



Huddersfield Railway Station Proposed Access Improvements

BC.HUD.2023.00091.002

Design, Access and Heritage Support Statement Revision 002 – April 2024





REVISION SCHEDULE

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1.00 Design, Access and Heritage Support Statement

1.1.0 Introduction & Statement of Intent

1.1.1 This statement has been written to support the information submitted as part of the Listed Building Consent application in relation to the proposed access improvement works to the main entrance at Huddersfield Railway Station on behalf of the applicant, TransPennine Express (TPE).

1.1.2 Huddersfield Railway Station is the second busiest station in the West Yorkshire region seeing almost 5 million passengers annually. The station carries significant historic value and has not been adapted to provide a suitable level of access. Accessibility to the station is particularly challenging to those with an impairment. In the UK nearly two million people live with sight loss, and face a much higher risk at train stations. Proposed access improvements to the station entrance will provide an approach that is sympathetic to the historic building whilst addressing the existing issues with accessibility to and from the station.

1.1.3 Huddersfield Railway Station is a Grade I Listed Building under the Planning (Listed Buildings and Conservation Areas) Act 1990 for its special architectural and historic interest. The aim of this report is to outline the rationale behind proposed access improvements and the impact of them on the significance of the existing building. This report will include a review of the stations building and layout, the station history, details of the listing, background to the development proposals, an overview of the proposed works, a design statement, details of sustainability and construction waste, a drawing register, and photographs.

1.1.4 TransPennine Express have shown their commitment to this area through investment in projects at this and other local railway stations and they understand the importance and the need for these improvements to the station. This is also a business plan commitment to deliver access improvements at this railway station.

1.1.5 AHR Building Consultancy Ltd (AHR) have undertaken previous design works at several Listed railway stations occupied by TransPennine Express and are aware of the historic status of this railway station and its primary significance on the route.

1.1.6 Further information has also been prepared through development of the design and a detailed drawing package is included as part of this application to demonstrate the full proposals. The drawings include architectural plans, layouts and elevations in order to show the proposals in detail.

1.1.7 The address of the station and site involved in this application is as follows:

Huddersfield Railway Station,
St. George's Square,
Huddersfield,
West Yorkshire, HD1 1JB

1.1.8 This report should be read in conjunction with the drawings submitted with the Listed Building application and detailed in Section 13 of this report.

1.2.0 Huddersfield Railway Station

1.2.1 Huddersfield Railway Station is located on St. George's Square in Huddersfield town centre and is managed by TransPennine Express. The station is served by TransPennine Express trains together with Northern Trains services. The station is staffed 24 hours a day with a ticket office operating 05.45am to 20:00pm daily. Ticket vending machines are also available inside the entrance hall.

1.2.2 Image 1 shows how the station and the railway line it serves are orientated, serving trains from both a north-east and south-west axis. The station is located with a central entrance along the

western border of St. George's Square that provides pedestrian access. Adjacent land uses include the neighbouring Grade II* Listed George Hotel and Britannia Buildings serving residential and commercial uses. Site parking is provided to the north of St. George's Square.

