

P & S Consulting Engineers
Limited

Fearnley Street/Fall Lane/Brooks
Yard,
Westtown, Dewsbury, WF13 2SW
Stage 1 Road Safety Audit



Control Sheet

CLIENT: P & S Consulting Engineers Limited
PROJECT TITLE: Fearnley Street/Fall Lane/Brooks Yard,
 Westtown, Dewsbury, WF13 2SW
REPORT TITLE: Stage 1 Road Safety Audit
PROJECT REFERENCE: 164318
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	Prepared by		David Colley		A signed copy is available on request		24/01/2025	
	Checked by		Ashley Armitage				24/01/2025	
	Approved by		David Colley				24/01/2025	
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Approved								

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Acknowledgements

Reference has been made to Crashmap to check the accident history on the local highway network;

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Stage 1 RSA Mark-up plan.

Project Summary

Report title:	Stage 1 Road Safety Audit
Date:	24 January 2025
Document reference and revision:	164318-002-01
Prepared by:	Sanderson Associates
On behalf of:	P & S Consulting Engineers Limited
Overseeing Organisation:	Kirklees Metropolitan Borough Council

1. Introduction

- 1.1 Sanderson Associates Consulting Engineers has been appointed by Sanderson Associates on behalf of P & S Consulting Engineers Limited to carry out a Stage 1 Road Safety Audit (RSA) of the proposed highway works associated with the temporary access road and service protection design on the footway crossover for the Trans-Pennine Route Upgrade. The site is located off Fearnley Street/Fall Lane/Brooks Yard, Westtown, Dewsbury WF13 2SW.
- 1.2 Sanderson Associates were instructed by Matt Holmes of P & S Consulting Engineers Limited on the 6 November 2024 to proceed with the Stage 1 Road Safety Audit. A Stage 1 Road Safety Audit Brief (Ref 164318-001-04) has been prepared by Sanderson Associates. The Audit Brief was subsequently signed by Dominic Wing of Kirklees Council dated 15 January 2025. The Highway Authority is Kirklees Metropolitan Borough Council. The Audit Team undertook a previous Stage 1 Road Safety Audit Brief ref 161464-002-01 in June 2024 but since that time there have been some changes to the design details of the service protection measures on the temporary access road footway crossover. The Highway Authority has requested that the new proposals are subject to a further Stage 1 RSA.
- 1.3 The Audit Team members are as follows:
- Audit Team Leader – David Colley MCIHT, Associate Director at Sanderson Associates
 - Audit Team Member – Ashley Armitage MIHE, Assistant Engineer at Sanderson Associates
 - Tom Dunlop Engineer at Sanderson Associates also attended the site visit as an observer.
 - Simon Burkinshaw FIHE Principal Engineer at Sanderson Associates also attended the site visit as an observer.
- 1.4 The purpose of this Stage 1 RSA is to consider the highway works associated with the temporary access road and services protection design shown on drawings reference 151667-TSA-W3B-MVL3-DRG-C-CV-000046 Rev. C3 and W3B-T-073 P02 and the connection to Fall Lane and junction visibility shown on drawing 161464-01 rev A.
- 1.5 The following drawings and information have been used for Audit purposes. No details of any supplementary street lighting have been provided.
- 151667-TSA-W3B-MVL3-DRG-C-CV-000046 Rev. C3 – 26.09.2024 - Temporary haul road and services protection design – Rev C3 Services Protection Changed
 - 7722-30 Rev. C0 - 05.02.2024 - Brooks yard resident temporary access
 - 51667-TSA-W3B-MVL3-DRG-C-CV-000295 Rev. P03 - 12.11.2024 - Temporary Haul Road – Long Section
 - 151667-TSA-36-MDL1-DRG-D-HW-078503 Rev. P03 - 19.04.2024 - MDL1-9 Thornhill Rd & Fall Ln Proposed Highway Geometry General Arrangement
 - 151667-TSA-35-MDL1-DRG-R-SE-021108 Rev. P01 - 03.09.2021 - W3 Lineside Civils Accesses Fall Lane Road RRAP General Arrangement
 - 161464-01 rev A - November 2024 - Visibility Splays

