



TRANSPORT STATEMENT
PARKWOOD ROAD, GOLCAR
MANDALE
NOVEMBER 2025

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1. INTRODUCTION

INTRODUCTION

- 1.1 TPS Transport Consultants Ltd. (TPS) has been appointed by Mandale to prepare a Transport Statement to support a planning application for residential development of 28 dwellings on land to the south of Parkwood Road, Golcar.

SITE LOCATION AND DEVELOPMENT PROPOSALS

- 1.2 The site is located approximately 5.0km west of Huddersfield town centre and is bound by Parkwood Road to the north, residential dwellings to the west and south and Quarry Bank Mill to the east. The site location is shown in **Figure 1.1**, whilst the proposed site layout is provided at **Appendix A**.

Figure 1.1: Site Location



(Source: Google Earth)

- 1.3 The development proposals comprise 28 dwellings, made up of 1-4 bed properties. Vehicular access to the site will be taken from an extension of Weavers Lane, on the western boundary, with pedestrian links provided to the north and south of the site.

PLANNING HISTORY

- 1.4 The site has previously been granted consent for 27 dwellings in 2021 and 2022 (2019/92164 and 2020/92118 respectively) establishing the principle for a residential development of this scale on the site.

REPORT STRUCTURE

- 1.5 Following this introductory section:

Section 2 describes the transport planning policy context within which the proposals will be assessed;

Section 3 details the accessibility of the development site, focusing on the means by which residents could access the site by non-car modes of travel;

Section 4 describes the existing highway network in the vicinity of the development and key routes to the site, with reference to historic road safety records;

Section 5 summarises the current and proposed development operation and the impact upon trip generation;

Section 6 considers parking and servicing arrangements; and

Section 7 offers a summary and conclusions.

2. POLICY CONTEXT

INTRODUCTION

- 2.1 This section of the Transport Statement identifies the policy context within which the development proposals have been assessed; it clearly demonstrates how the proposed development would contribute to the overarching principles of national and local transport policy.

NATIONAL POLICY CONTEXT

Government White Paper: Creating Growth, Cutting Carbon – Making Sustainable Local Transport Happen (Dft, 2011).

- 2.2 In the Integrated Transport White Paper, a wide range of measures to deal with congestion and pollution are set out. The White Paper highlights the need for action at both a national level as well as within a local context.
- 2.3 In addition, the White Paper identifies the costs of transport – in particular, the issues surrounding delay, pollution, health problems and accidents all caused by local congestion. Recognising this, it states that access to sustainable travel modes and improving accessibility can:

“Make a significant contribution to public health and quality of life”.

- 2.4 In terms of public transport improvements, the White Paper stresses the need to make public transport more attractive so that it provides a viable alternative to car journeys, especially for trips of less than five miles.
- 2.5 The White Paper also outlines the need for positive influences upon travel behaviour, including ‘nudge’ theory, which is concerned with soft promotion of public transport and identifying targets (people) susceptible to changes in travel behaviour.

National Planning Policy Framework (NPPF – DCLG, December 2024)

- 2.6 The revised National Planning Policy Framework was published in December 2024 and sets out the government's planning policies for England and how these are expected to be applied. It continues to encourage development through the planning system, with a presumption in favour of sustainable development. Paragraph 109 states that “Transport issues should be considered from the earliest stages of plan-making and development proposals, using a vision-led approach to identify transport solutions that deliver well-designed, sustainable and popular places. This should involve:

- a) Making transport considerations an important part of early engagement with local communities;
- b) Ensuring patterns of movement, streets, parking and other transport considerations are integral to the design of schemes, and contribute to making high quality places;
- c) Understanding and addressing the potential impacts of development on transport networks; and
- d) Realising opportunities from existing or proposed transport infrastructure, and changing transport technology and usage – for example in relation to the scale, location or density of development that can be accommodated;
- e) Identifying and pursuing opportunities to promote walking, cycling and public transport use; and
- f) Identifying, assessing and taking into account the environmental impacts of traffic and transport infrastructure – including appropriate opportunities for avoiding and mitigating any adverse effects, and for net environmental gains."

2.7 Paragraph 115 highlights that "in assessing sites that may be allocated for development in plans, or specific applications for development, it should be ensured that:

- a) Sustainable transport modes are priorities taking account of the vision for the site, the type of development and its location;
- b) Safe and suitable access to the site can be achieved for all users;
- c) The design of streets, parking areas, other transport elements and the content of associated standards reflects current national guidance, including the National Design Guide and the National Model Design Code; and
- d) Any significant impacts from the development on the transport network (in terms of capacity and congestions), or on highway safety, can be cost effectively mitigated to an acceptable degree through a vision-led approach."

2.8 Paragraph 116 states that: "*Development should only be prevented or refused on highways grounds if there would be an unacceptable impact on highway safety, or the residual cumulative impacts on the road network, following mitigation, would be severe, taking into account all reasonable future scenarios.*"

2.9 Paragraph 117 sets out that applications for development should:

- a) Give priority first to pedestrian and cycle movements, both within the scheme and with neighbouring areas; and second – so far as possible – to facilitating access to high quality public transport, with layouts that maximise the catchment area for bus or other public transport services, and appropriate facilities that encourage public transport use;
- b) Address the needs of people with disabilities and reduced mobility in relation to all modes of transport;
- c) Create places that are safe, secure and attractive – which minimise the scope for conflicts between pedestrians, cyclists and vehicles, avoid unnecessary street clutter, and respond to local character and design standards;
- d) Allow for the efficient delivery of goods, and access by service and emergency vehicles; and
- e) Be designed to enable charging of plug-in and other ultra-low emission vehicles in safe, accessible and convenient locations.

2.10 Paragraph 118 suggests that *“all developments that will generate significant amounts of movement should be required to provide a travel plan, and the application should be supported by a vision-led transport statement or transport assessment so that the likely impacts of the proposal can be assessed and monitored.”*

LOCAL POLICY CONTEXT

Kirklees Local Plan (February 2019)

2.11 The Kirklees Local Plan is the statutory development plan for the Kirklees borough. It sets out the vision and overall development strategy for the Council's area and the ways in which it will be achieved up to 2031.

2.12 The Local Plan includes a number of strategic policies which set out the overall pattern, scale and quality of development to deliver the development principles and spatial development strategy for the district. The following policies are of relevance to the development proposals:

- **Policy LP1: Presumption in favour of Sustainable Development** - When considering development proposals, the council will take a positive approach that reflects the presumption in favour of sustainable development contained in the National Planning Policy Framework. The council will always work pro-actively with applicants jointly to find solutions which mean that proposals can be approved wherever

possible, and to secure development that improves the economic, social and environmental conditions in the area.

- **Policy LP20: Sustainable Travel** - New development will be located in accordance with the spatial development strategy to ensure the need to travel is reduced and that essential travel needs can be met by forms of sustainable transport other than the private car. The council will support development proposals that can be served by alternative modes of transport such as public transport, cycling and walking and in the case of new residential development is located close to local facilities or incorporates opportunities for day-to-day activities on site and will accept that variations in opportunity for this will vary between larger and smaller settlements in the area.

The council will support demand management measures which discourage single occupancy car travel within new development and encourage the use of low emission vehicles to improve areas with low levels of air quality. Proposals should include measures to encourage the use of sustainable travel options, including public transport, the promotion of personal journey planning, walking, cycling, car sharing, electronic communication and home working.

- **Policy LP21: Highways and Access** - Proposals shall demonstrate that they can accommodate sustainable modes of transport and be accessed effectively and safely by all users.

New development will normally be permitted where safe and suitable access to the site can be achieved for all people and where the residual cumulative impacts of development are not severe.

