



DESIGN AND ACCESS STATEMENT

Parkwood Road, Longwood

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Revision Record					
Rev	Description	Date	Author	Checked	Approved
0	Final Issue	16.12.25	DM	DM	DM

1. Introduction

- 1.1 This Design and Access Statement (DAS) has been prepared to accompany a full planning application for the erection of 28no. dwellinghouses, together with associated landscaping and external works, at land off Parkwood Road/Weavers Lane, Longwood, Huddersfield.

2. Site Context

2.1 Located towards the northwestern edge of Longwood, the greenfield application site (see Figure 2.1) is 1.2 hectares (ha) in area, comprising of grassland, with trees and shrubs present along its edges (see Photos 2.1 – 2.2 overleaf).

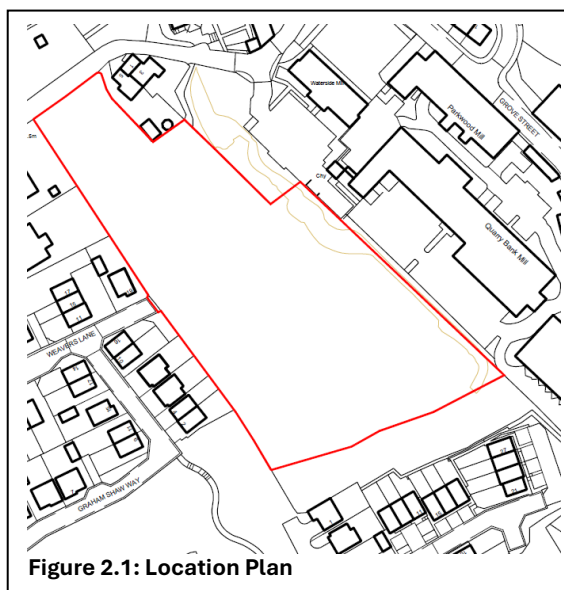


Figure 2.1: Location Plan

2.2 In terms of the surrounding context, the site lies southeast of Parkwood Road, with residential development (Weavers Chase) to its southwest and southeast. To the northeast is the Parkwood Mills, a

Grade II listed woollen mill complex now in residential use, whilst to the north and northwest - either side of the application site's short frontage to the aforementioned Parkwood Road - are residential dwellings, some of which are Grade II listed and within Longwood Edge Conservation Area (1, 3, and 5 Parkwood Road). To confirm, the conservation area designation covers part of the site's north/east boundary.

2.3 Although no trees on-site are protected by Tree Preservation Orders (TPO's), all trees lining the north/east boundary are protected by the aforesaid conservation area designation.

2.4 With regards to access, the site does not currently provide access for vehicles. No Public Rights of Way (PRoW) cross the site, however there is a pedestrian entrance at the north end of the site at Parkwood Road, and evidence of well-trodden pedestrian routes from this entrance and across the site towards Grange Road.

2.5 The site is located in Flood Zone 1, albeit with an open watercourse located adjacent to the southern boundary. It is culverted upstream through the Barratt David Wilson Homes development, and culverted downstream as it heads under Quarry Bank Mills and into Longwood Brook.



Photos 2.1 – 2.2: Site Photos

3. Design Response

3.1 In this instance, the design and layout of the scheme has been dictated by the overarching principles set out below:

- The delivery of a range of attractive dwellings of various sizes;
- To provide a 10 metre (m) buffer between the application site and the adjacent watercourse;
- To broadly follow the previously approved layouts under consent refs. 2020/62/92118/W and 2019/62/92164/W;
- The delivery of an area of Public Open Space (POS) on-site;
- To ensure all areas of the site are positively addressed;
- The retention of the existing hedgerows and trees around the boundary of the site, where possible and warranted;
- To provide pedestrian and vehicular access via Weavers Lane;
- To protect the residential amenity of both future residents and those living in the adjacent estate;
- To provide an appropriate amount of parking; and
- To enable the achievement of a suitably detailed drainage strategy in due course, incorporating Sustainable Drainage Systems (SuDS).