- 151667-TSA-00-TRU-REP-W-EN-001472 - 15.02.2024 - Land off Fall Lane – Provision of temporary parking area for residents and associated access road, Planning Supporting Statement
- K14-9SE/10 - 08.05.2024 - Consultation Response from KC Highways Development Management
- 151667-TSA-00-TRU-BRF-W-MF-000107 Rev C - 23.05.2024 - Technical engineering form TEF16: Request for design services - W3/TRS/002 Rev C – Design Change
- 151667-TSA-W3B-MVL3-CAL-W-CV-000027 - 26.09.2024 - Haul Road & Services Protection Design Rev cc2 - 26/09/2024 - services protection changed
- 151667-TSA-W3B-MVL3-CRF-W-CV-000016 - 26.09.2024 - Temporary Works Design Certificate 7722-26-CHK3
- 151667-TSA-W3B-MVL3-RSA-W-CV-000125 - 09.10.2024 - Design Risk Assessment - Temporary Haul Road & Services Protection Design
- Draft Thornhill Road Closed, Fall Lane Access only for Residents (unnumbered traffic management plan)
- A Stage 1 Road Safety Audit Brief (Ref 164318-001-04) signed by Kirklees Council dated 15 December 2025.

- 1.6 Reference to the Audit Brief under the Collision Data section there have been no recorded accidents in the vicinity of the access on Fall Lane in the 5-year period 2018 to 2022.
- 1.7 The Audit took place on site on Wednesday 22 January 2025 in the morning. During the site visit the weather was dry after previous rain and was overcast. The road surface was damp. The current phase of Traffic Management (TM) is expected to be in place until June 2025 and closes the existing rail bridge to through traffic in both directions but still allows pedestrian access. Cyclists are advised to dismount and use the pedestrian route (along Fall Lane) although there are cycle diversions for the Spen Valley Calder Valley Greenway from the western end of Fall Lane and also on the approach along Thornhill Road from the south. Access is allowed for residents and for construction purposes. Residents can access along Fall Lane from the west and leave following the route to the east and along Thornhill Road to join the A644. No vehicular access under the rail bridge is permitted as part of the TM. Cyclists were observed travelling in both directions to and from Fall Lane to Thornhill Road.
- 1.8 The terms of reference of the Road Safety Audit are as described in GG119. The team has examined the works and reported only on the road safety implications of the scheme as presented and has not examined or verified the compliance of the works to any other criteria.
- 1.9 All of the problems described in Section 2 of this report are considered by the Audit Team to require action in order to improve the safety of the scheme. However, any recommendation included within this report should not be regarded as being a prescriptive design solution to the problem raised. They are intended only to indicate a proportionate means of eliminating or mitigating the identified problem. It is noted that there may be alternative methods of addressing a problem that would be equally acceptable in achieving the desired elimination or mitigation and these should be considered when responding to this report.

- 1.10 A marked up plan is included in **Appendix A**, which identifies the approximate location of problems that have been raised. General problems or those with multiple locations have not all been shown.
- 1.11 Following the completion of the Road Safety Audit, the design team should prepare a '*Road Safety Audit Response Report*' in collaboration with the Overseeing Organisation. The response report should incorporate the following:
- Decision Log spreadsheet, where each Problem and Recommendation in the Road Safety Audit report is reiterated;
 - In the Decision Log, a response should be provided by the Design Team and then by the Overseeing Organisation for each problem raised. This should then be followed by an agreed action.

Further information to assist the preparation of the Road Safety Audit Response Report is provided in **GG119 Sections 4.11 to 4.19 and Appendix F** (that includes a Road Safety Audit Response Report template). In accordance with GG119, the response report should be produced and finalised within one month of the issue of the Road Safety Audit report. A copy of the response report should be issued to the Road Safety Audit Team for information.

2. Items Raised for this Stage 1 Road Safety Audit

- 2.1 The problems that are raised in the Stage 1 Road Safety Audit are presented in this section. A copy of a marked-up plan showing Stage 1 RSA problem locations is included in **Appendix A** of this Audit Report.

Problem 1.1

Location: Temporary Haul Road connection with Fall Lane carriageway.

Summary: An ACO drain has been shown to intercept surface water on the haul road running over the footway and into the carriageway but details of the outfall are not shown.

Whilst it is recognised that this is a stage 1 RSA no details of the proposed surface drainage outfall have been provided. The concern is that if adequate drainage is not provided it could lead to skid or loss of control incidents in the carriageway during periods of inclement weather and where vehicles could be braking to negotiate the traffic signal junction (when in operation).