Proposals shall demonstrate adequate information and mitigation measures to avoid a detrimental impact on highway safety and the local highway network. Proposals shall also consider any impacts on the Strategic Road Network. All proposals shall:

- a. ensure the safe and efficient flow of traffic within the development and on the surrounding highway network;
- b. where needed, provide new infrastructure or improvements on or off site to ensure safe access from the highway network for pedestrians, cyclists, public transport users and private vehicles;
- c. be accompanied by a supporting Transport Assessment or Transport Statement where the development would generate significant trip generation, providing detail as to the impact on highway safety, air quality, noise and light restrictions;

- d. take into account changes in site levels and topography to ensure the development can be accessed easily and safely by all sections of the community and by different modes of transport;
- e. take into account the features of surrounding roads and footpaths and provide adequate layout and visibility to allow the development to be accessed safely;
- f. take into account access for emergency, service and refuse collection vehicles;
- g. provide on-site safe, secure and convenient cycle parking/storage facilities to encourage sustainable travel modes.

SUMMARY

- 2.13 This Transport Statement will demonstrate that the development proposals take full advantage of existing facilities for sustainable travel, locally, and will not result in a significant traffic impact on the local road network, therefore, satisfying the requirements of NPPF and local planning policy.

3. ACCESSIBILITY

INTRODUCTION

3.1 This section of the Transport Statement describes the existing infrastructure that will facilitate and encourage trips to the site by foot, bicycle or public transport, rather than by car.

ACTIVE TRAVEL OPTIONS

Pedestrian Access

3.2 Guidance provided by the Institute for Highways and Transportation (IHT) on the preferred maximum walking distance by journey type is provided in **Table 3.1**, below; for commuting, this is two kilometres.

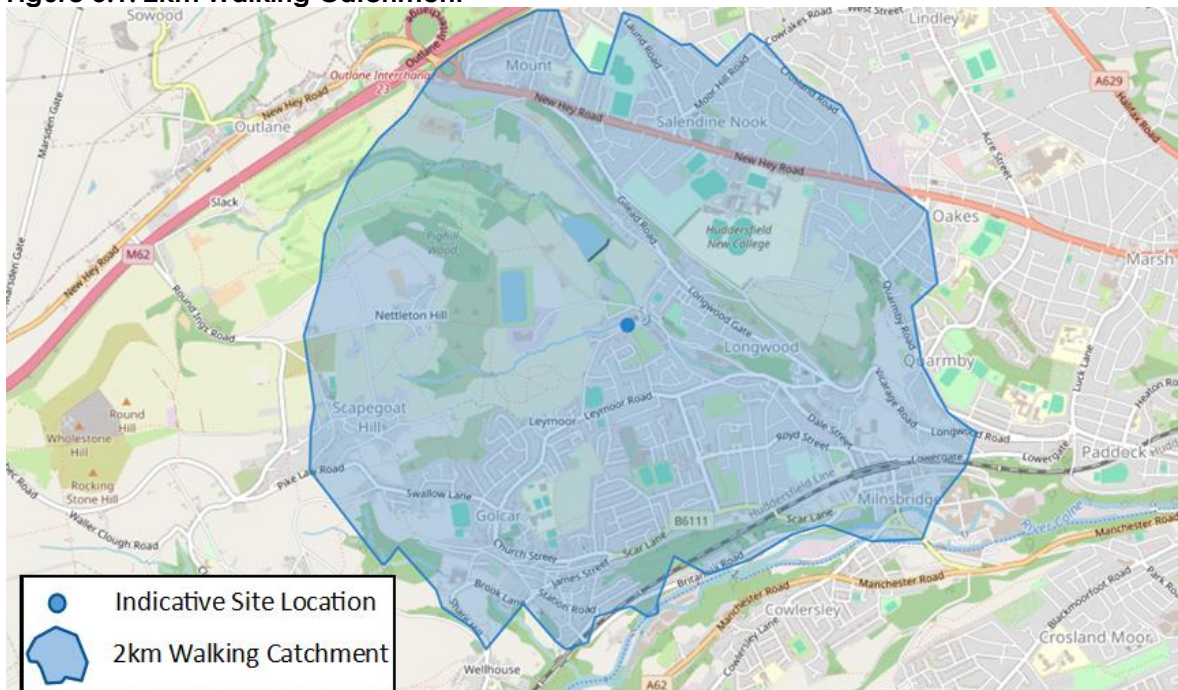
Table 3.1: IHT Walking Guidelines

Criteria	Town Centres	Commuting/ School
Desirable	200m	500m
Acceptable	400m	1000m
Preferred Maximum	800m	2000m

(Source: IHT)

3.3 **Figure 3.1** illustrates a 2km walking catchment from the site. A 2km walking catchment from the site includes Golcar, Salendine Nook and Milnsbridge.

Figure 3.1: 2km Walking Catchment



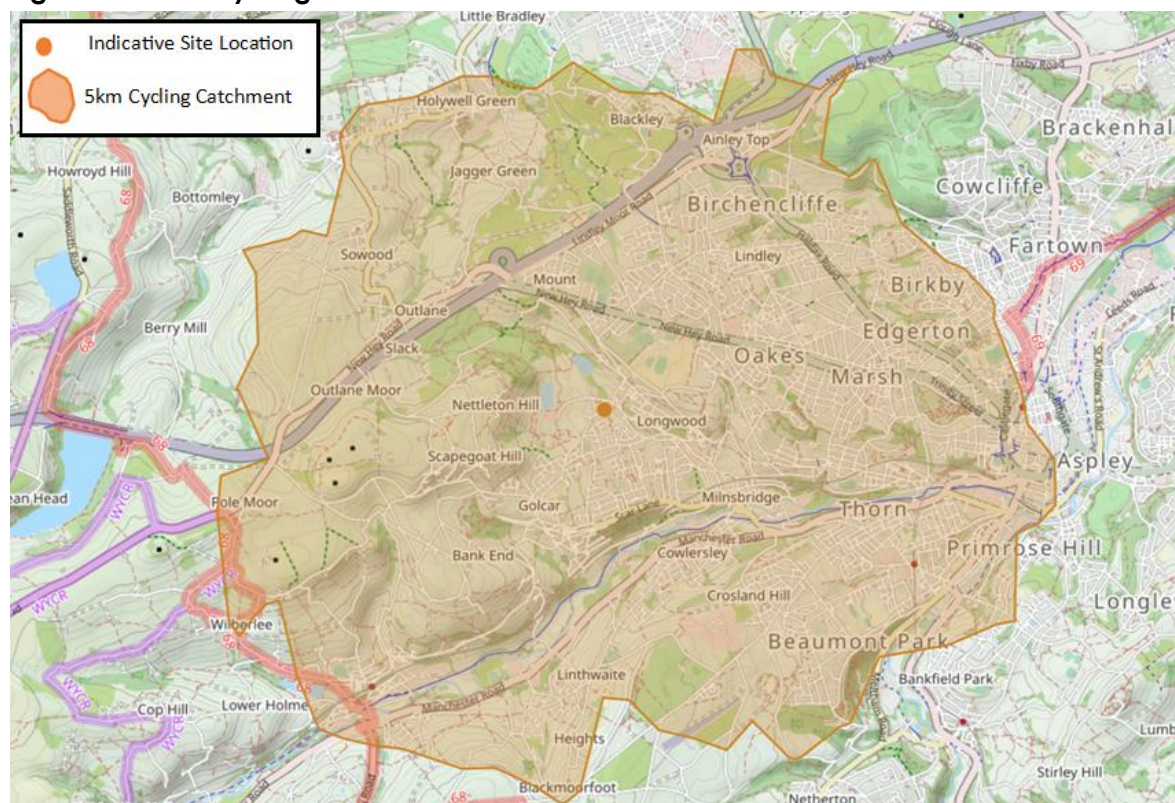
(Source: Open Street Map)

- 3.4 Two pedestrian accesses will be provided, one via the extension of Weavers Lane into the site on the western boundary of the site and a second pedestrian only access directly onto Parkwood Road on the northern boundary of the site.
- 3.5 The proposed vehicle access will be a 5.5m wide shared surface with a 600mm hard margin on both sides, providing a continuation of the existing provision in place along Weavers Lane. As Weavers Lane meets Thorpe Green Drive at a T-junction, 110m from the site access, a footway becomes available on both sides of the road, leading to Leymoor Road. The residential estate benefits from streetlighting throughout.
- 3.6 Footpaths with dropped kerbs are provided on all approaches to the junction to facilitate pedestrian movement. However, footways reduce to the southern side of the carriageway approximately 30m in both directions beyond the junction. Leymoor Road is street-lit throughout.
- 3.7 Direct pedestrian access to Parkwood Road from the site is available via a dedicated footpath to the north. However, pedestrian infrastructure along Parkwood Road is limited, with no continuous footway provision along its length.