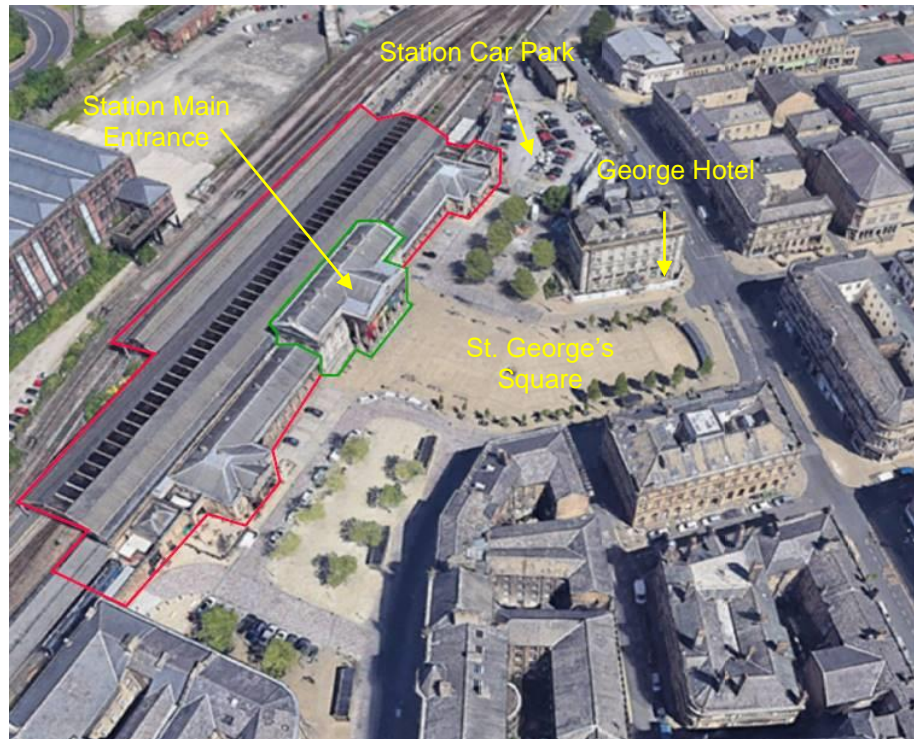


Image 1 – Huddersfield Railway Station – St. George's Square (Source: Google Maps)

1.3.0 Station Building and Layout

1.3.1 The station consists of eight platforms covered by a large canopy, serving trains from both TransPennine Express and Northern Trains. Upon entering the two-storey entrance into the station concourse there is a booking office to the left, with train timetables and newsagents to the right. Passing through the station concourse leads to platform one. Platform two is located to the south of platform one, with platforms four, five, six, and eight reached via a subway which can be accessed by either steps or lift from platform one. Two pubs are located on each wing of the station's frontage 'The Head of Steam' to the south and the 'King's Head' to the north.



Image 2 – Main entrance



Image 3 – Adjacent St. George Hotel

- 1.3.2 The main entrance to the railway station is located on the western side of St. George's Square facing the opposite Lion Buildings (Image 5). Central double doors and two side doors are located at the top of the entrance podium, providing access to the station concourse. Stepped stonework and a ramp provide access to the podium entrance from St. George's Square.
- 1.3.3 Pedestrian drop-off and pick up is provided to the north of St. George's Square with accessible parking located to the southern wing of the façade (Image 4). Five steps of stonework are present to the station entrance of which the bottom step undulates with the surface of St. George's Square. No handrails are currently present to the stepped entrance route, ramped access is provided with a full height rail at either side.
- 1.3.4 The central entrance hall gives access to information points, ticket stalls, waiting area and retail amenities. Passenger access to platform one is provided through three glazed automatic double door sets. Other access points are provided however limited to staff and emergency exit usage.



Image 4 – Taxi drop off / pick up area



Image 5 – View of St. George's Square

1.4.0 Station History

- 1.4.1 Huddersfield Railway Station provides access to train services located in Huddersfield town centre. Residents of Huddersfield, prior to the construction of the railway station, were forced to travel to the nearest station at Cooper Bridge to access the Manchester and Leeds Railway line that had opened in 1841 and was the first railway line to cross the Pennines between Lancashire and Yorkshire. The Huddersfield Railway Station was designed by architect James Pigott Pritchett of York and constructed by the Joseph Kaye building firm between the years 1846 and 1850, in a neoclassical design with Corinthian columns. The station was partly opened for the commencement of services in 1847 but not completed until 1850 with the station only having one platform until 1886 when the roof over the tracks had been completed. The station originally served the Lancashire and Yorkshire Railway (L&YR) and Huddersfield and Manchester Railway and Canal (H&MR) Companies, whose booking offices were situated at either end of the station's façade (where the two station pubs are located at current date).
- 1.4.2 As constructed in circa late 1840's the station has a two-storey central entrance block with single storey wings either side and a portico of Corinthian columns. The building is constructed of sandstone ashlar with a pitched tile roof in a neoclassical design. The central block has a giant composite palistrade on high plinth, with full dentilled and modillioned entablature. A central, free-standing pedimented portico holds a turret-clock in tympanum. The station was successfully extended in 1886 with the completion of a canopy over the tracks with a central island that housed elaborate refreshment rooms that functioned until at least 1883
- 1.4.3 Huddersfield Railway Station is widely claimed to be the finest classical station in Britain, described as 'a stately home with trains in' by John Betjeman. The station is Grade I Listed Building and lies within the Huddersfield Town Centre Conservation Area.

- 1.4.5 The station is situated to the west of St. George's Square which was refurbished in 2009 to create a pedestrian zone with new fountains, and new access to the car park and a taxi waiting area. In 2010 Network Rail and TransPennine Express completed a series of improvements which provided improved access for passengers around the station, a new staircase was constructed from Platform one to the subway and new lifts were added at either end of the subway.