Recommendation: provide appropriate drainage and outfall to intercept surface water before it discharges off the haul road into the carriageway. It should be ensured that the existing gully just to the west is in effective working order.

Problem 1.2:

Location: Temporary Haul Road as it crosses Fearnley Street.

Summary: An ACO drain has been shown along Fearnley Street across the haul road to intercept surface water running from it but no details of the outfall are shown. Construction work for the Fall Lane road realignment is well advanced and the proposed works would cross Fearnley St. However, at the time of the site visit Fearnley St was being used for the connection to Fall Lane whilst the dropped footway crossover was being constructed at the end of the temporary haul road access.

Whilst it is recognised that this is a stage 1 RSA no details of the proposed surface drainage outfall have been provided to prevent surface water on the haul road (which slopes to Fearnley Street) running onto the carriageway. The next highway gully is at the Fearnley Street / Fall Lane junction. If this gully is not effective in intercepting all surface water and some could overshoot into Fall Lane where there is only one further gully before the signalised junction area. The concern is that if adequate drainage is not provided it could lead to skid or loss of control incidents in the carriageway during periods of inclement weather and where vehicles could be braking to negotiate the traffic signal junction (when in operation). It is recognised that Fearnley St will be cut by the new alignment with a new junction being provided as part of the permanent works.

Recommendation: Provide appropriate drainage and outfall to intercept surface water before it discharges off the haul road / Fearnley Street into the Fall Lane carriageway. It should be ensured that the existing gullies on Fearnley Street and Fall Lane are in effective working order until Fearnley St is cut by the new junction works.

Problem 1.3:

Location: Temporary Haul Road connection with Fall Lane carriageway.

Summary: Drawing 151667-TSA-W3B-MVL3-DRG-C-CV-000046 Rev. C3 shows the swept path of a 7.5tonne vehicle associated with deliveries to the Shepherds Boy public house although it appears to be closed. The swept path crosses into the contra flow cycle lane on the southern side of Fall Lane.

Traffic Management (TM) has now been installed on Fall Lane and Thornhill Road with the temporary haul road in use by residents. The current phase of Traffic Management is expected to be in place until June 2025. At the time of the site visit the new dropped kerb crossover was under construction and residents were using the original alignment of Fearnley St to join Fall Lane. The TM on Fall Lane comprises of tapered hatch markings up to the haul road to guide traffic away from the northern kerb line and the closed section of carriageway circa 1m wide further to the east. In addition, plastic bollards were to be provided along a new solid road marking separating the contraflow cycle lane. However, the plastic bollards had been removed as the temporary traffic management called for cyclists travelling in both directions to dismount and use the temporary pedestrian route. Whilst it is recognised that the public house is closed and Traffic Management is in place the concern is that if vehicles, similar in size to the 7.5t vehicle, needed to gain access it would have to cross the line of bollards, dislodging them creating a hazard for road users on Fall Lane and potentially into the path of cyclist. This could lead to sudden braking, skidding or loss of control incidents in the carriageway during periods of inclement weather or conflicts with cyclists.

Recommendation: Manage the egress of larger vehicles and turning into the TM lane together with temporary removal and replacement of the bollards.

Problem 1.4:

Location: Temporary Haul Road connection with Fall Lane carriageway.

Summary: The Temporary Haul Road joins Fall Lane where it is one way and users of the Temporary Haul Road would have to turn left only in the same manner as the existing Fearnley Street junction.

The concern is that if a vehicle was to try and turn right it would do so head on in to traffic proceeding from the west in the correct direction of the one-way system. This could lead to head on collisions and sudden braking together with skidding or loss of control incidents in the carriageway during periods of inclement weather or conflicts with contra flow cyclists.

Recommendation: provide appropriate road markings and signage to direct drivers to turn left out of the Temporary Haul Road.