Cycling

- 3.8 Five kilometres is typically considered to be a distance which people can realistically be expected to cycle, with relevant guidance stating that *“cycling also has the potential to substitute for short car trips, particularly those under 5km, and to form part of a longer journey by public transport”*.
- 3.9 A 5km cycling catchment from the site includes the western parts of Huddersfield, including Golcar, Mount, Scapegoat Hill, Quarmby, Lindley and Milnsbridge. This cycle catchment can be seen in **Figure 3.2**, overleaf.

Figure 3.2: 5km Cycling Catchment



(Source: Open Street Map)

- 3.10 Whilst there are no formal cycle facilities in the immediate vicinity of the site, National Cycle Network (NCN) Route 68 is accessible within the 5km cycle catchment. It is accessible in 24 minutes by bike, via Britannia Road in Slaithwaite, southwest of the site. NCN Route 68 is a long distance cycle route which runs in sections between Derbyshire and Northumbria. This route provides convenient and attractive options for both commuter and recreational cycling.
- 3.11 It is anticipated that cycling might be an attractive travel option for future residents of the site. The road network in the vicinity of the site is considered to be suitable for cyclists. Further details of the amenities accessible within cycling distance of the site are provided in the latter part of this section.

PUBLIC TRANSPORT ACCESSIBILITY

Local Bus Services

- 3.12 There are a number of bus stops located within a short walk distance of the site. The closest bus stops are located approximately 500m south of the site (7-minute walk or 2-minute bike), on Leymoor Road. Both the eastbound and westbound stops comprise flagpoles and timetable information. The Eastbound stop provides access to more services than the

westbound stop. These are summarised in **Table 3.2**, whilst the bus stops in the vicinity of the site can be seen in **Figure 3.3**.

Figure 3.3: Bus Stop Locations



(Source: Google Maps)

Table 3.2: Bus Service Summary

No.	Route	Frequency		
		Monday – Friday	Saturday	Sunday
Leymoor Road (Eastbound)				
301	Huddersfield – Golcar Circular	30 mins	30 mins	60 mins
Leymoor Road (Westbound)				
302	Huddersfield – Golcar Circular	30 mins	30 mins	60 mins
Longwood Gate				
356	Longwood – Huddersfield	60 mins	60 mins	60 mins

(Source: PT Operators) *irregular frequencies

3.13 Additional bus services can also be accessed at Huddersfield Bus Station, located an approximately 30-minute public transport journey via the 302 bus service, from which residents can access additional local services, as well as National Express intercity routes across the UK.

Rail Services

- 3.14 The closest railway station to the site is Lockwood, which is located approximately 4km southeast of the site, accessible in a 51-minute walk or 15-minute cycle. Lockwood Station benefits from 10no. cycle stands and offers step free access onto all platforms. Lockwood offers a service between Huddersfield and Sheffield, once every hour in each direction.
- 3.15 Located slightly further away, but still within good distance, is Huddersfield Train Station, which offers services to more regional and national destinations. Located approximately 6km east of the site, the station is accessible in 20 minutes by bike, or 40 minutes via the 301 bus service.
- 3.16 Huddersfield train station benefits from 33no. cycle compounds and racks and is a Category B1 station with step-free access onto all platforms.
- 3.17 Huddersfield railway station also benefits from six platforms and is served by approximately 13 trains in the peak periods, to a number of regional and national destinations. **Table 3.3**, below, outlines the key destinations

Table 3.3: Huddersfield Railway Station Services

Destination	Frequency
Liverpool Lime Street	2 per hour
Newcastle	1 per hour
Bradford Interchange	1 per hour
Manchester Victoria	1 per hour
Manchester Piccadilly	1 per hour
Manchester Airport	1 per hour
York	1 per hour
Leeds	1 per hour
Hull	1 per hour
Sheffield	1 per hour
Scarborough	1 per hour
Redcar Central	1 per hour

(Source: National Rail) *Sundays only.

- 3.18 It is considered that there is good public transport infrastructure in the vicinity of the development site.

LOCAL AMENITIES

3.19 **Table 3.4**, below, summarises some of the health, education, retail / leisure, and employment amenities which are located within walking (2km) or cycling (5km) distance of the site. It is considered that the wide range of amenities available demonstrates that residents can undertake sustainable journeys using modes of active travel. All measurements are taken from the site access on Weavers Lane.

Table 3.4: Local Amenities

Amenity	Distance	Walk Time	Cycle Time
Health			
Golcar Delivery Pharmacy	950m	14 mins	5 mins
Fieldhead Surgery	1000m	15 mins	5 mins
S T Shaw Pharmacy	1300m	20 mins	8 mins
Longwood Village Surgery	1400m	19 mins	6 mins
Bupa Dental Care Milnsbridge	1900m	25 mins	5 mins
Education			
St John's CE (Va) J & I School	1000m	16 mins	5 mins
Aspire Nursery	1100m	15 mins	4 mins
Huddersfield New College	2400m	38 mins	14 mins
Moorlands Primary School	2500m	41 mins	18 mins
Retail and Leisure			
Walkers Arms	450m	5 mins	1 min
Leymoor Cricket Club	450m	6 mins	1 min
Thorpe Green Bowling & Social Club	550m	8 mins	3 mins
Co-op Food – Leymoor Road / Golcar Fisheries	650m	9 mins	2 mins
The Fifth Fiend Taphouse	800m	11 mins	3 mins
HAVELI Huddersfield	1300m	20 mins	7 mins
Golcar Library	1300m	20 mins	7 mins
Aldi	2100m	28 mins	7 mins
HD3 Fitness Centre	2200m	36 mins	19 mins
Employment			
Colne side Business Park	2500m	32 mins	7 mins
Huddersfield Royal Infirmary	3300m	-	19 mins

(Source: Google Maps)

3.20 **Table 3.4** demonstrates the site is located within walking and cycling distance of a range of amenities, indicating residents will not need to rely on access to a car to access healthcare, education, retail / leisure or employment opportunities.

SUMMARY

- 3.21 Overall, it is considered that there are good opportunities for walking and cycling locally, with good pedestrian and cycling infrastructure surrounding the site. Trips to and from the proposed development site can, therefore, easily be undertaken by sustainable modes of travel, thus minimising the use of the private car.

