

Greave Road

Proposed Residential

Hade Edge

Design & Access Statement

On behalf of Jones Homes to provide a full planning application submission for 17no. residential dwellings.

Client Number:	0005
Project Number:	015
Author:	BLW
Date:	07.05.26

NIEMEN BLUME



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1.0 Introduction



Introduction

This statement has been prepared on behalf of Jones Homes to support the submission of a full planning application for a residential development scheme.

This document provides background information on the matters taken into account in formulating the scheme design.

1.1 Proposed Development

The application proposes 17no. new build residential dwellings, located at the site off Greave Road, Hade Edge.

The scheme includes proposed off Greave Road, private amenity and parking allocation proposals, alongside the proposed dwellings..

The proposed scheme has considered the characteristics of a well-designed place, as set out in the National Design Guide.



Hade Edge Google Maps View

2.0 The Site



2.1 Site Location

The site is located to the south of Greave Road and east of Dunford Road, within the village of Hade Edge. Hade Edge sits within the wider Huddersfield area, to the immediate south of Holmfirth, at the eastern edge of the Pennines.

The site sits to the immediate north of an existing residential scheme previously developed by Jones Homes circa 2019, which is situated around the main route of Boshaw View.



Site Location Illustration



2.2 Site Characteristics

The site is roughly square in shape, positioned to the north of existing residential dwellings along Dunford Road. The site's northern boundary sits adjacent to Greave Road. There are also existing residential dwellings to the west along Dunford Road which the proposed site sits around.

The site itself has previously been used as agricultural fields framed by Dunford Road and Greave Road, and is bound by existing dry stone walling which is very typical of the surrounding area.

The site is comprised of a number of paddocks/fields divided from one another by a series of dry stone wall boundary treatments, typical of the surrounding area. Each has either a large or small gate providing access from the surrounding highways.

2.3 Existing Routes & Access

The site has existing gated access from both Greave Road and Dunford Road into each of its larger paddocks, and gated pedestrian access to the smaller areas.



Existing Access Locations off Greave Road & Dunford Road

2.4 Existing Landscape & Topography

The existing topography of the site has a rise of approximately 4.5 metres across the site from the north-east to the south-west.

The site is sub-divided by dry stone walling into 4 smaller land parcels/paddocks, and is comprised primarily of grassland.

2.5 Existing Vegetation & Boundary Treatments

The site is bound on all sides by dry stone wall boundaries, with further divisions internally.

The site is primarily comprised of grassed paddocks/parcels and has a small number of existing mature trees to the north-west corner and to the west, sitting adjacent to the internal dry stone walls.

2.6 Constraints & Opportunities

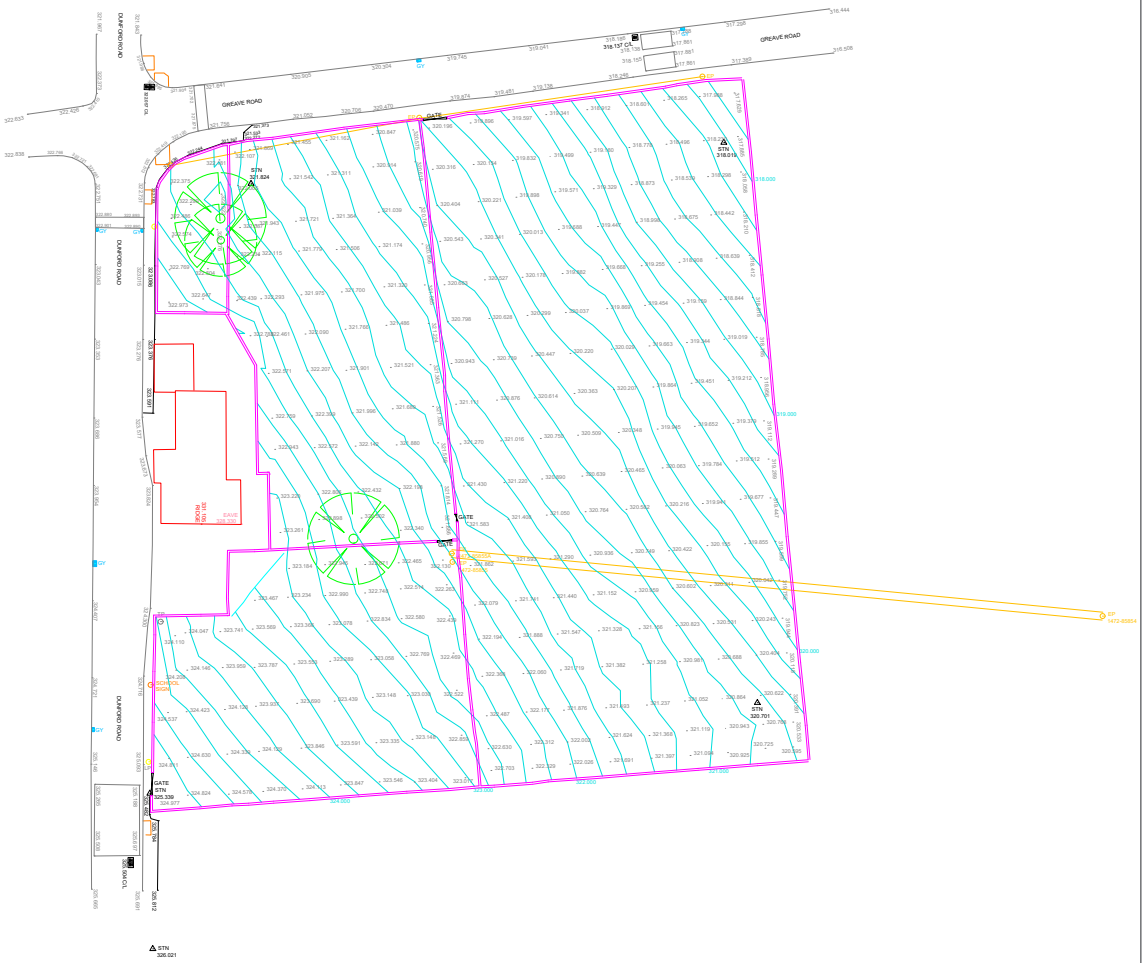
The site is subdivided by existing dry stone walls which require removal to allow for development. The site has few landscape features, however the mature trees should be considered within the design for possible retention, depending upon their condition and value.