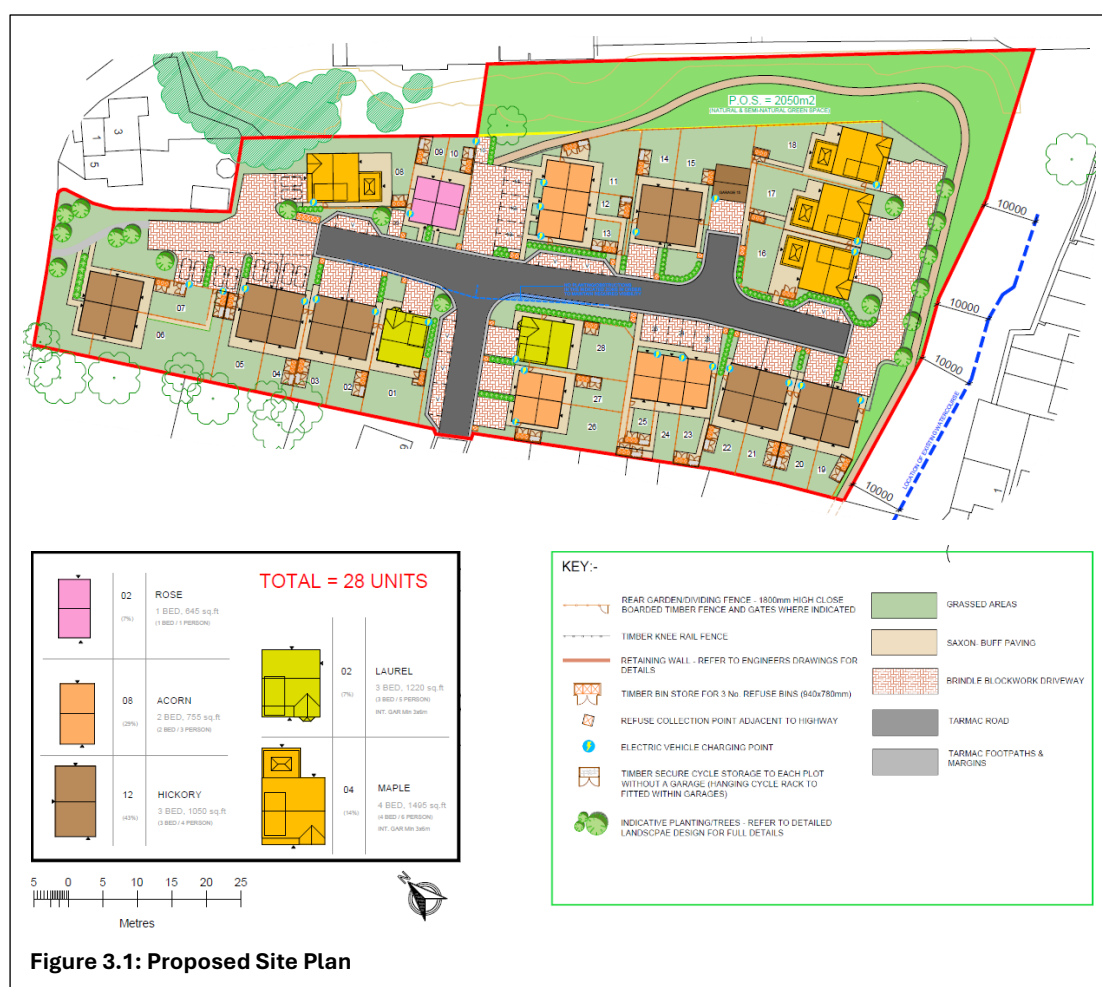
Use and Amount

3.2 The scheme proposes 28no. dwellings in total, with the site plan (see Figure 3.1 overleaf) confirming that there would be 5no. house types delivered, with the mix as follows:

- 2no. one-bed semi-detached houses (Rose);
- 8no. two-bed semi-detached houses (Acorn);
- 12no. three-bed semi-detached houses (Hickory);
- 2no. three-bed detached houses (Laurel); and
- 4no. four-bed detached houses (Maple).