4. BASELINE CONDITIONS

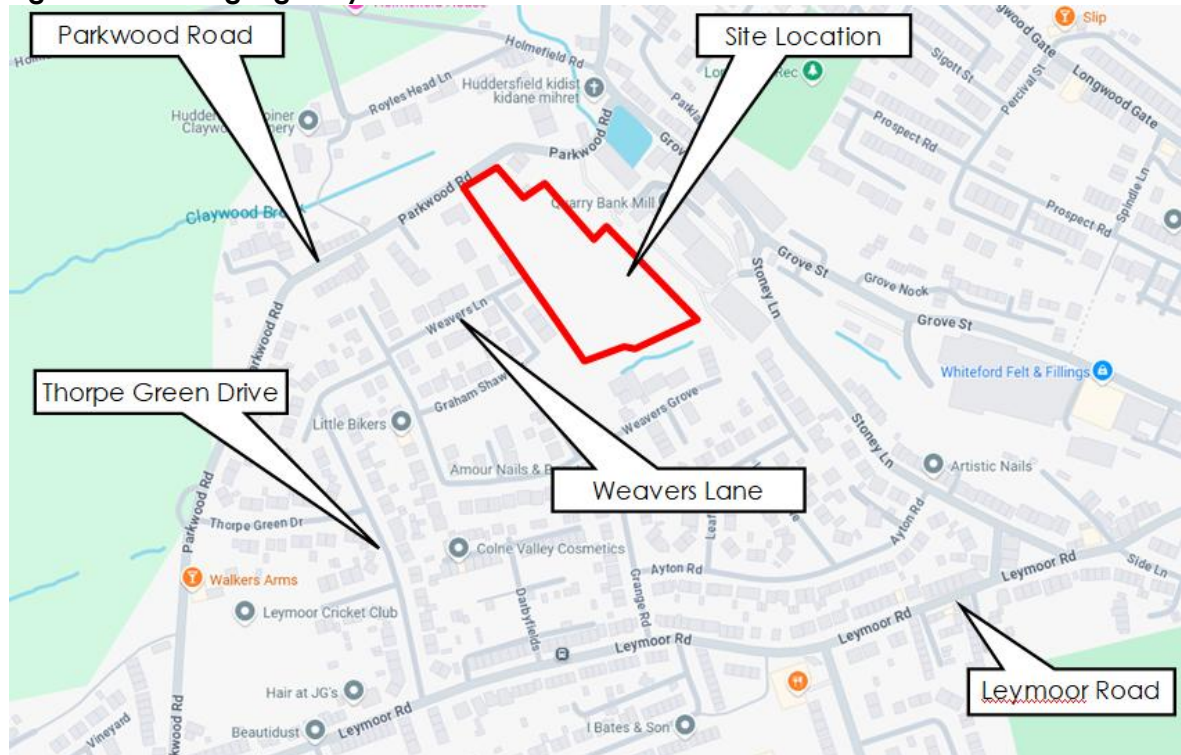
INTRODUCTION

- 4.1 This section of the Transport Statement describes the existing highway network in the vicinity of the site, provides an overview of the historic road safety record and reviews the infrastructure that will facilitate and encourage future residents of the site to walk, cycle or use public transport, rather than to travel by car.

EXISTING HIGHWAY NETWORK

- 4.2 A description is provided below of the local highway network in the immediate vicinity of the site; for ease, it is also shown in **Figure 4.1**, below.

Figure 4.1: Existing Highway Network



(Source: Google Maps)

- 4.3 The proposed development will take access from an extension of Weavers Lane, on the western boundary of the site. Weavers Lane, at current, is a cul-de-sac on a residential estate. The estate takes access from Leymoor Road, the nearest main road, and runs broadly east-west, south of the site.
- 4.4 From the proposed site access, Weavers Lane extends for approximately 110m in a westerly direction towards a staggered crossroads with Weavers Avenue and Thorpe Green Drive.

Thorpe Green Drive extends westward before bearing south towards priority T-junction with Leymoor Road, where it forms the minor arm.

- 4.5 Leymoor Road is subject to a 30mph speed limit, and a speed bump is present on the easterly approach, immediately after the junction. There are no parking restrictions along Leymoor Road within the vicinity of this junction. It forms the major arm of several priority junctions along its length, with the minor arms consisting mainly of residential access roads on both the eastern and western sides of the carriageway, with some residential dwellings taking direct frontage access.
- 4.6 Parkwood Road forms the northern boundary of the site but does not provide direct vehicular access to it. Vehicular access to Parkwood Road is achievable via Thorpe Green Drive, located within the adjacent residential area, or via a priority-controlled T-junction from Leymoor Road. Parkwood Road operates as a two-way carriageway and is subject to a 30mph speed limit.
- 4.7 From its junction with Leymoor Road, the carriageway has a width of approximately 6.9m, sufficient to accommodate two-way traffic flow. However, approximately 110m north of this junction, the carriageway narrows to approximately 4.4m in width, with intermittent passing places provided along its length to facilitate opposing vehicle movements. Several dwellings take direct frontage access to both sides of the road.

Road Safety

- 4.8 Accident data for the most recent 5-year period (2020 - 2024) has been sought from www.crashmap.co.uk for the highway network surrounding the site. Crashmap offers a definitive map of the official road collision statistics. The locations of accidents recorded within the vicinity of the site are shown in **Figure 4.2**, overleaf.

Figure 4.2: Accident Locations



(Source: Crashmap)

- 4.9 As can be seen in **Figure 4.2**, there have been a total of 2 accidents recorded in the most recent 5-year study period, both of which were slight in nature. This equates to an average of 0.4 accidents per year. It should be noted that in the immediate vicinity of the site access there have been no collisions recorded during the most recent five-year period.
- 4.10 Both accidents involved vehicles only and were recorded on Leymoor Road, which serves as the nearest main access route to the development site. It is considered that the level of accidents recorded over the most recent 5-year period does not indicate that there is an existing road safety issue in the vicinity of the site. Furthermore, as will be discussed further in **Section 5**, the development proposals will result in a negligible traffic impact and, will not, therefore, impact on road safety within the vicinity of the site.

5. TRIP GENERATION

TRIP GENERATION

5.1 This section considers the likely trip generation associated with the proposed development. The following parameters of the TRICS database have been selected:

- Land Use: Residential, Houses Privately Owned;
- Range: 15-54;
- Date Range: 01/01/2016 – 23/05/2024; and
- Location: Edge of Town.

5.2 **Table 5.1** summarises the vehicle trip rates, and resultant trip generation associated with the residential dwellings; the full TRICS output is also provided at **Appendix B**.

Table 5.1: Vehicle Trip Generation

	AM Peak			PM Peak		
	Arrival	Departure	Two-Way	Arrival	Departure	Two-Way
Trip Rates	0.164	0.365	0.529	0.339	0.181	0.520
Trip Generation	5	10	15	9	5	15

(Source: TRICS)

5.3 As can be seen in **Table 5.1**, the proposed development is anticipated to generate 15 two-way vehicle trips in both the AM and PM peak hour. This level of vehicle trip generation would not be considered as material and, as a result, no further assessment of the impact of the proposed development on the wider highway network is required.

6. ACCESS, PARKING AND SERVICING

INTRODUCTION

- 6.1 This section of the Transport Statement considers the proposed access, parking and servicing arrangements for the site.

ACCESS

- 6.2 Vehicular access to the site will be taken from an extension of Weavers Lane, on the western boundary, with pedestrian links provided to the north and south of the site. Within the development, the carriageway will be a 5.5m wide shared surface with a 600mm hard margin on both sides. A copy of the proposed site layout is attached at **Appendix A**.
- 6.3 The drawing attached at **Appendix C** shows 2.4m x 25m visibility splays at the internal access junction, in line with the 20mph design speed.

PARKING

Car Parking

- 6.4 In regard to car parking, the Kirklees Highway Design Guide SPD states; "Kirklees Council has not set local parking standards for residential and non-residential development, however in practice the majority of new 2-3-bedroom dwellings with Kirklees have provided 2 off-street parking spaces; with 4+ bedroom dwellings providing 3 off street car parking space. In most circumstances this has been supplemented by visitor parking at the rate of 1 per 4 dwellings".
- 6.5 As can be seen at **Appendix A**, each dwelling is to be provided with 2 allocated, off-street parking spaces. Furthermore, 7 visitor parking spaces have been provided across the development, in line with the Kirklees Highway Design Guide SPD.
- 6.6 Each dwelling at the development will be provided with a dedicated EV charging point and cycle parking in the form of a shed or garage.
- 6.7 It is considered that the proposed level of parking is provided to an appropriate standard, reflecting the size and nature of the development, and considering the site's proximity to public transport facilities and amenities.

SERVICING

General Servicing and Refuse Collection

- 6.8 It is anticipated that refuse collection will be undertaken by Kirklees Council from the kerbside within the site, utilising the turning head as shown in the site plan at **Appendix A**. Swept path analysis of the proposed turning head and access arrangements are attached at **Appendix D**. This drawing shows the swept path analysis of an 11.85m refuse vehicle accessing and egressing the site and utilising the turning head, to be provided within the site. Bin collection points have been provided throughout the site, to be accessed from the kerbside.

Fire Appliance Access

- 6.9 Manual for Streets (MfS) indicates that the access requirements for emergency vehicles are generally stipulated by the Fire Service. Table 13.1 of the *The Building Regulations 2010 'Fire Safety' Approved Document B (2019 edition, incorporating the 2020 amendments) Section 13 'Vehicle Access'*, sets out that a minimum road width of 3.7m be provided and turning facilities should be provided in any cul-de-sac that is more than 20m long. Fire tenders and emergency vehicles will access the site via the site access from Weavers Lane.