The site as a number of direct gated access points which may prove suitable as access locations from both Dunford Road and Greave Road.

Overlooking of existing the existing dwelling to the west of the site off Dunford Road should be considered.

There are existing electric poles located to the centre of the site, and electric cabling which crosses the site from this location, and to the north-west.

Tel : 01772044741 Email: ben@rns-surveying.com	
Client	JONES HOMES
Site	HADE EDGE
Title	LAND SURVEY
Ref No	FS25012HAE
Date	21/01/2025
Scale	1 : 1
Surveyed by	BS / DS
Drawn by	BS / DS
Notes	1. The accuracy of this drawing is dependent on the original information before use of this information the user should contact P & S Surveying Services and verify the accuracy of the data and accuracy before undertaking any work. 2. All requested work has been taken in the survey detail represented on this drawing but any discrepancies must be reported immediately to P & S Surveying Services Ltd. 3. This survey provides position and description of features as identified in current planning and it is not a legal title of boundary document. 4. Current station data was used along with GPS system coordinates and units are in National Grid GCS. Current location provided by client on original survey.
Legend	 ORANGE BOLT  ORANGE FENCE  STY LEVELS  SURVEYED ROAD  SURVEYED PATH  SURVEYED FENCE  SURVEYED TACTILE PAVING  SURVEYED FENCE  CONTOUR  SURVEYED LAMP POST  SURVEYED ELECTRIC POLE
 NORTH	





2.8 Immediate Surroundings

The site is bound by Greave Road to the north and Dunford Road to the west. Traffic calming measures (road humps) have been added to the highways situated adjacent to the proposed site. A row of existing dwellings sits between Dunford Road and a portion of the western boundary. An existing field sits adjacent to the eastern boundary beyond which is Snittle Road. To the immediate south of the site is the existing residential development situated primarily around Boshaw View, which was constructed by Jones Homes circa 2019.

Existing residential development is located to the west of the site beyond Dunford Road and to the north beyond Greave Road. Dunford Road provides the main route to the centre of the village of Hade Edge.

The existing residential properties to the West of the site, facing Dunford Road, are a mix of traditional terraced cottages and more modern detached, semi-detached, and townhouse dwellings, built in a style which compliments the more historic properties. To the West along Dunford Road there is also a Methodist Church and grounds, of a similar materiality to the residential dwellings. Existing residential development to the north of the site off Greave Road is a modern mix of detached and bungalow dwellings built in traditional stone materials.

Stone/reconstituted stone is the predominant building material used in the existing properties facing Dunford Road and Greave Road, and within the settlement area in general.



Image of Greave Road



2.9 Local Area

Hade Edge is a village located at the eastern side of the Pennines within the wider Huddersfield area. It is located to the south of Huddersfield itself beyond Holmfirth. Holmfirth is located directly to the north of Hade Edge, and the village of Hepworth is located to the north-east.

The village is located near to a number of reservoir locations in the surrounds, some of which offer outdoor activities.

Hade Edge village is part of the Holme Valley parish, and part of the wider metropolitan borough of Kirklees.



Images of the Local Area



2.10 Precedents

The existing residential properties to the West of the site, facing Dunford Road, are a mix of traditional terraced cottages and more modern detached and townhouse dwellings, built in a style which compliments the more historic properties. To the West along Dunford Road there is also a Methodist Church and grounds, of a similar materiality to the residential dwellings.

Stone/reconstituted stone is the predominant building material used in the existing properties facing Dunford Road to the West of the proposed site.

Existing dwellings within the area are traditional in architectural style, reflective of the vernacular of the area and the materials used prominently within Kirklees. A large amount of the existing development within Hade Edge however has been constructed post 1990.

The existing development situated around Boshaw View provides the ideal precedent for the scheme, and was developed previously by Jones Homes. The proposed scheme has been designed to follow the visual vernacular established by this earlier development.



Local Precedents





2.11.1 Access by Road

The site is situated to the east of Dunford Road which provides a direct route north from the site access to Holmfirth, from which routes through to Huddersfield and other connections are located.

2.11.2 Access to Public Transport

Local bus services can be accessed from stops to the west situated along Dunford Road, providing services which connect the village to Holmfirth and Huddersfield. Holmfirth has a bus and coach station providing transport options nearby. The nearest train station to the site can be found in Brockholes, with a wider range of connections and routes found from Huddersfield Station.