3. Audit Team Statement

3.1 We certify that the terms of reference of the audit are as described in GG119.

Audit Team Leader:

David Colley MCIHT

Highways England Approved RSA Certificate of Competency

Associate Director at Sanderson Associates Consulting Engineers

Signed:

Dated: 24th January 2025

Audit Team Members:

Ashley Armitage MIHE

Assistant Engineer at Sanderson Associates Consulting Engineers

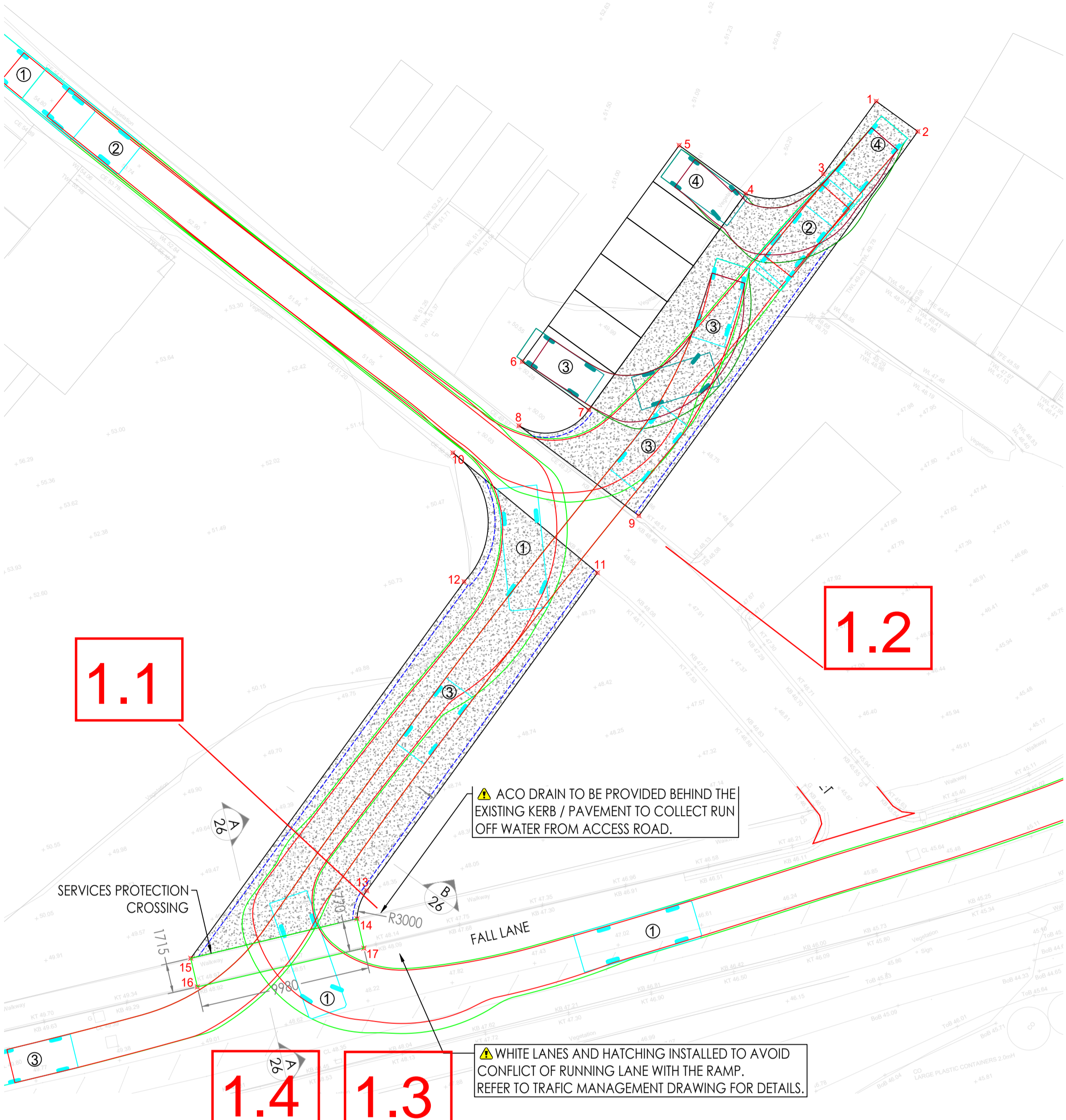
Signed:

Dated: 24th January 2025

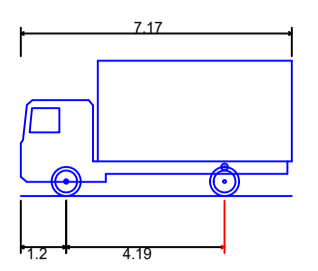


Appendix A

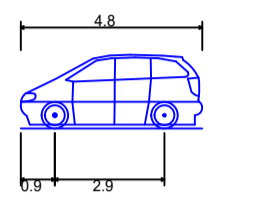
Stage 1 RSA Mark-up plan.