7. SUMMARY AND CONCLUSIONS

SUMMARY

- 7.1 TPS has prepared this Transport Statement to support a planning application for 28 dwellings on land south of Parkwood Road, Golcar. The following summarises the key points:
- The proposals are in keeping with both the local and national transport and land use planning policy agenda;
 - An analysis of contemporary accident data suggests that there are no accident trends that might be exacerbated by the addition of development related traffic;
 - The site benefits from good connectivity with surrounding facilities, with opportunities for future residents to travel by non-car modes, such as walking, cycling and public transport;
 - An assessment of the likely vehicle trip generation indicates that the proposed development would generate 15 two-way vehicle trips in both the AM and PM peak hours. This is unlikely to have a material impact on the surrounding highway network;
 - The servicing arrangements for the site have been considered; swept path analysis has been undertaken of the proposed turning head within the site, to demonstrate that a refuse vehicle will be able to enter and exit the site in forward gear; and
 - Car parking and cycle parking will be provided broadly in line with the Kirklees Council Highway Design Guide SPD.

CONCLUSION

- 7.2 Given the above, it is considered that the proposals will not result in a 'severe residual cumulative impact' (the test set out in NPPF); indeed, they will be complementary to the prevailing policy agenda. As such, there are no substantive highway grounds why the development should not be granted consent.



TPS Transport Consultants Ltd

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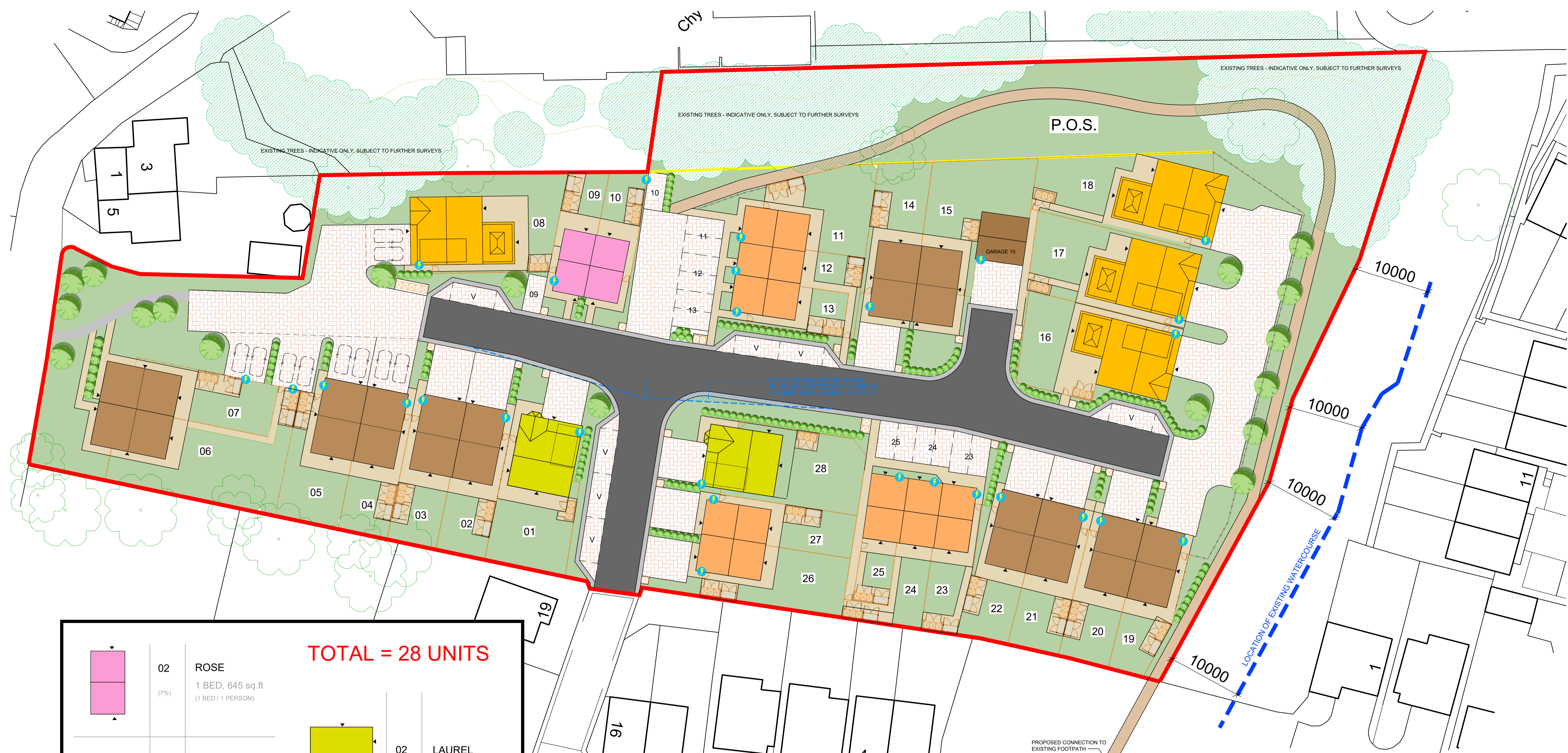
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REPORT APPENDICES

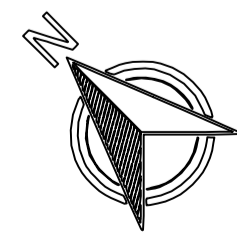
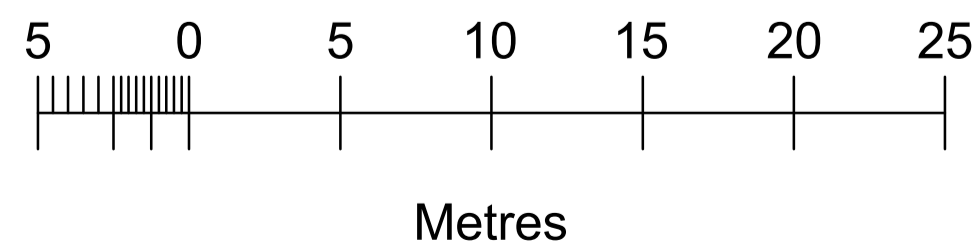
Appendix A

Proposed Site Layout



TOTAL = 28 UNITS

	02 ROSE 1 BED, 645 sq.ft (7%) (1 BED / 1 PERSON)		02 LAUREL 3 BED, 1220 sq.ft (7%) (3 BED / 5 PERSON) INT. GAR Min 3x6m
	08 ACORN 2 BED, 755 sq.ft (29%) (2 BED / 3 PERSON)		04 MAPLE 4 BED, 1495 sq.ft (14%) (4 BED / 6 PERSON) INT. GAR Min 3x6m
	12 HICKORY 3 BED, 1050 sq.ft (43%) (3 BED / 4 PERSON)		



KEY:-

	REAR GARDEN/DIVIDING FENCE - 1800mm HIGH CLOSE BOARDED TIMBER FENCE AND GATES WHERE INDICATED		GRASSED AREAS
	TIMBER KNEE RAIL FENCE		SAXON- BUFF PAVING
	RETAINING WALL - REFER TO ENGINEERS DRAWINGS FOR DETAILS		BRINDLE BLOCKWORK DRIVEWAY
	TIMBER BIN STORE FOR 3 No. REFUSE BINS (940x780mm)		TARMAC ROAD
	REFUSE COLLECTION POINT ADJACENT TO HIGHWAY		TARMAC FOOTPATHS & MARGINS
	ELECTRIC VEHICLE CHARGING POINT		
	TIMBER SECURE CYCLE STORAGE TO EACH PLOT WITHOUT A GARAGE (HANGING CYCLE RACK TO FITTED WITHIN GARAGES)		
	INDICATIVE PLANTING/TREES - REFER TO DETAILED LANDSCAPE DESIGN FOR FULL DETAILS		