2.11.3 Access by Air

Manchester Airport is approximately 32 miles from the site travelling by road, with a travel time of approximately an hour, whilst Leeds Bradford Airport is approximately 42 miles from the site by road, with a travel time of roughly 1 hour 10 minutes.

2.11.4 Access to Education

Hade Edge School, providing primary level education, is located to the north-western edge of the village. Further primary level education can be found within nearby Scholes, Holmbridge, and Holmfirth. Holmfirth High School, located to the north-east of the town, provides secondary education to the surrounding area.

2.11.5 Defined Retail Centres

The nearest defined retail centre can be found to the north of the site in nearby Holmfirth which has a small centre that includes a number of retail outlets, notably local small supermarket provision, amongst other retail outlets, food retail, and restaurants and services.

Additionally there are a few local farm stores and food retail outlets within Hade Edge itself serving the locality.

Larger retail parks and supermarket locations can be found further afield within Stocksbridge to the south-east and Huddersfield to the north.

2.11.6 Access to Other Facilities

There is a public house located at the northern boundary of the settlement area off Dunford Road which serves the village, along with a butchers and food hall located on Penistone Road.

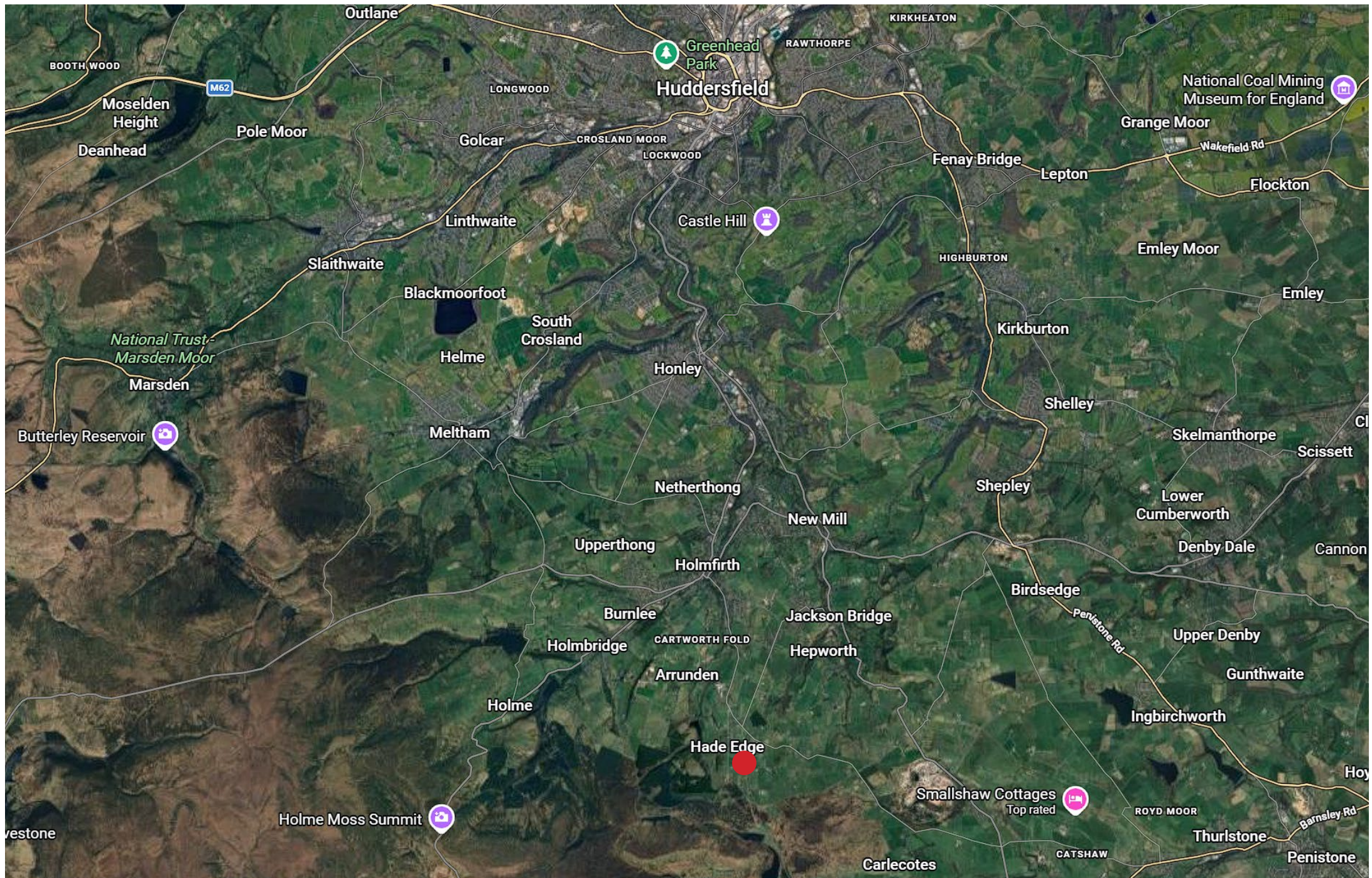
The nearest healthcare facilities can be found to the north in Holmfirth, with a local hospital, a number of GP practices, and a pharmacy all available. Further hospital services can be found in Huddersfield. There are also a number of health services and clinics located within Holmfirth.