Image 6 – Huddersfield Conservation Area
(Source, Kirklees Council)



Image 7 –Huddersfield Station
(Source, Historic England)

- 1.4.6 The station is currently included within the plans for the TransPennine Route Upgrade. Plans for the upgrade include the restoration of the tearoom and station roof. Capacity is set to be increased with the addition of two new platforms and the extension of existing. Improved accessibility is also included to provide ease of access in and around the station.

1.5.0 Listing

- 1.5.1 Historic England identifies the station as a Grade I Listed Building, as follows:

Name: Huddersfield Railway Station

List Entry Number: 1277285

Date First Listed: 03-Mar-1952

Details: St. George's Square Railway Station. 1846-50. Architect JP Pritchett of York. Sandstone ashlar. Pitched tile roof. Two-storey central block with one-storey wings. Central block has 11 bays, articulated by giant Composite pilastrade on high plinth, with full dentilled and modillioned entablature. Central five bays have free-standing pedimented portico, two bays deep, with clock in tympanum. Inside portico, central three bays break forward for one bay. Podium of five steps. Central double doors with six moulded panels. End elevations are three bays deep and take pediments. Wings have nine bays each, fronted by open Composite colonnades, on three-step podia and less high plinths: full entablature. Third bay on north side has doorway with moulded surround and cast iron gates of plain but elegant geometrical design.

Terminal blocks of five bays each, breaking forward one bay in front of colonnade, astylar but continuing the entablature of colonnade. Central three bays have free-standing portico, one bay deep, with no pediment but a solid panelled parapet in front and balustrades at sides. Elaborate scrolled consoles flank balustrades and two are placed above parapet flanking armorial badges inscribed "Huddersfield and Manchester Railway and Canal Company", at north end, and "Lancashire and Yorkshire Railway Company," at south end. South block is continued south by four more bays of wall with same entablature: first bay has double doors with moulded surround, next three have plain blind panels.

Detached block north of north terminal block 11 bays, first three have plain blind panels and full entablature, next five break back and have attic storey, last three also have attic and break back still further, and ground floor of these last eight is masked by a rock-faced stone lean-to. Platform elevation has, from north to south, as follows: one door with six moulded panels and fanlight, one sash with sunk and panelled apron, one door with fanlight, one sash with sunk and panelled apron, one door with four moulded panels and fanlight, one door with six moulded panels and fanlight, two sashes with sunk and panelled aprons, one door with six moulded panels and fanlight, one sash with sunk and panelled apron, two open passageways, one door with six moulded panels and fanlight, two sashes with sunk and panelled aprons, one door with eight moulded panels and fanlight, three pairs of panelled doors in pilastered wooden frames with fanlights and side lights, two sashes with sunk and panelled aprons, two blocked doors with fanlights, one sash with sunk and panelled apron, one door with six moulded panels and fanlight, three sashes with sunk and panelled aprons, one modern door with fanlight, one sash with sunk and panelled apron, one door with six moulded panels and apron, one canted wooden bay with pilasters and full entablature, one sash with sunk and panelled apron, one door with six moulded panels and fanlight, one sash with sunk and panelled apron, one open passageway and one modern door. All windows are sashes with glazing bars, in plain surrounds on platform side, in moulded surrounds on street side; those to central block have cornices on ground floor, and pediments on first floor.

Parcels office has two and Booking Office one fluted cast iron Tuscan columns supporting ceiling. Tiled Art Deco ticket kiosk with bronze mullions and case racks. Platforms are covered by one very wide and one other wrought iron trusses with elaborate bosses at intersection of bracing members. Original supports have been replaced or reinforced, except for two on platform 4 which are columns with elaborately moulded bases and colectic capitals. Buffet and Waiting Room between platforms 4 and 8 is a separate match-boarded structure with panelled pilasters, each taking paired brackets and cornice. Steps down to underpass between platforms 4 and 8 have wooden handrails and cast iron balustrade with turned newels, both around stair well and dorm centre of steps. Massive stone paving slabs in underpass, and patent wooden non-slip steps down.

History: The grandeur of the station is the result of its being built at the joint expense of the Huddersfield and Manchester Railway and Canal Company (absorbed by the LNWR in July 1847) and the Manchester and Leeds Railway. The former built the line, and planned to extend it to Leeds via Dewsbury. The latter, having failed to win this concession, needed running rights to connect their main line at Cooper Bridge with their subsidiary from Springwood Junction to Sheffield.