1. CHECK ALL DIMENSIONS ON SITE PRIOR TO WORK COMMENCING
 2. CONTRACTOR TO REPORT ANY DISCREPANCIES TO ELDER LESTER ARCHITECTS
 3. ORDINANCE SURVEY DATA IS USED ON ELDER LESTER DRAWINGS. ELDER LESTER IS NOT RESPONSIBLE FOR THE ACCURACY OF DIMENSIONS RELATING TO ORDINANCE SURVEY DATA OR THAT OUTSIDE OF INSERTED TOPOGRAPHIC SURVEY INFORMATION
 4. WORK WITHIN THE CONSTRUCTION (DESIGN & MANAGEMENT) REGULATIONS 2015 IS NOT TO START UNTIL A HEALTH & SAFETY PLAN HAS BEEN PRODUCED (BY OTHERS)
 5. THIS DRAWING IS FOR USE BY THE CLIENT ONLY AND FOR THE LOCATION SPECIFIED. COPYRIGHT & ANY DESIGNS THEREIN ARE THE PROPERTY OF ELDER LESTER ARCHITECTS & MUST NOT BE COPIED WITHOUT WRITTEN AGREEMENT



PROJECT/CLIENT:
 RESIDENTIAL DEVELOPMENT
 PARKWOOD ROAD
 GOLCAR
 HUDDERSFIELD
 THE MANDALE GROUP

DRAWING INFORMATION:
 PROPOSED SITE PLAN

RECENT REVISION NOTES:
 REV A - 07/10/2024 - PLOT 14 AMENDED - DM
 REV B - 11/10/2024 - PLOT 11 HANDING AND PATH AMENDED
 REV C - 04/11/2025 - VARIOUS AMENDS TO ADDRESS PRE-APP RESPONSE COMMENTS - DM
 REV D - 19/11/2025 - ROAD AMENDED FOR VISIBILITY SPLAYS - DM

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ELDER LESTER
 ARCHITECTS

SCALE & PAPER SIZE: 1:250 @ A1
 DATE CREATED: 07/10/2024
 DATE OF LAST ISSUE: 19/11/2025
 DRAWING No: 2440-P001D
 DRAWN/ISSUED BY: RJT

Appendix B

TRICS Output

Calculation Reference: AUDIT-640801-250425-0447

TRIP RATE CALCULATION SELECTION PARAMETERS:

Land Use : 03 - RESIDENTIAL
Category : A - HOUSES PRIVATELY OWNED
TOTAL VEHICLES

Selected regions and areas:

02	SOUTH EAST	
	BO BEDFORD	1 days
	CT CENTRAL BEDFORDSHIRE	1 days
	ES EAST SUSSEX	2 days
	HC HAMPSHIRE	4 days
	MW MEDWAY	1 days
	SC SURREY	1 days
03	SOUTH WEST	
	DC DORSET	2 days
04	EAST ANGLIA	
	NF NORFOLK	3 days
05	EAST MIDLANDS	
	NT NOTTINGHAMSHIRE	1 days
06	WEST MIDLANDS	
	ST STAFFORDSHIRE	1 days
	WK WARWICKSHIRE	1 days
	WO WORCESTERSHIRE	1 days
07	YORKSHIRE & NORTH LINCOLNSHIRE	
	NY NORTH YORKSHIRE	1 days
08	NORTH WEST	
	LC LANCASHIRE	1 days
09	NORTH	
	IM ISLE OF MAN	1 days

This section displays the number of survey days per TRICS® sub-region in the selected set

Primary Filtering selection:

This data displays the chosen trip rate parameter and its selected range. Only sites that fall within the parameter range are included in the trip rate calculation.

Parameter: No of Dwellings
Actual Range: 17 to 50 (units:)
Range Selected by User: 15 to 54 (units:)

Parking Spaces Range: All Surveys Included

Parking Spaces per Dwelling Range: All Surveys Included

Bedrooms per Dwelling Range: All Surveys Included

Percentage of dwellings privately owned: All Surveys Included

Public Transport Provision:

Selection by: Include all surveys

Date Range: 01/01/16 to 23/05/24

This data displays the range of survey dates selected. Only surveys that were conducted within this date range are included in the trip rate calculation.

Selected survey days:

Monday	4 days
Tuesday	3 days
Wednesday	8 days
Thursday	3 days
Friday	4 days

This data displays the number of selected surveys by day of the week.

Selected survey types:

Manual count	21 days
Directional ATC Count	1 days

This data displays the number of manual classified surveys and the number of unclassified ATC surveys, the total adding up to the overall number of surveys in the selected set. Manual surveys are undertaken using staff, whilst ATC surveys are undertaken using machines.

Selected Locations:

Edge of Town	22
--------------	----

This data displays the number of surveys per main location category within the selected set. The main location categories consist of Free Standing, Edge of Town, Suburban Area, Neighbourhood Centre, Edge of Town Centre, Town Centre and Not Known.

Selected Location Sub Categories:

Residential Zone	21
No Sub Category	1

This data displays the number of surveys per location sub-category within the selected set. The location sub-categories consist of Commercial Zone, Industrial Zone, Development Zone, Residential Zone, Retail Zone, Built-Up Zone, Village, Out of Town, High Street and No Sub Category.

Inclusion of Servicing Vehicles Counts:

Servicing vehicles Included	4 days - Selected
Servicing vehicles Excluded	18 days - Selected

Secondary Filtering selection:

Use Class:

C3	22 days
----	---------

This data displays the number of surveys per Use Class classification within the selected set. The Use Classes Order (England) 2020 has been used for this purpose, which can be found within the Library module of TRICS®.

Population within 500m Range:

All Surveys Included

Secondary Filtering selection (Cont.):

Population within 1 mile:

1,001 to 5,000	3 days
5,001 to 10,000	7 days
10,001 to 15,000	8 days
15,001 to 20,000	2 days
20,001 to 25,000	1 days
25,001 to 50,000	1 days

This data displays the number of selected surveys within stated 1-mile radii of population.

Population within 5 miles:

5,001 to 25,000	4 days
25,001 to 50,000	4 days
50,001 to 75,000	2 days
75,001 to 100,000	2 days
100,001 to 125,000	1 days
125,001 to 250,000	8 days
250,001 to 500,000	1 days

This data displays the number of selected surveys within stated 5-mile radii of population.

Car ownership within 5 miles:

0.6 to 1.0	6 days
1.1 to 1.5	15 days
1.6 to 2.0	1 days

This data displays the number of selected surveys within stated ranges of average cars owned per residential dwelling, within a radius of 5-miles of selected survey sites.

Travel Plan:

Yes	11 days
No	11 days

This data displays the number of surveys within the selected set that were undertaken at sites with Travel Plans in place, and the number of surveys that were undertaken at sites without Travel Plans.

PTAL Rating:

No PTAL Present	22 days
-----------------	---------

This data displays the number of selected surveys with PTAL Ratings.

Covid-19 Restrictions	Yes	At least one survey within the selected data set was undertaken at a time of Covid-19 restrictions
-----------------------	-----	----------------------------------------------------------------------------------------------------

LIST OF SITES relevant to selection parameters (Cont.)

8	HC-03-A-22	MIXED HOUSES	HAMPSHIRE
	BOW LAKE GARDENS NEAR EASTLEIGH BISHOPSTOKE Edge of Town Residential Zone Total No of Dwellings: 40 <i>Survey date: WEDNESDAY 31/10/18</i>		
	<i>Survey Type: MANUAL</i>		
9	HC-03-A-31	MIXED HOUSES & FLATS	HAMPSHIRE
	KILN ROAD LIPHOOK Edge of Town Residential Zone Total No of Dwellings: 44 <i>Survey date: FRIDAY 07/10/22</i>		
	<i>Survey Type: MANUAL</i>		
10	HC-03-A-37	MIXED HOUSES	HAMPSHIRE
	REDFIELDS LANE FLEET CHURCH CROOKHAM Edge of Town Residential Zone Total No of Dwellings: 50 <i>Survey date: WEDNESDAY 27/03/24</i>		
	<i>Survey Type: MANUAL</i>		
11	IM-03-A-05	MIXED HOUSES	ISLE OF MAN
	SCARLETT ROAD CASTLETOWN Edge of Town Residential Zone Total No of Dwellings: 45 <i>Survey date: TUESDAY 21/05/24</i>		
	<i>Survey Type: MANUAL</i>		
12	LC-03-A-31	DETACHED HOUSES	LANCASHIRE
	GREENSIDE PRESTON COTTAM Edge of Town Residential Zone Total No of Dwellings: 32 <i>Survey date: FRIDAY 17/11/17</i>		
	<i>Survey Type: MANUAL</i>		
13	MW-03-A-02	MIXED HOUSES	MEDWAY
	OTTERHAM QUAY LANE RAINHAM Edge of Town Residential Zone Total No of Dwellings: 19 <i>Survey date: MONDAY 06/06/22</i>		
	<i>Survey Type: MANUAL</i>		
14	NF-03-A-05	MIXED HOUSES	NORFOLK
	HEATH DRIVE HOLT Edge of Town Residential Zone Total No of Dwellings: 40 <i>Survey date: THURSDAY 19/09/19</i>		
	<i>Survey Type: MANUAL</i>		
15	NF-03-A-10	MIXED HOUSES & FLATS	NORFOLK
	HUNSTANTON ROAD HUNSTANTON Edge of Town Residential Zone Total No of Dwellings: 17 <i>Survey date: WEDNESDAY 12/09/18</i>		
	<i>Survey Type: DIRECTIONAL ATC COUNT</i>		

LIST OF SITES relevant to selection parameters (Cont.)