The site is ideally located for access to the countryside within the surrounding area. The Huddersfield Angling Association and Huddersfield Sailing Club are located just to the north-east of the village at Boshaw Whams Reservoir. Hade Edge AFC is located to the east of the village off Snittle Road, and provides a local club and sports pitch to the village area. Hiking trails are located to the south of Hade Edge located art Winscar Reservoir.

Holmfirth also provides further leisure provision for the area, with a live music venue, library, and various restaurants and cafés located within the town centre.



The Site: Accessibility



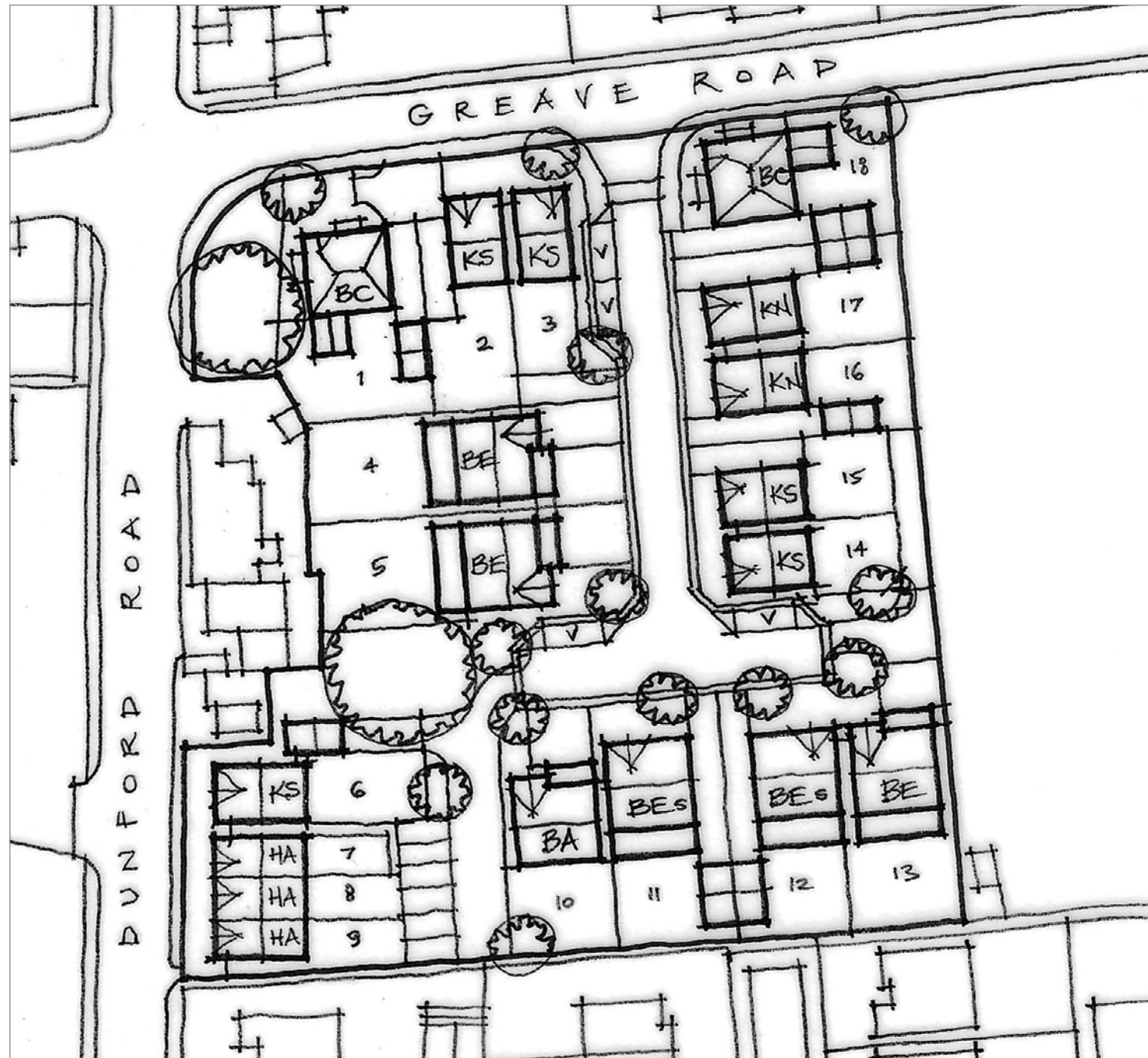
3.0 Scheme Design

The project brief by our client Jones Homes was to provide a scheme which utilises the available space and provides an appropriate level of accommodation.

The proposed scheme was to consider the opportunities and constraints of the site, and ensure that any scheme was designed cohesively to sit alongside the existing Jones Homes development immediately south of the proposed site.

The proposed scheme has developed from a number of concept sketch layouts which have led to the scheme as proposed.

The scheme has been taken from the initial concept design stage through to the proposal that is the subject of this application. The scheme was presented for public consultation during its development, and has taken the comments received into consideration when formulating the final proposal.



Initial Concept Sketch



3.3 Proposed Site Layout

The proposed scheme is comprised of 17no. residential dwellings in a varied range of house types. The mix of dwellings has been carefully considered to reflect the vernacular of surrounding development, and is primarily comprised of detached types, with a smaller number of terraced dwellings to the south-west of the site. The overall scheme has been designed to sit comfortably alongside existing residential dwellings to the north and south, and follows a similar format to the development around Boshaw View, ensuring that the proposed and existing sit together cohesively.