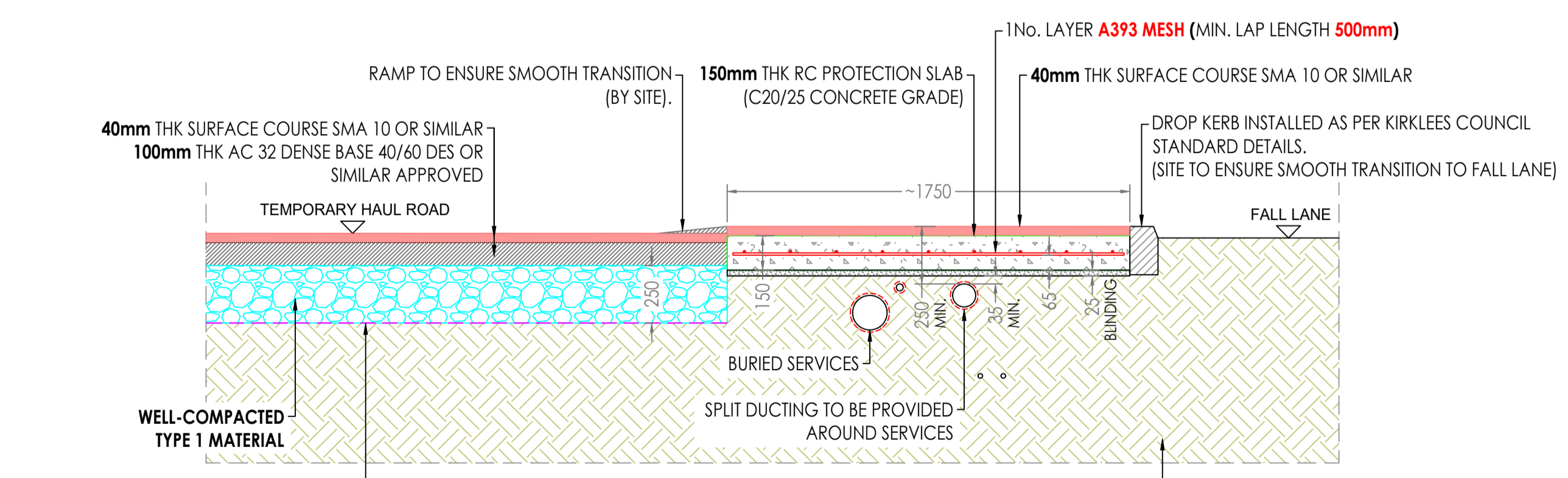
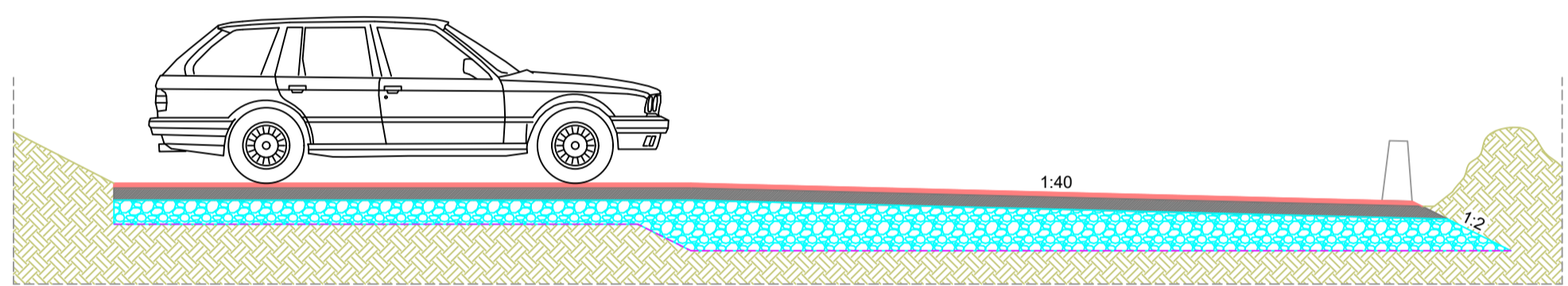
HAUL ROAD LAYOUT
1:200