3.3 With an application site size of 1ha, 28no. dwellings would equate to 28 dwellings per hectare (dph). This is considered a suitable density, bearing in mind that a portion of the site is along the northeastern boundary of the site is not going to be developed (this is proposed as POS).

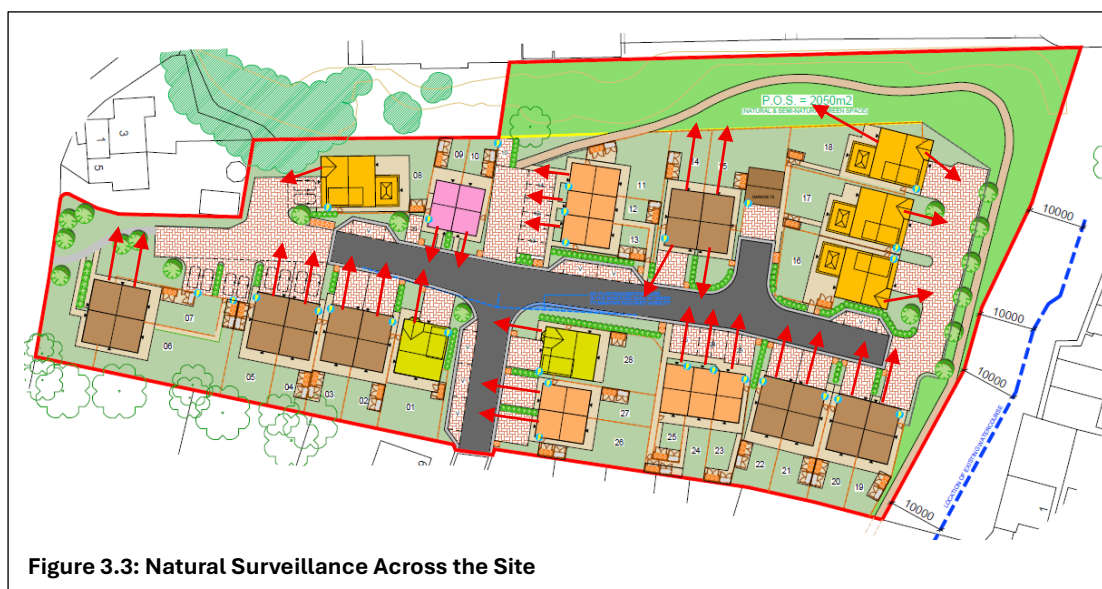
3.4 In addition, 28no. dwellings would take the total amount of housing developed across the wider housing allocation (ref. HS148: Land to the south of, Parkwood) to 124no., which is just 1no. short of the indicative 125no. units.



Layout

3.5 As can be seen when the proposed site plan (see Figure 3.1) is compared against the previously approved plans (see Figure 3.2 overleaf), the layout is very similar and would

