this record will be kept at the site office and will be available to Kirklees Council for inspection when requested. They will comprise written handout information sheets and/or slide pack presentations, as appropriate.
- 4.8.4 The Toolbox Talks material for Occupation Underbridge (MDL1/10) will include reference to:
- Extent of the granted Listed Building Consent and requirement to adhere to consented documentation, including relevant conditions attached
  - Relevant historic environment legislation and policy
  - Procedure for obtaining Listed Building Consent for any works outside of the granted Consent
  - Agreed methodologies and processes, including details of agreed working practices
  - Areas of particular heritage sensitivity at Occupation Underbridge (MDL1/10) and within its setting
  - Best practice guidance on avoiding accidental damage to heritage assets and minimising temporary settings impacts from construction activity
  - Reporting requirements for contacting Environmental, Consents and Heritage staff, both within the TRU project and Kirklees Council, in respect of any unforeseen circumstances
- 4.8.5 The material of all historic environment-related Toolbox Talks on the Scheme will continue to be reviewed and revised, as necessary, throughout the duration of the construction phase of the Scheme.

## 5. MAINTENANCE AND MANAGEMENT SCHEDULES TO PROTECT THE LONG-TERM CONDITION OF THE HERITAGE ASSETS AFFECTED

### 5.1 Asset Management

5.1.1 Network Rail is among the largest land and property owners in the UK, owning and managing a unique estate of railway infrastructure which is widely acknowledged as being of international significance. This estate includes circa 30,000 bridges, tunnels and viaducts to be maintained, with circa 1450 listed buildings forming part of this number. Many of these assets continue to form part of the active network subject to ongoing heavy physical wear and tear. This context prompts the two main drivers of current Network Rail maintenance policy namely: the safe management of railway structures and the long-term care and maintenance of the asset. A planned programme of routine inspection is key to satisfying both of these drivers.

5.1.2 Structures within Network Rail's estate are subject to two types of inspection. These aim to ensure that inspection of structures are both appropriately regular and detailed, in order to maintain and protect the structures' long-term condition. The two types of inspection comprise:

- Visual examinations – these are undertaken on an annual basis from ground level to check for changes to tracked defects and to identify any new areas of concern; and
- Detailed surveys – these are where every structural element is examined, photographed and its condition recorded, and are undertaken at a frequency based on the assessed risk at each specific structure.

5.1.3 Some structures with non-critical defects also require additional examination to monitor their condition until they are repaired.

#### Occupation Underbridge (MDL1/10)

5.1.4 As stated in paragraph 5.1.2 the assessed risk at each structure dictates the frequency of detailed examination. Detailed examination at Occupation Underbridge takes place every 12 years. There is the potential for this examination interval to change where the risk factors are considered to have increased or decreased. For example, at the inspection following the completion of the Trans-Pennine Railway Upgrade the inspector may determine the risk at Occupation Underbridge (MDL1/10) has decreased due to the infilling of the underbridge and recommend a change to the detailed examination interval. This decision will be made by a suitably qualified professional following the completion of the works.

## **6. IMPLEMENTATION OF THE CIMP**

### **6.1 Implementation**

- 6.1.1 On approval of this document by the relevant local planning authority it will be implemented as approved for the works undertaken as agreed by the Secretary of State for the Network Rail (Huddersfield to Westtown (Dewsbury) Improvements) Order.
- 6.1.2 As outlined above, in Section 2, this document comprises the sole submission of the CIMP for Occupation Underbridge (MDL1/10) and is intended to cover the full details of the scheme construction. The methodologies included within the CIMP, will be implemented in accordance with the detail provided, as approved by Kirklees Council, for the duration of construction works. Any necessity to depart from the methodologies due to unforeseen or emergency circumstances will be discussed and agreed with Kirklees Council via the TRU Heritage Working Group.

APPENDIX A – LOCATION PLAN



## APPENDIX B – AGREED LISTED BUILDING CONSENT CONDITIONS

**Occupation Underbridge (MDL1/10)- Application Reference 2021/91334 1. (Time Limit)** The works must be begun not later than the expiration of five years beginning with the date of this permission.

**2. (Materials)** Before the works hereby approved commence, or within a timescale to be otherwise agreed in writing by the local planning authority, samples and specifications of all materials to be used on all external elevations of the works shall be submitted to and approved in writing by the local planning authority. The works shall be constructed only using the approved materials unless otherwise agreed in writing by the local planning authority.

**3. (Historic Structures Recording)** No works shall take place until a methodology for full structure recording has been approved in writing with the local planning authority. Subsequent recording will take place prior to demolition and be deposited with the West Yorkshire Archive Service and West Yorkshire Historic Environment Record in accordance with the timescales agreed in the approved methodology.