16	NF-03-A-37 GREENFIELDS ROAD DEREHAM	MIXED HOUSES		NORFOLK
	Edge of Town Residential Zone Total No of Dwellings:		44	
	Survey date: TUESDAY		27/09/22	Survey Type: MANUAL
17	NT-03-A-08 WIGHAY ROAD HUCKNALL	DETACHED HOUSES		NOTTINGHAMSHIRE
	Edge of Town Residential Zone Total No of Dwellings:		36	
	Survey date: MONDAY		18/10/21	Survey Type: MANUAL
18	NY-03-A-14 PALACE ROAD RIPON	DETACHED & BUNGALOWS		NORTH YORKSHIRE
	Edge of Town Residential Zone Total No of Dwellings:		45	
	Survey date: WEDNESDAY		18/05/22	Survey Type: MANUAL
19	SC-03-A-07 FOLLY HILL FARNHAM	MIXED HOUSES		SURREY
	Edge of Town Residential Zone Total No of Dwellings:		41	
	Survey date: WEDNESDAY		11/05/22	Survey Type: MANUAL
20	ST-03-A-08 SILKMORE CRESCENT STAFFORD MEADOWCROFT PARK	DETACHED HOUSES		STAFFORDSHIRE
	Edge of Town Residential Zone Total No of Dwellings:		26	
	Survey date: WEDNESDAY		22/11/17	Survey Type: MANUAL
21	WK-03-A-04 DALEHOUSE LANE KENILWORTH	DETACHED HOUSES		WARWICKSHIRE
	Edge of Town Residential Zone Total No of Dwellings:		49	
	Survey date: FRIDAY		27/09/19	Survey Type: MANUAL
22	WO-03-A-07 RYE GRASS LANE REDDITCH	MIXED HOUSES & FLATS		WORCESTERSHIRE
	Edge of Town Residential Zone Total No of Dwellings:		47	
	Survey date: THURSDAY		01/10/20	Survey Type: MANUAL

This section provides a list of all survey sites and days in the selected set. For each individual survey site, it displays a unique site reference code and site address, the selected trip rate calculation parameter and its value, the day of the week and date of each survey, and whether the survey was a manual classified count or an ATC count.

TRIP RATE for Land Use 03 - RESIDENTIAL/A - HOUSES PRIVATELY OWNED

TOTAL VEHICLES

Calculation factor: 1 DWELLS

BOLD print indicates peak (busiest) period

Time Range	ARRIVALS			DEPARTURES			TOTALS		
	No. Days	Ave. DWELLS	Trip Rate	No. Days	Ave. DWELLS	Trip Rate	No. Days	Ave. DWELLS	Trip Rate
00:00 - 01:00									
01:00 - 02:00									
02:00 - 03:00									
03:00 - 04:00									
04:00 - 05:00									
05:00 - 06:00									
06:00 - 07:00									
07:00 - 08:00	22	39	0.081	22	39	0.271	22	39	0.352
08:00 - 09:00	22	39	0.164	22	39	0.365	22	39	0.529
09:00 - 10:00	22	39	0.161	22	39	0.211	22	39	0.372
10:00 - 11:00	22	39	0.135	22	39	0.163	22	39	0.298
11:00 - 12:00	22	39	0.165	22	39	0.161	22	39	0.326
12:00 - 13:00	22	39	0.193	22	39	0.190	22	39	0.383
13:00 - 14:00	22	39	0.172	22	39	0.153	22	39	0.325
14:00 - 15:00	22	39	0.154	22	39	0.208	22	39	0.362
15:00 - 16:00	22	39	0.273	22	39	0.179	22	39	0.452
16:00 - 17:00	22	39	0.274	22	39	0.160	22	39	0.434
17:00 - 18:00	22	39	0.339	22	39	0.181	22	39	0.520
18:00 - 19:00	22	39	0.241	22	39	0.131	22	39	0.372
19:00 - 20:00									
20:00 - 21:00									
21:00 - 22:00									
22:00 - 23:00									
23:00 - 24:00									
Total Rates:			2.352			2.373			4.725

This section displays the trip rate results based on the selected set of surveys and the selected count type (shown just above the table). It is split by three main columns, representing arrivals trips, departures trips, and total trips (arrivals plus departures). Within each of these main columns are three sub-columns. These display the number of survey days where count data is included (per time period), the average value of the selected trip rate calculation parameter (per time period), and the trip rate result (per time period). Total trip rates (the sum of the column) are also displayed at the foot of the table.

*To obtain a trip rate, the average (mean) trip rate parameter value (TRP) is first calculated for all selected survey days that have count data available for the stated time period. The average (mean) number of arrivals, departures or totals (whichever applies) is also calculated (COUNT) for all selected survey days that have count data available for the stated time period. Then, the average count is divided by the average trip rate parameter value, and multiplied by the stated calculation factor (shown just above the table and abbreviated here as FACT). So, the method is: COUNT/TRP*FACT. Trip rates are then rounded to 3 decimal places.*

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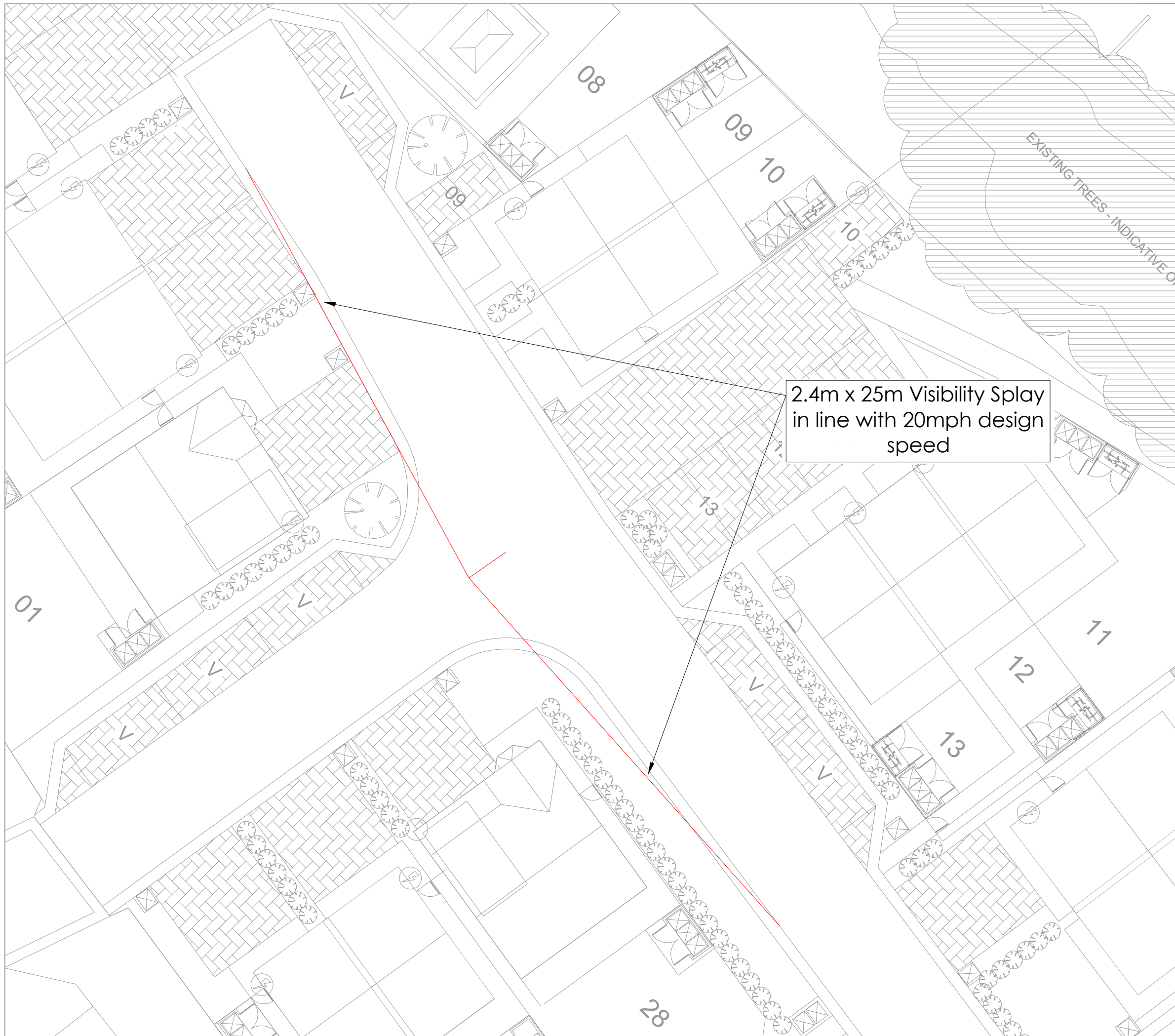
Parameter summary