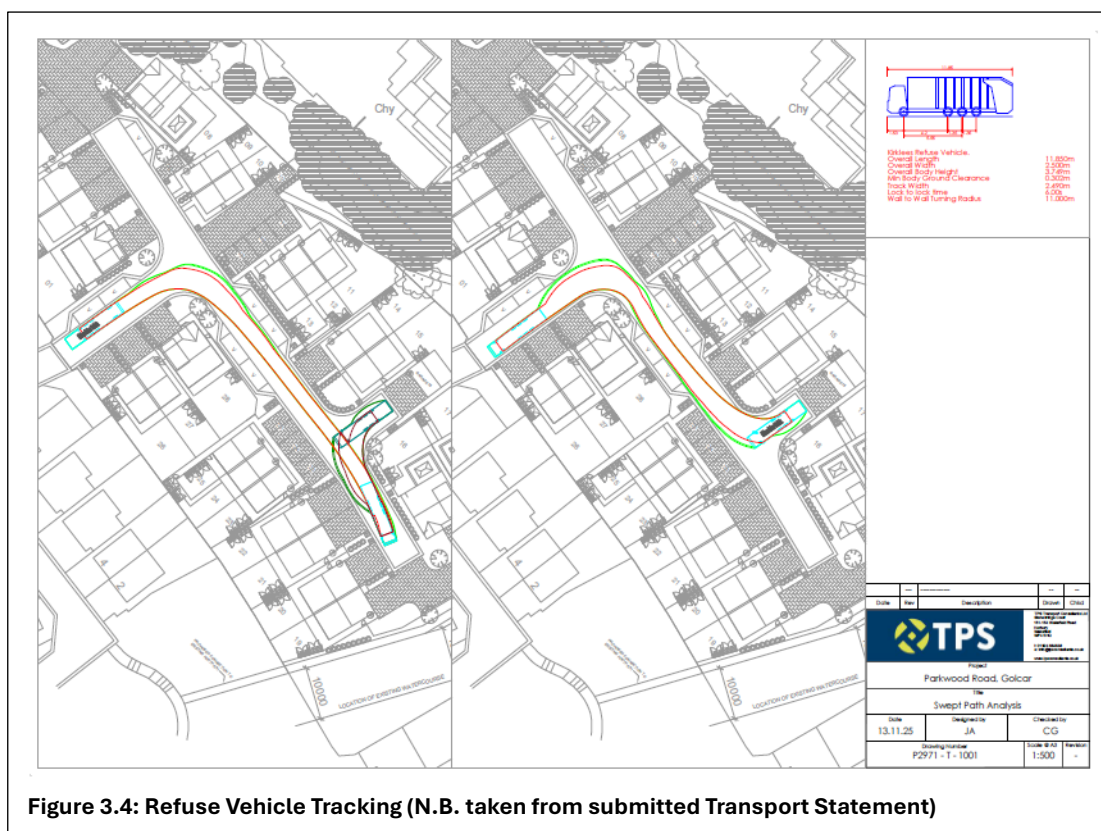
3.6 Figure 3.3 demonstrates that the proposed layout would ensure that all areas of the application site, including pedestrian routes and the POS, would be sufficiently overlooked by the front and/or rear elevations of properties, providing good levels of natural surveillance across the site and enhancing security as a result.



3.7 In line with advice from the Lead Local Flood Authority (LLFA) received as part of the pre-application response from Kirklees Council (ref. 2024/21271), the southern boundary of the site has been moved back 10m from the adjacent watercourse. In addition, and in response to advice from the applicants' engineers, Lynas Engineers, the proposed layout would allow the incorporation of various sustainable drainage features, including permeable paving, bioretention tree pits, and swales, which are intended to improve the quality of water that is ultimately discharged to the watercourse.

3.8 The Arboricultural Report by Elliott Consultancy submitted as part of the application indicates those trees and hedgerows to be retained as part of the proposals, and how this would be achieved.

3.9 The submitted Site Waste Management Plan confirms that the proposed layout would provide sufficient storage space within the curtilage of each property for storing 3 x 240 litre bins securely within rear garden areas, a refuse collection point adjacent to the estate road, and adequate turning areas for refuse collection vehicles within the site (see Figure 3.4).



Scale and Appearance

3.10 As displayed at Figure 3.5 on page 12, the proposals would comprise of two-storey semi-detached, detached, and short terrace dwellings, which would reflect the prevailing scale and type of the existing dwellings to the north and east (see Photos 3.1 - 3.3 overleaf).