The scheme is proposed to be accessed off Greave Road and not to connect to the existing development to the south, so as to avoid creating areas of increased vehicular speed through the area. The scheme sits around a primarily private access surface, which devolves immediately from the proposed access, branching into a T shaped route. A small number of dwellings are proposed in a back to front orientation, utilising Dunford Road to provide pedestrian access, and proposing rear access to parking allocations via a small cul-de-sac within the scheme.

Dwellings within the proposal are oriented towards the access route, creating a devolving micro-neighbourhood, and rear access to the dwellings situated towards Dunford Road.

Dwellings are generally positioned with an open frontage of defensible space, with primarily front loaded parking allocations. This increases the offset of the property line from the access road. Each dwelling is proposed with a secure rear garden amenity, which generally backs onto the site boundaries.

Particular care has been taken with the positioning of dwellings adjacent to the southern and western boundaries, so as to ensure the privacy of neighbouring existing dwellings is maintained.



Illustrative Proposed Site Layout



3.4 Built Form

The proposed layout makes effective use of the site and responds appropriately to its surroundings.

The development will continue the existing street scene of Dunford Road, infilling currently unoccupied space, making best use of the sites potential density.

The impact of the proposed built form on the existing development to the south is lessened by the offset distances provided between the new and the existing built forms created by the back to back positioning of the rear garden amenities existing and proposed along the southern boundary. The scheme itself has carefully considered the existing development and has been designed to sit cohesively alongside it, with a similar materials palette and aesthetic of house type chosen in line with both the existing scheme and the wider area as a whole.

This layout and urban grain aligns with the character of the area, and in particular the adjacent development and the existing street scene located along Dunford Road, whilst meeting the necessary standards on residential amenity, road widths, vehicle turning, parking, private garden space and landscaping.

Buildings in Hade Edge are typically low rise, consisting generally of 2 or 2.5 storeys with pitched roofing. The scheme proposes a maximum height of 2 storeys throughout, with a similar scale and volume of house type proposed in line with the existing development to the south. The built form employed by the scheme has been carefully considered and how it will comfortably sit within the existing environment and local vernacular of Hade Edge.



@ 1:100



@ 1:100



@ 1:100



3.5 Proposed Access

Vehicular and pedestrian access into the site is proposed off Greave Road. There will also be direct vehicular access to two plots with driveways accessed directly off Greave Road. Three plots will have pedestrian access off Dunford Road, with vehicular access provided within the site in a back to front layout to the south-west corner of the proposed scheme.

3.6 Movement

The vehicle and pedestrian access point is located towards the middle of the northern boundary with Greave Road. This will lead directly into a branched devolved access route, divided by a T shaped junction to the southern half of the site, devolving into micro-neighbourhoods. The layout will encourage low vehicle speeds and improve the road safety within the development through the use of junctions strategically positioned within the branched route proposed. The scheme also proposes direct driveway access to two plots from Greave Road, further reducing vehicular activity within the site. The site will not connect to the existing site to the south so as to avoid the creation of a vehicular rat run through the proposed and existing residential development.



Proposed Site Layout Access Detail



3.7 Access Into Dwellings

Gradients leading to dwellings will be designed in accordance with Part M of the Building Regulations and all plots include an allowance for a minimum 900 x 1200mm level access to the main entrance.

3.8 Design & Visibility Splays

The proposed scheme seeks to create vehicular and pedestrian access into the site off Greave Road, with the proposed vehicular entrance located to provide the best visibility internally and externally from the site access.

Vehicular turning facilities have been afforded where necessary using private access roads and driveways/parking allocations within individual plot curtilages.

3.9 Parking Provision

Parking provision has been allocated at predominantly 2 spaces per dwelling or where feasible, for the larger dwellings 2 spaces plus integral/detached garage space have been provided.

Parking spaces will be generally located to the property frontage in an accessible naturally surveilled location, emulating the layout of the existing development to the north. An EV charging plan is provided as part of the application drawings pack outlining the proposals for EV charging location details..

3.10 Amenity

The proposed layout seeks to maintain the private amenity of neighbouring properties with particular care given to the existing development located to the immediate south of the site, and to the existing dwellings located along Dunford Road to the west. Dwellings have been positioned so that existing and proposed rear gardens abut one another so as to increase the offset distances between dwellings where possible.

Plots and proposed dwellings throughout been carefully positioned to maintain the private amenity of proposed homes with the offset between dwellings considered within the scheme design. A range of garden sizes which meet requirements are included per dwelling.

View lines and window positioning have been considered within each house type. This

ensures that overlooking is avoided within the scheme between neighbouring homes. Offset distances at key locations have been proposed to ensure the private amenity of dwellings is achieved both internally and with existing properties within the adjacent development and along Dunford Road.

Private amenity within the site is provided through secure rear gardens bound by proposed 1.8m timber fencing.

3.11 Private & Public Spaces

Solid design methodologies have been adopted concerning the design of public and private space. The proposed scheme is positioned around a road network which is designed to adoptable highways standards. The proposed access from Greave Road devolves into shared access routes and finally private parking allocations and driveways within plot curtilages. Dwellings are proposed with open frontage of defensible space throughout the scheme internally. Frontages situated along either Dunford Road or Greave Road utilise the existing dry stone wall boundary treatment. The private garden amenity to the rear of properties is clearly defined by timber fenced proposed boundary treatments. The boundary treatment plan provides details of this, and is included both within the application, and on the following page of this document. Due to the small size of the site and the ease of access to the surrounding countryside public open spaces have not been proposed as part of the scheme. The village itself has a good amount of public open space, including playing fields and an equipped play area, all of which are a short distance from the proposed site.