Trip rate parameter range selected: 17 - 50 (units:)
Survey date range: 01/01/16 - 23/05/24
Number of weekdays (Monday-Friday): 22
Number of Saturdays: 0
Number of Sundays: 0
Surveys automatically removed from selection: 0
Surveys manually removed from selection: 0


This section displays a quick summary of some of the data filtering selections made by the TRICS® user. The trip rate calculation parameter range of all selected surveys is displayed first, followed by the range of minimum and maximum survey dates selected by the user. Then, the total number of selected weekdays and weekend days in the selected set of surveys are shown. Finally, the number of survey days that have been manually removed from the selected set outside of the standard filtering procedure are displayed.

Appendix C

Visibility Splays

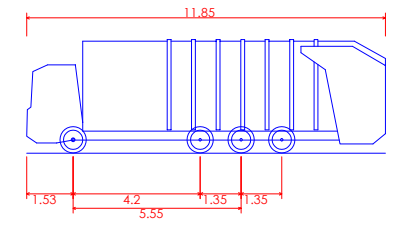
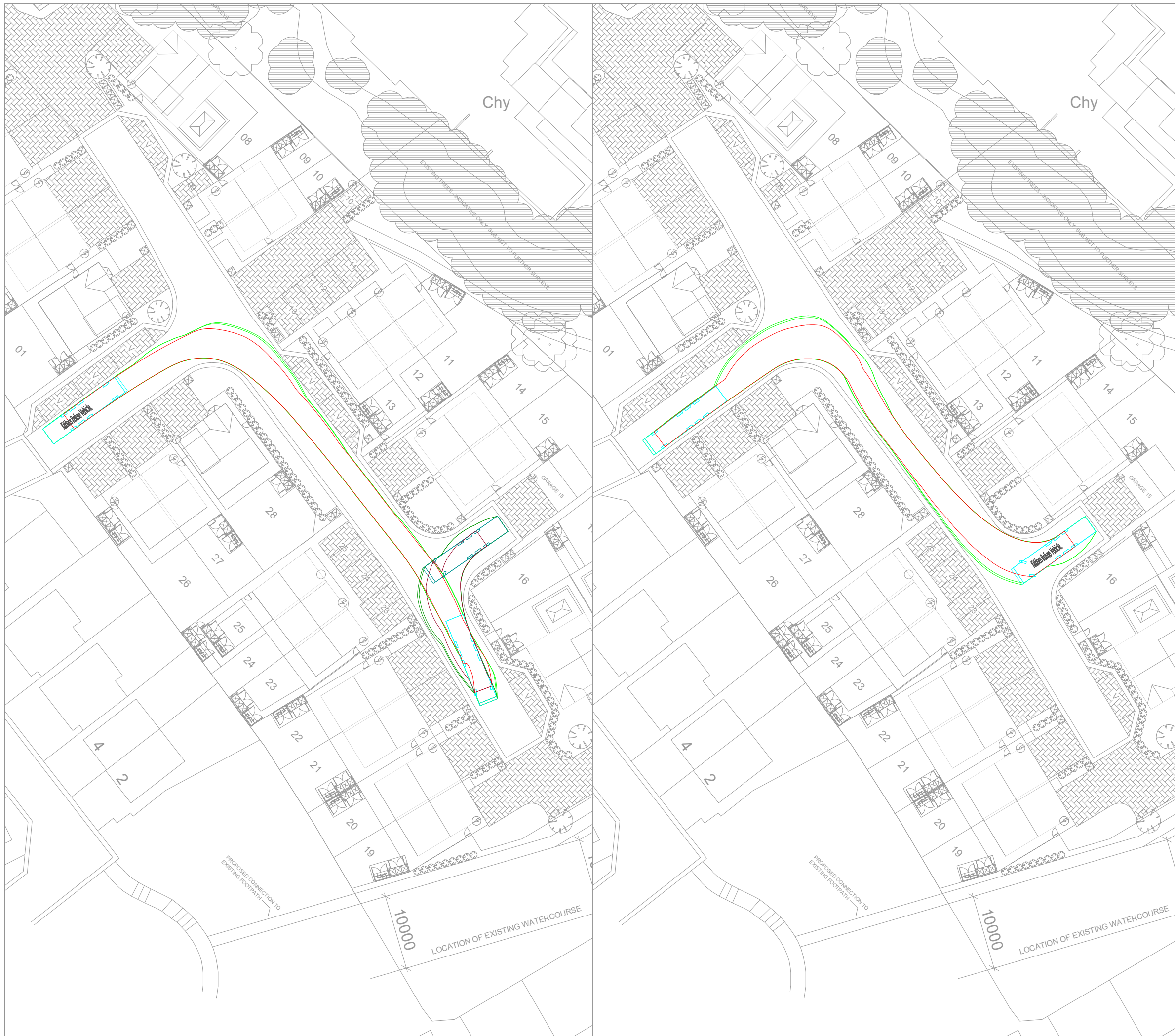


2.4m x 25m Visibility Splay
in line with 20mph design
speed


20.11.25	A	Revised site layout	JA	JT
Date	Rev	Description	Drawn	Chkd
				
<small>TPS Transport Consultants Ltd Stonebridge Court 151-153 Wakefield Road Horbury Wakefield WF4 5HQ t: 01924 664638 e: info@tpsconsultants.co.uk www.tpsconsultants.co.uk</small>				
Project				
Parkwood Road, Golcar				
Title				
Visibility Splays				
Date	Designed by		Checked by	
13.11.25	JA		CG	
Drawing Number			Scale @ A3	Revision
P2971 - V - 1001			1:200	A

Appendix D

Swept Path Analysis



Kirkles Refuse Vehicle.
 Overall Length 11.850m
 Overall Width 2.500m
 Overall Body Height 3.749m
 Min Body Ground Clearance 0.302m
 Track Width 2.490m
 Lock to lock time 6.00s
 Wall to Wall Turning Radius 11.000m

20.11.25	A	Revised site layout	JA	JT
Date	Rev	Description	Drawn	Chkd
				
Project Parkwood Road, Golcar				
Title Swept Path Analysis				
Date	Designed by	Checked by		
13.11.25	JA	CG		
Drawing Number			Scale @ A3	Revision
P2971 - T - 1001			1:500	A

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