Photos 3.1 – 3.3: Dwellings Within the Adjacent Estate
Source: Google Streetview

3.11 With regards to the floor areas, all proposed dwellings would meet - and in most instances exceed - nationally described minimum space standards¹. These floor areas are set out below:

- Rose (one-bed semi-detached): 60 square metres (sq.m);
- Acorn (two-bed semi-detached): 70sq.m;
- Hickory (three-bed semi-detached): 98sq.m;
- Laurel (three-bed detached): 113sq.m; and
- Maple (four-bed detached):139sq.m.

3.12 Turning to appearance, Figure 3.5 overleaf illustrates that there would be a range of house types and sizes delivered across the application site, providing some visual interest to enhance the scheme in an appropriate manner. The use of stone for corbels and detailing around doors and above windows and garages would also help to assimilate the scheme into the local context, with properties adjacent displaying with such features already (see Photos 3.1 - 3.3). Moreover, following the pre-application comments received, traditional style doors and windows with glazing bars have been introduced, together with vertical emphasis garage doors, all of which would be consistent with the adjacent estate.

¹*Technical housing standards nationally described space standard*, Department for Communities and Local Government (March 2015)



Figure 3.5: Proposed House Types
N.B. Front elevations shown only

3.13 All of the above, together with the use of high-quality, appropriate materials, would help create an attractive housing scheme that would be suitably complementary to the local area. Having noted that the use of artificial stone was approved previously on the site, it is currently envisaged that the materials would comprise of coursed artificial stone and grey roof tiles (Figure 3.6 shows typical products, albeit the final materials can be agreed via condition), reflecting dwellings in the adjacent estate (see Photo 3.4 overleaf).



Landscaping

- 3.14 As is evident from the proposed site plan (see Figure 3.1), the key landscaping feature of the scheme is the planned POS area along the northeastern and southern boundaries of the application site, and, as confirmed at paragraph 3.8, existing trees and hedgerows would be retained as part of the proposals where feasible and the species are worthy of retention, guided by Elliott Consultancy following their recent site visit that assessed the quality of existing specimens. Other areas of soft landscaping would be interspersed throughout the site.

3.15 Figure 3.7 confirms the provision of semi-natural greenspace and amenity grassland POS on-site, with 1,130sq.m of the former and 440sq.m of the latter proposed, respectively. The above represents a slight shortfall on semi-natural greenspace based on the Open Space SPD standards, and it is envisaged that the residual amount would be secured as part of a Section 106 (S.106) Legal Agreement. In contrast, the SPD standards would be achieved for amenity grassland. Contributions towards the other forms of open space, specifically parks and recreation, children’s and young people (Local Area for Play [LAP]), and outdoor space, would also be secured as part of a S.106 Agreement, in line with the aforementioned SPD.



3.16 Turning to hard landscaping, the proposed site plan confirms that the estate road, visitor parking bays, and hard margins would be tarmac, principally due to its hardwearing nature for the most highly trafficked areas, with block paving used in shared drive areas and for

in curtilage parking.

Access

Vehicular and Pedestrian Access, and Car Parking

- 3.17 As shown at Figure 3.1, vehicular and pedestrian access would be provided from the adjoining residential estate, Weavers Lane, to the west. A further pedestrian access point would be retained from Parkwood Road (see Photo 3.5), with another route provided to the corner of the application site close to Weavers Grove, where there is a path *in situ* through the existing open space (see Photo 3.6 overleaf).



Photo 3.5: Pedestrian Access from Parkwood Road



Photo 3.6: The Corner of the Site Close to Weavers Grove

3.18 In response to the pre-application comments, the existing Weavers Lane shared surface carriageway (5.5m wide) would be extended into the site with hard margins to both sides (600 millimetre [mm]) (see Photo 3.7 overleaf), and the road arrangement between plots 1 and 10 has been revised to something deemed more suitable by the Local Highway Authority (LHA), with the previous angular arrangement removed.