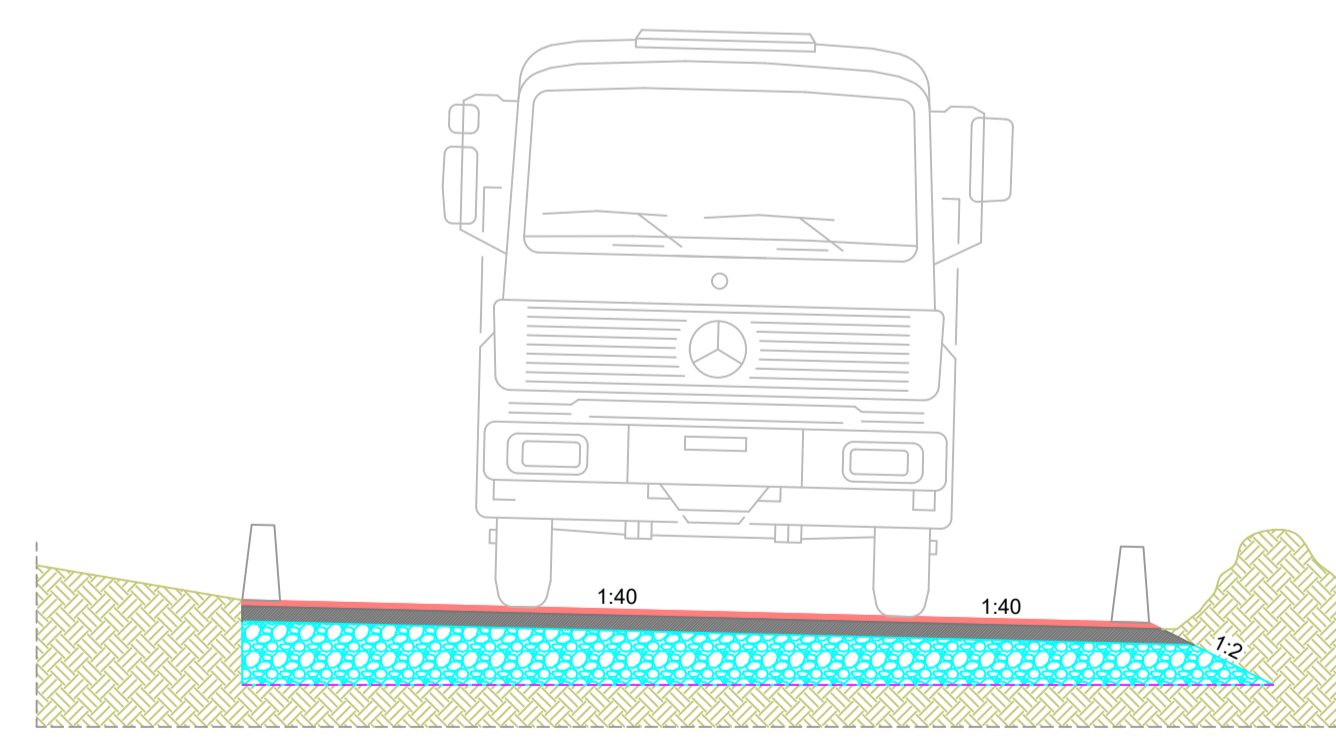
FTA Design 7.5 Tonne Rigid Vehicle (2016)
 Overall Length 7.170m
 Overall Width 2.300m
 Overall Body Height 3.580m
 Min Body Ground Clearance 0.375m
 Track Width 2.120m
 Lock-to-lock time 3.00s
 Curb to Curb Turning Radius 7.000m



Standard Design Vehicle (SDV)
 Overall Length 4.800m
 Overall Width 2.000m
 Overall Body Height 1.950m
 Min Body Ground Clearance 0.100m
 Track Width 2.000m
 Lock-to-lock time 4.00s
 Wall to Wall Turning Radius 6.000m



SECTION A-A
1:20



CONCRETE GRADE TO BE MIN. C20/25

COVER TO REINFORCEMENT:
50mm TO SIDES

MAXIMUM PRESSURE UNDER RC PROTECTION SLAB $\sigma=20kPa$.
MUST BE APPROVED BY THE UTILITY OWNER.

Road Safety Audit St 1
Marked up plan



Sanderson House, Jubilee Way, Grange Moor,
Huddersfield, WF4 4TD



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