**4. (Conservation Implementation Management Plan)** No works shall commence until a Conservation Implementation Management Plan (CIMP) has been submitted to and approved in writing by the local planning authority. The approved CIMP will include as a minimum requirement contents based on the model template CIMP structure attached to this list of conditions. The CIMP will specifically include methodologies for:

i) fabric removal, masonry repairs, vegetation removal, repointing, metalwork repairs and application of protective paint systems as appropriate;

13

ii) the identification of historic elements of the fabric which once removed may be reused or preserved, and a strategy for their storage or reuse where appropriate;

iii) any improvements to the setting to sustain, enhance and better reveal the heritage asset affected;

iv) details of any maintenance access regime required (if any);

v) provision of heritage interpretation boards during construction works; and vi) dissemination of “toolbox talks” to personnel involved in demolition and construction works.

The works must be carried out in accordance with the approved CIMP unless otherwise agreed in writing with the local planning authority.

### **SCHEDULE 7: Approved Plans**

151667-TSA-35-MVN2-DRG-T-LP-163800 Rev P02 Existing & Proposed Plan Sheet (1)

151667-TSA-35-MVN2-DRG-T-LP-163801 Rev P02 Existing & Proposed Elevation Sheet (2)

151667-TSA-35-MVN2-DRG-T-LP-163802 Rev P02 Existing & Proposed Elevation Sheet (3)

151667-TSA-35-MVN2-DRG-T-LP-163803 Rev P01 Existing & Proposed Section Sheet (4)

**Proposed Outline Contents of Conservation Implementation Management Plans (CIMPs)**

| CIMP – proposed contents:   | Details:  |
|---|---|
| <p>1. Introduction</p> <ul style="list-style-type: none"> <li>• (a) Overview</li> <li>• (b) Purpose of the Document</li> <li>• (c) Consultation Process</li> <li>• (d) Other Supporting Information [<i>TBC dependant on structure</i>]</li> </ul>  | <p>Aims to provide succinct introduction to the document, including placing it in the context of ongoing consultation processes (with Kirklees Council and/or Historic England as applicable)</p> <p>The Other Supporting Information section will outline those other documents alongside which the CIMP should be read (e.g. the Design Guide for Huddersfield Station, WSIs for Building Recording etc.)</p>   |
| <p>2. Strategic Overview</p> <ul style="list-style-type: none"> <li>• Strategic overview of the CIMP in relation to the wider TRU scheme</li> </ul>   | <p>Inclusion in each CIMP of a strategic overview of the particular works in the wider context of TRU, group value of the asset(s) and the overall Transpennine Route Upgrade</p> <p><i>This section has been included in response to a recommendation from Kirklees Council in their Objection (OBJ/33)</i></p>  |
| <p>3. Understanding the Site</p> <ul style="list-style-type: none"> <li>• (a) Heritage Context Overview: History of the Transpennine Route</li> <li>• (b) Historic Development of [<i>Structure</i>]</li> <li>• (c) Heritage Context: Other Designated Heritage Assets [<i>TBC dependant on structure</i>]</li> <li>• (d) Significance of Heritage Assets: [<i>Structure</i>]</li> <li>• (e) Significance of Other Designated Heritage Assets [<i>TBC dependant on structures</i>]</li> </ul> | <p>A succinct overview of the heritage context and significance of the assets affected by the proposed works covered by the particular CIMP. This will draw on pre-existing accepted sources, including the ES, Heritage Assessments, Statements of Significance etc. This aims to provide the context in which the methodologies which follow have been developed.</p> <p>This section will also include opportunity for identification of any associated heritage assets which will also be affected by the proposals, for example consideration of the Huddersfield Town Centre Conservation Area in the CIMPs concerning Huddersfield Station and Huddersfield Viaduct (MVL 3/92)</p> |

4. [Structure] - Methodologies

- Summary
- (a) Historic Building Recording and Monitoring before and after the works
- (b) Fabric removal
- (c) Introduction of new fabric
- (d) Repairs, vegetation removal, repointing
- (e) The identification of historically or architecturally significant elements of fabric which once removed may be reused or preserved, and a strategy for their storage or reuse [TBC]
- (f) Any improvements to the setting to sustain, enhance and better reveal the significance of the heritage assets affected
- (g) Any improvements to sustain the long-term conservation of the heritage assets affected [TBC]

*N.B. Those applicable to the asset*

This section will form the bulk of the document and will outline the specific methodologies for the individual elements of work at the heritage assets concerned. These will be supported by appendices where appropriate (e.g. for additional information, supplementary documentation)

Where appropriate, this section of the CIMP may be sub-divided according to the Stages of development; this is particularly the case where structures will undergo the proposed works over a longer period of time, such as at Huddersfield Station. Similarly, for those structures such as Huddersfield Station where there are multiple interventions over multiple elements proposed, each methodology sub-section will be divided by area, so for example (a) Historic Building Recording and Monitoring will be split into the relevant methodologies for the principal Roof A, Roofs B and C, the Tea Rooms etc.