3.12 Crime & Prevention (Crime Impact Statement)

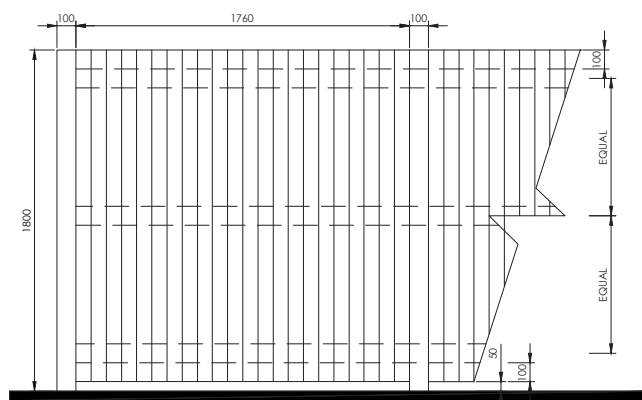
Prevention of crime has been taken into consideration in the design of the layout of the proposed site. The open plot frontages and sight lines to the adjacent highways or shared access work in conjunction to provide clear and defensible spaces alongside natural surveillance from proposed dwellings. The security of the rear gardens is provided using fencing to create a barrier between properties and the site boundary where applicable. Integral garages within a number of house types provide further secure storage for plots within the scheme.



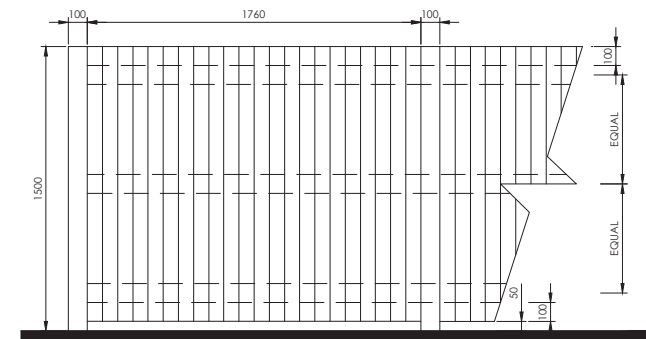
3.13 Proposed Boundary Treatments

A range of boundary treatments are proposed within the scheme. The boundary treatments proposed are two types of timber fencing, with the details shown to within the boundary treatment plan and boundary treatment details drawings.

Rear garden amenity is secured by a combination of 1.8 metre and 850mm timber fencing.



SF10 FEATHER EDGE BOARDED TIMBER SCREEN @ 1:20
100x100mm TIMBER POSTS WITH 100x37mm SW RUNNERS (2NO.) AND 100x25mm THICK FEATHER EDGE SAWN BOARDS WITH 20mm LAP TO BOARDS (TIMBER TREATMENT-ALL TIMBERS TO RECEIVE PRESERVATIVE STAIN FINISH AS NOTE). BOTTOM EDGE OF BOARDS TO BE SET 50mm CLEAR OF FINISHED GROUND LEVEL. ALL FIXINGS TO BE GALVANISED MILD TIMBER POSTS TO BE TAKEN DOWN 600mm MINIMUM AND ENCASED CENTRALLY IN 450x450x600mm DEEP CONCRETE LAID 100mm BELOW GROUND LEVEL. ALL FENCING TO BE SITE ASSEMBLED.



SF7 FEATHER EDGE BOARDED TIMBER SCREEN @ 1:20
100x100mm TIMBER POSTS WITH 100x37mm SW RUNNERS (2NO.) AND 100x25mm THICK FEATHER EDGE SAWN BOARDS WITH 20mm LAP TO BOARDS (TIMBER TREATMENT-ALL TIMBERS TO RECEIVE PRESERVATIVE STAIN FINISH AS NOTE). BOTTOM EDGE OF BOARDS TO BE SET 50mm CLEAR OF FINISHED GROUND LEVEL. ALL FIXINGS TO BE GALVANISED MILD TIMBER POSTS TO BE TAKEN DOWN 600mm MINIMUM AND ENCASED CENTRALLY IN 450x450x600mm DEEP CONCRETE LAID 100mm BELOW GROUND LEVEL. ALL FENCING TO BE SITE ASSEMBLED.

Proposed Boundary Treatment Plan & Boundary Treatment Details N.T.S.



3.14 Proposed Materiality

The predominant surface finish of the surrounding area is one of stone, and artificial stone, with a small amount of traditional brickwork. Hade Edge presents a consistent mix of roof finishes, the majority of which are grey slate.

Materials have been proposed to compliment and reflect the existing vernacular within the surrounding area, and in particular the existing residential area to the south, situated around Boshaw View, which was previously developed by Jones Homes.

The materials palette chosen is proposed to sit alongside and complement this existing development.

The quality of the development will be enhanced by the choice of stone, which will sit comfortably within the existing surroundings, reflecting the architectural vernacular of the surrounding area.

The plan to the right details the materials placement within the proposed scheme. There is a single materials palette proposed for the site, a weathered stone to the elevations, and a grey slate roof tile, both in keeping with the surrounding area. Elevational variety will be provided through differing front and garage door colour choices. Full detailing can be seen within the materials plan to the right, which is included within the planning application.