- 3.19 As confirmed at paragraph 3.9, the proposed layout would provide adequate access and turning facilities for refuse vehicles (see Figure 3.4). Indeed, the Transport Statement provided in support of the application confirms that the proposals can be accessed safely and suitably.
- 3.20 All properties would have dedicated car parking and an electric vehicle charging point, and benefit from at least 1no. space for the one-bed properties, at least 2no. spaces for the two and three-bed properties, and at least 3no. spaces for the four-bed properties. Of the above, some properties' parking provision would include a garage, which would all be 3m x 6m minimum. Further to the above, 7no. visitor parking bays would also be provided on-site. Turning to cycle parking, those dwellings without a garage would be provided with a lockable timber cycle store within rear gardens.

Proximity to Facilities

- 3.21 The submitted Transport Statement confirms that the site is located within walking and cycling distance of a range of amenities, meaning that residents would not need to rely on access to a car in order to access healthcare, education, retail/leisure or employment opportunities (see Table 3.1).

Amenity	Distance	Walk Time	Cycle Time
Health			
Golcar Delivery Pharmacy	950m	14 mins	5 mins
Fieldhead Surgery	1000m	15 mins	5 mins
S T Shaw Pharmacy	1300m	20 mins	8 mins
Longwood Village Surgery	1400m	19 mins	6 mins
Bupa Dental Care Milnsbridge	1900m	25 mins	5 mins
Education			
St John's CE (Va) J & I School	1000m	16 mins	5 mins
Aspire Nursery	1100m	15 mins	4 mins
Huddersfield New College	2400m	38 mins	14 mins
Moorlands Primary School	2500m	41 mins	18 mins
Retail and Leisure			
Walkers Arms	450m	5 mins	1 min
Leymoor Cricket Club	450m	6 mins	1 min
Thorpe Green Bowling & Social Club	550m	8 mins	3 mins
Co-op Food – Leymoor Road / Golcar Fisheries	650m	9 mins	2 mins
The Fifth Fiend Taphouse	800m	11 mins	3 mins
HAVELI Huddersfield	1300m	20 mins	7 mins
Golcar Library	1300m	20 mins	7 mins
Aldi	2100m	28 mins	7 mins
HD3 Fitness Centre	2200m	36 mins	19 mins
Employment			
Colne side Business Park	2500m	32 mins	7 mins
Huddersfield Royal Infirmary	3300m	-	19 mins

(Source: Google Maps)

Table 3.1: Proximity to Local Amenities (N.B. taken from submitted Transport Statement)

- 3.22 The Transport Statement also identifies that there are a number of bus stops within a short walk of the site, the closest of which are located approximately 500m south of the site (7-

minute walk or 2-minute bike), on Leymoor Road. From these stops, regular Monday-to-Sunday services are available, providing connections between the site and Huddersfield town centre (an approximate 30-minute bus journey from the site), from which additional local and national bus services can be accessed. Rail services can be accessed from Huddersfield town centre also, as well as from Lockwood, the latter of which is in slightly closer proximity.

3.23 With regards to cycling, the Transport Statement confirms that:

'It is anticipated that cycling might be an attractive travel option for future residents of the site. The road network in the vicinity of the site is considered to be suitable for cyclists.'

4. Conclusions

- 4.1 For the reasons set out within this Design and Access Statement, it is assessed that the proposed development would accord with all design-related Local Plan policies and guidance identified in the Planning Statement submitted as part of this application, as well as National Planning Policy Framework section 12.

- 4.2 Specifically, the proposals would include a mix of dwelling types and sizes, all of which would be constructed from an attractive palette of materials suitable for the locality. Furthermore, vehicular access would be achieved in a safe and straightforward manner, and a suitable quantum of parking would be provided. The development would also be accessible via sustainable means.

- 4.3 For all the above reasons, the proposals represent a suitable form of development from a design and access perspective.