The foundation stone was laid by Josh Fitzwilliam, the Lord Lieutenant, on 9 October 1846, when a public holiday was declared and church bells were rung from dawn till dusk. It was partly opened for the commencement of services in August 1847, but not completed until October 1850. It had only one platform until October 1886, when the roof over the tracks, which had been begun in 1878, but had collapsed in course of construction in August 1885 (killing 4 men), was finally completed. The central part housed elaborate refreshment rooms which functioned until at least 1883.

1.5.2 Detail taken from: <https://historicengland.org.uk/listing/the-list/list-entry/1277385> - list entry summary.

1.5.3 Having reviewed previous Listed building applications for the railway station we are aware of the most recent planned works, their impact on the railway station and to aid our understanding of the history of the area. A summary of the most recent applications is as follows (note that none of these were submitted by AHR):

- 2024/90044/LBC – Platform alterations and extensions.
- 2023/93177/LBC – Platform alterations and extensions.
- 2023/93173/LBC – Temporary installation of artworks.
- 2023/93044/LBC – Platform alterations and extensions.

1.6.0 Background to the Development Proposals

- 1.6.1 The submitted proposals reflect TransPennine Express' wish to enhance the accessibility for customers. The rationale behind improving accessibility is to create an environment that is safe, efficient and integrated, reducing the risk of accidents and allowing people to easily access the station that is required for jobs, education, and other key services. The station currently lacks the necessary amenities required to provide safe and efficient access that many people require.
- 1.6.2 Our designs will aim to provide simple proposals that can be implemented in a manner that has minimal effect to the buildings heritage yet is effective in addressing the need for improved access to the station. Incidents of passengers tripping / falling is commonplace at the station entrance and exemplify the need for access enhancements.
- 1.6.3 The design process involved a site survey in preparation for the development of initial design proposals for approval by TransPennine Express and consultation with the Kirklees Council Conservation Officer. Some of the key issues identified during our visit were the lack of wayfinding, particularly in relation to the access ramp which is not visible from the drop off area nor upon exiting some of the doorings. The access ramp is however well located adjacent to the accessible parking spaces.
- 1.6.4 The proposed access improvements will be installed by specialist suppliers capable of producing bespoke products to that of the designs specified within this report.

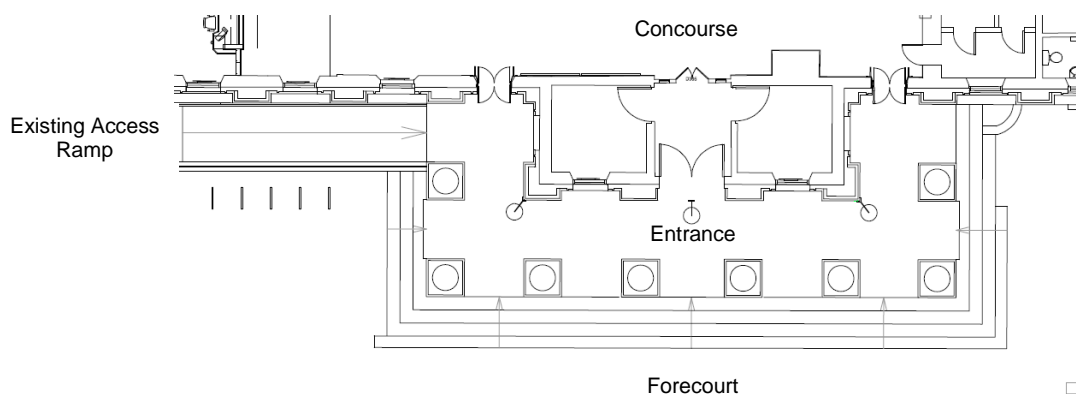


Image 8 – Existing entrance plan

- 1.6.5 The original project brief prepared by TransPennine Express was to develop options for the improvement of accessibility for the entrance to Huddersfield Railway Station. The main requirements were as follows:
- Site briefing meeting/visit/survey
 - Preparation of initial sketch options for further discussion
 - Client design review meeting and updates
 - Site meeting with Conservation Officer and liaison
 - Brief report following discussions and outcome.

- 1.6.6 Consequently, a proposal was developed for a series of access improvements that incorporate the above-mentioned requirements and is described in more detail below.