This section of the CIMP, supported as necessary by the appendices, will provide the details of the approach to the works, and for the different relevant work areas will cover items including, but not limited to:

- Definition of the required standard of works and workmanship
- Methodologies around storage of any temporarily removed material (for example regarding the Tea Rooms at Huddersfield Station)
- Construction methods and adopted techniques
- Specification of materials, for example for new elements (such as at Huddersfield Station or Wheatley's Overbridge (MVL 3/103))
- Use of equipment
- Details of heritage interpretation

|  |  |
|--|--|
| <p>5. Maintenance and management schedules to protect the long-term condition of the heritage assets affected</p>  | <p>Agree any particular management needs and set out a timetable for regular inspection as part of NR’s management of historic assets. As with Section 4 (above) this will be supported by appendices, where relevant and applicable, such as proposed maintenance schedules.</p>  |
| <p>5. Implementation and Review of the CIMP</p> <ul style="list-style-type: none"> <li>• (a) Implementation</li> <li>• (b) Review</li> </ul>   | <p>Very succinct identification of how the document will be implemented (once approved) and any elements which will require further review (e.g. building recording etc.) and how the continual engagement and review of works with stakeholders will be planned.</p>  |
| <p>Appendices [<i>would include</i>]</p> <ul style="list-style-type: none"> <li>• Site Plan</li> <li>• Planning and Listed Building Consent conditions</li> <li>• Additional drawings and visualisations</li> <li>• Samples</li> <li>• Maintenance schedules</li> <li>• Specific method statements</li> <li>• Results of any intermediary surveys etc.</li> </ul> <p><i>N.B. Those applicable to the asset</i></p> | <p>Each CIMP would include various appendices to provide supplementary information, for example providing specific method statements around fabric removal, strengthening works or introduction of new elements.</p> <p>Each CIMP would also include as Appendices both the relevant Planning and Listed Building Consent conditions which seek to be fully or partially discharged through the contents of the CIMP</p> |

## APPENDIX C – TOOLBOX TALK OCCUPATION UNDERBRIDGE (MDL1/10)

The below text comprises the material which will be included in the Toolbox Talk regarding Occupation Underbridge (MDL1/10) and the historic environment. Note - the handout and/or presentation to accompany the Toolbox Talk will be accompanied by illustrative pictures to supplement the text below.

### Toolbox talk – Occupation Underbridge

#### **Listed Buildings**

Listed Buildings are structures considered of special architectural or historical importance which are protected by legislation. There are around 400,000 Listed Buildings in England. Listed Buildings are classified into three grades:

- Grade I Listed Buildings are of exceptional interest.
- Grade II\* Listed Buildings are particularly important buildings of more than special interest.
- Grade II Listed Buildings are of special interest warranting every effort to preserve them.

A Listed Building may not be demolished, extended, or altered without special permission from the local planning authority and the relevant central government agency. Occupation Underbridge is one of nine sites where Listed Building Consent has been applied for and granted as part of section W3 of the Transpennine Route Upgrade. Approved works to Occupation Underbridge will therefore consist of undertaking masonry repairs and infilling the structure.

#### **What?**

Occupation Underbridge is a Grade II Listed Building which was built in the 1840s and carries the railway over Fall Lane. It was originally constructed of masonry and has undergone minimal alteration since its inception. It is significant for being a largely unaltered example of a masonry underbridge to the design of Thomas Grainger and for its architectural expression using masonry.

Listed Building Consent has been approved based on the exact designs and programme of works proposed. This is legally binding and any deviation from the agreed plans without consent could result in serious penalties, including up to two years imprisonment or an unlimited fine.

The approved Listed Building Consent includes various conditions which must be adhered to. These conditions include agreed specifications for materials, methodologies for the works, and requirements for recording, toolbox talks and heritage interpretation.

Works must be undertaken in accordance with the methodologies, specifications and working practices as included in the construction pack.

#### **Why?**

**Protected heritage:** Occupation Underbridge is a Grade II Listed structure. By working on this project, you will be helping to safeguard the future of an important piece of railway history for future generations.

**Avoid prosecution:** It is illegal to make changes to a Listed Building without prior consent, any breaches can result in criminal prosecution, including up to two years imprisonment and unlimited fines.

**Avoid reputational risk:** Making changes to a Listed Building that are not approved has the potential to reflect badly on the reputation of TRU and Network Rail. Due to the high-profile nature of the works to the station and viaduct, it is likely that progress of these works will be of particular interest to stakeholders, special interest groups and the general public.

**Do**

- Be prepared to find any historical artefacts/features and report them immediately to the site manager.
- Take care when using tools and equipment close to and on Listed Buildings.
- When possible, fence off any heritage assets which are not being worked on.
- Follow the advice of an appointed archaeologist or heritage consultant.
- Contact Environmental, Consents and Heritage staff, both within the TRU project and Kirklees Council, in respect of any unforeseen circumstances.

**Don't**

- Make any changes to the Listed Building which were not included in the granted Listed Building Consent.
- Deviate from the documentation approved by Kirklees Council as part of the granted Listed Building Consent application or subsequent condition discharge processes.
- Use equipment, tools or materials which have not been approved in the Listed Building Consent.



**Network Rail**  
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