Proposed Materials Plan N.T.S.



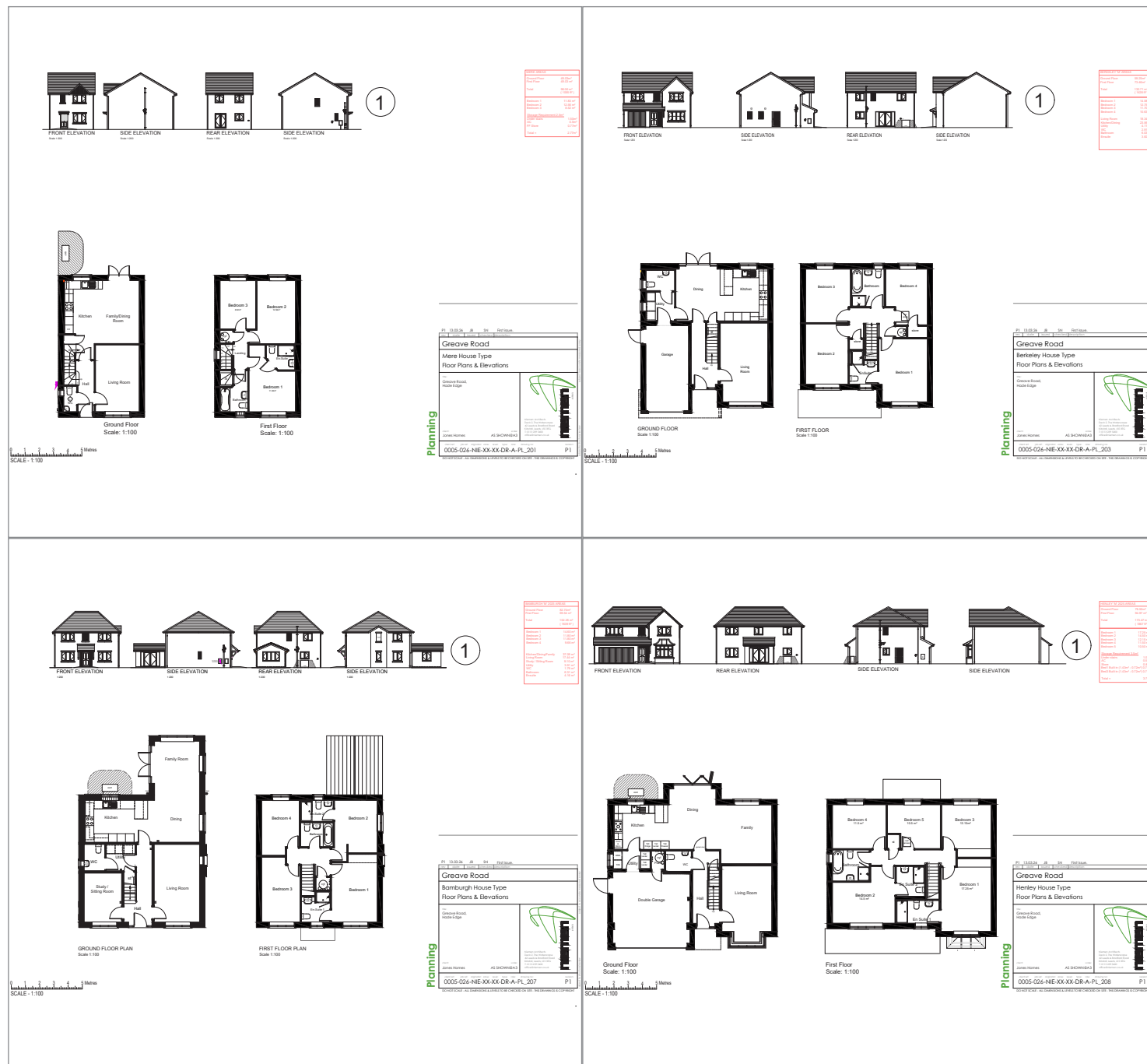
3.15 Housing Mix

The housing mix provides a range of detached dwellings, and a small run of three terraced units. The scheme proposes primarily detached dwellings in line with the existing vernacular of Hade Edge, so as to increase the housing stock. The terrace proposed fits with the existing Dunford Road, completing the street scene.

3.16 Proposed House Types

An example of the proposed house types can be seen to the right, with the full pack of proposed elevations and floor plans provided within the application. The house types proposed provide varied levels of accommodation with dwellings proposed containing between 2 and 5 bedrooms within the scheme.

House types have been carefully chosen in line with the existing types used within the adjacent development. A number of types proposed appear within the existing scheme, with further types proposed so as to expand the scope of the available house types, and to complement the existing development and its character, and to reflect local market conditions



Proposed House Type Drawings N.T.S.



3.17 Proposed Elevations

Careful consideration has been given to the arrangement of the various house types within the scheme to ensure visual diversity and interest is achieved within the proposed street scape. The variation created within the roof scape is key to producing visually varied distances between the different dwellings, creating the impression of added spacing within the development.

All proposed elevations consist of two storey detached dwellings with pitched roof lines in keeping with the existing theme within the immediate locality, and emulating the existing aesthetic of the development to the south of the site. The stone walling to the elevations reflects the existing visual language seen within Hade Edge and the surrounding area, ensuring the development sits comfortably within the surrounding landscape.