1.7.0 Proposed Works

Overview of Proposed Works

- 1.7.1 As mentioned previously, station access is currently lacking provisions to allow for safe and inclusive access. The entrance is the only passenger access and egress routes to the station. Our initial ideas to improve access were as follows:

- Tactile paving to bottom steps in 'newer' paving.
- Existing ramp – possible double handrail.
- Explore possible tactiles to top of steps which won't impact on stonework – rods.
- Handrails to steps at end elevations only, to be symmetrical and link in with the ramp handrails – note the heritage style.
- Look at possible stonework restoration, pointing and reinstatement of nosing dots.
- Additional wall lights – in a style to match the others and carefully positioned for symmetry.
- Signage near the drop off area to highlight where the current access ramp is.

1.7.2 The station entrance forms part of the Grade I Listing, the proposed works aim to improve access by providing reasonable adjustments to ensure that people are not put at a disadvantage when accessing the station and associated services.



Image 9 – Proposed visual of new installations

Proposed Handrails – Ramped Access – Low-height Rail

- 1.7.3 The ramp currently has handrails at a height of 950mm. Low-height handrails should be provided to buildings as specified within BS 8300. The installation of second handrails at a height of 600mm provides support at a lower height.
- 1.7.4 Huddersfield Railway Station is Grade I Listed, the installation of a handrail should have limited impact to the architectural significance of the building. Ramped access at Huddersfield Railway Station currently has full height handrails of an ornate style in place at either side of the ramp. One side is affixed to the façade of the station and the other is mounted with support posts as shown in the below image.
- 1.7.5 The handrail will be fixed to a height between 550mm and 650mm extending at least 300mm from the top and bottom of the ramp landing. Fixings of the handrail are to comply with BS EN 1991 – 1 -1.
- 1.7.6 An additional upper handrail will also be provided on the outer side. This will be directly above the new lower handrail for ease of use and to prevent over reaching.

Proposed Handrails – Side Elevations

- 1.7.7 Handrails should be provided to stepped access as specified in Approved Document B (Building Regulations). There are currently no handrails present to stepped access at the station entrance. As the building is Grade I Listed any proposed works to implement handrails should be assessed in relation to the impact they may have on the buildings architectural significance.
- 1.7.8 Stepped access at the station differs in riser height, presumably due to the age of stone. This

proves difficult for many people who have a physical difficulty in negotiating changes in level. Huddersfield Railway Station has had previous incidents of people falling and tripping to stepped access, incidents can be mitigated by introducing handrails (including mid-height rail) to the access route. A handrail to stepped access should be easily gripped, providing sufficient forearm support and comfortable to touch. The station currently operates a passenger assist service, it has been raised that there is currently little facility for passengers dropped off at the station, handrails will significantly benefit users of the service.



Image 10 – Existing handrail to ramped access

1.7.9 Our proposed design includes handrails to side elevations (north and south) only. This is to minimise impact of the stations front elevation view. The handrails are to be of a style to match the existing design of the ramped access handrail.

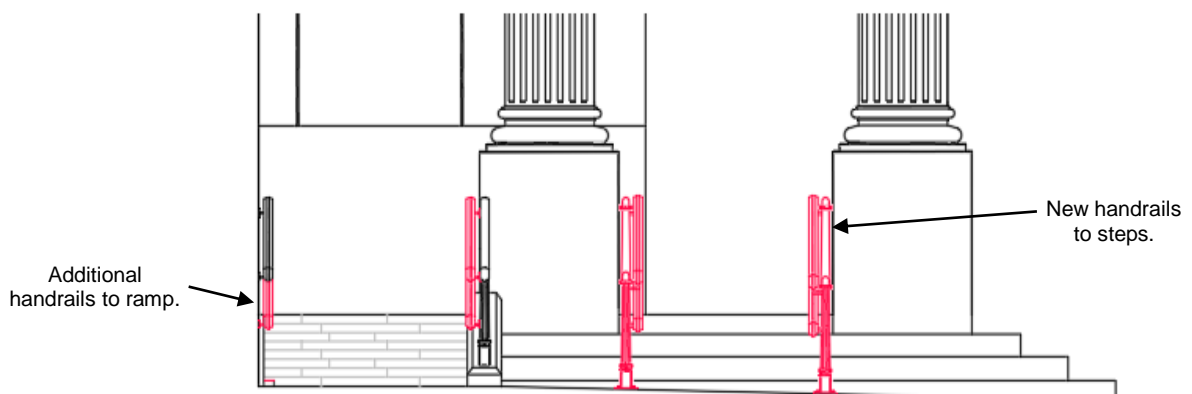


Image 11 – Proposed ramped access handrail (north elevation)

1.7.10 Handrails to side elevations are to be of an ornate style with posts supporting the rail. The top rail will be 1000mm from the step riser. The mid-height rail will be 600mm from the step riser.