The proposed elevations therefore complement and enhance much of the surrounding area in terms of form, scale and materiality, with proposed additional detailing to further enhance the proposed street-scape.



4.0 Conclusions



Conclusions

4.1 Summary

The proposed development consists of 17 dwellings on former agricultural land off Greave Road in Hade Edge. The site lies to the centre of the existing settlement area, infilling an unoccupied section of Dunford Road and Greave Road. The land has been safeguarded for residential development. As such, the proposals are an appropriate use.

This development will provide a positive addition to the street-scenes of Dunford Road and Greave Road. The proposed density reflects the rural village location, expanding the housing provision within the area, and sitting comfortably alongside existing development to the south.

The homes will positively contribute towards the character of the area. They will be constructed in a traditional style, with high-quality building materials which reflect the local vernacular, providing variety and visual interest.

The form of development reflects its surroundings, with scale extending no more than 2 stories in height. Rear gardens will be provided throughout, with front gardens of open defensible frontage.

In summary, the proposals perform well against the 10 characteristics of a well-designed place, as described in the National Design Guide.

5.0 Appendices



1m 10m 20m 30m 40m 50m



Planning

P2 28.04.26 JB SN Red edge adjusted.
P1 26.03.26 JB SN First issue.

rev	date	issued	checked	description
Greave Road				
Site Location Plan				
title: Greave Road, Hade Edge				
client: Jones Homes			scale: 1:1250@A3	revision:
client ref:	job ref:	original ref:	date:	level:
0005-026-NIE-XX-XX-DR-A-PL_000			drawing no: P2	

ALL DIMENSIONS & LEVELS TO BE CHECKED ON SITE - THIS DRAWING IS COPYRIGHT

subject to accurate measured survey



1m 10m 20m 30m 40m 50m

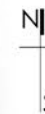
Play Area

Bandroom

TCR

Pump

Greengages



LEGEND:

- SF10 - 1800mm(N) FEATHER EDGED TIMBER FENCE
- SF7 - 850mm PLOT DEFINITION FENCE
- SF12 - 450mm TIMBER KNEE RAIL
- SF13 - 2000mm(N) MASONRY SCREEN WALL
- TIMBER GATE
- WHEELIE BINS
- BN COLLECTION POINT
- HARD STANDINGS (2.4x2.4m)
- TREES TO BE RETAINED
- ROOT PROTECTION AREA

- P14 26.04.26 JB SN Red edge adjusted to include footpath improvement to Greave Road.
- P13 15.04.26 JB SN Private drive adjusted to plots 4-9. Plot 10 position adjusted.
- P12 02.02.26 JB SN Garden room removed from Henley type. Schedule of accommodation updated. Bin store locations added.
- P11 28.01.26 JB SN Plot 10 position adjusted. Plot 13 handed and VP space position amended.
- P10 26.01.26 JB SN Minor adjustments to plot positions for plots 10, 11, 14 & 17. VP space position amended. Parking/private drive for plots 5-9 adjusted. BCP position added.
- P9 07.01.24 JB SN Plot 2 position adjusted and garage amended. additional parking added to plot 5 and VP space location amended.
- P8 16.10.25 JB SN Garden room added to Henley house type. Schedule of accommodation soft updated to all types.
- P7 01.06.25 JB SN House types amended to 2025 cribs.
- P6 09.06.25 JB SN Plot 17 type amended. Schedule of accommodation updated.
- P5 02.05.25 JB SN Plots 4, 5 & 13 handed. Plots 10 & 11 moved west to allow for retaining wall. Parking area serving plots 6-9 adjusted to suit.
- P4 17.03.25 JB SN Private drive and associated parking amended serving plots 5-9.
- P3 05.03.25 JB SN Existing TPO'd trees added. Plots 1-3 positions and orientation amended. Plot 5 type swapped. Plot 5 & 6 parking amended.
- P2 27.02.25 JB SN Visibility signs added. Minor adjustments to visitor parking. Private drive serving plots 1-3 adjusted slightly. Parking to rear of plots 7-9 adjusted. Plot 7 base.

Plot	Area	Notes	Description
Greave Road			
Proposed Site Layout (Site B)			
Greave Road, Hade Edge			
Client: Jones Homes		Scale: 1:500@A2	Project: P14
0005-026-NIE-XX-DR-A-PL_005		P14	

DO NOT SCALE - ALL DIMENSIONS & LEVELS TO BE CHECKED ON SITE - THIS DRAWING IS COPYRIGHT

Schedule of Accommodation									
To be used in conjunction with drawing no. 0002-026-NIE-XX-DR-A-PL_005-P12									
Rd	Name	Storeys	Beds	Formal	FP	M ²	No.	%	Total M ²
GMS									
ALDER	Alder	2	2	Semi-detached		733.00	49	54	3
DEL	Delamere	2	3	Detached		1110.00	103	12	1
MERE	Mere	2	3	Detached		1017.00	94	48	2
MEIK	Meikley	2	4	Detached		1362.00	128	45	2
TRICK	Thornbury	2	4	Detached		1473.00	136	40	1
DUR	Durham	2	4	Detached		1768.00	164	35	4
LEED	Leedon	2	4	Detached		1476.00	135	18	2
BAN	Barnborough	2	4	Detached		1639.00	152	27	1
HEN	Henley	2	5	Detached		1867.00	173	45	1
Total							100%		