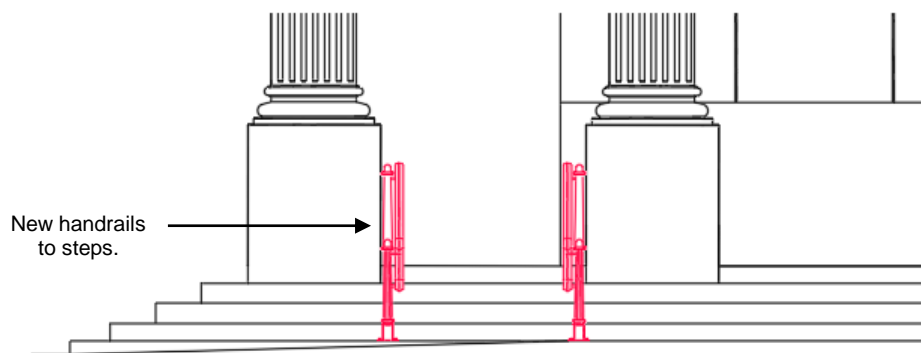


Image 12 – Proposed handrail to stepped access (south elevation)

Signage

- 1.7.11 Accessible entrances should be clearly signed and easily recognisable as specified in BS 8300. We have proposed that a post mounted sign is installed to the passenger drop off area providing direction to the accessible entrance at the opposite end. We have also proposed wall mounted signs to provide directions to ramped access upon exiting the station.
- 1.7.12 Passengers have been observed leaving the station with prams, luggage or in wheelchairs and being unaware of the access ramp to the southern side of the entrance. New signs have been provided inside the station to direct passengers to the exit and ramp, however, these do not continue upon exiting the station concourse. A series of discreet yet clear direction signs adhered to the rear of the column bases would assist wayfinding whilst not being visible from the forecourt.
- 1.7.13 Signage will follow the TransPennine Express design and font.

Lighting

- 1.7.14 Lighting to the station entrance should be improved by providing increased lux levels during low light. There are existing markings to the station façade that are believed to be from previous fixings (see Image 13). Currently there are three ornate globe style lighting units fixed to the station entrance.



Image 13 – Markings to façade from previous fixings

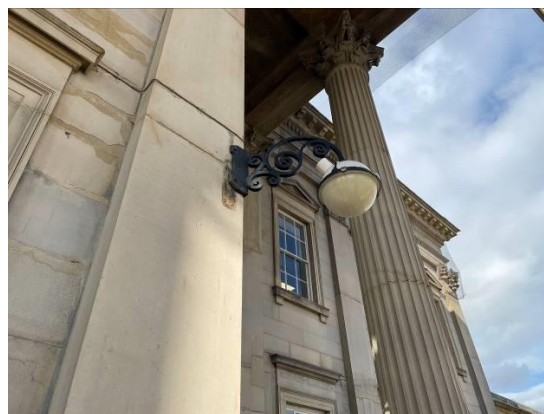


Image 14 – Existing entrance light fittings

- 1.7.15 The proposed works include replacing existing wall mounted globe lights (units only) for three identical units, with four additional lights provided at either side of the entrance. Additional lights will be fixed to areas exhibiting signs of patch repairs at locations shown on the plan view in

drawing HUD-004 – Suggested Access Improvements. Lights and brackets are to be designed and manufactured by DW Windsor or similar, equal and approved, and are to match existing fittings.



Image 15 – Station during low light (*Source: Geograph*)

- 1.7.16 Implementing four additional globe lights to the sides of the stations entrance routes will provide increased lux levels whilst keeping in touch with the stations existing architectural composition. The stations side access routes currently do not receive any direct light from fixtures (as seen in Image 15). Sufficient lighting is an important factor for accessibility to entrances.

Stone Repairs and Tactile Paving

- 1.7.17 Stepped stonework to the station entrance is worn with areas of deteriorated mortar and missing nosing dots - see Image 17. Stepped access in its current state presents the possible risk of trips / falls and requires enhancement for improved accessibility and usability of the station.



Image 16 – Entrance steps



Image 17 – Mortar erosion to steps

- 1.7.18 We have proposed that stonework is repointed and repaired where necessary using a lime

mortar mix, we have also included for the re-instatement of nosing dots. Works to the stonework will improve accessibility by providing a level surface free of spacings and gaps. Repairs and repointing of stone will also improve the visual appearance of the entrance.

- 1.7.19 Tactile paving provides warning of stepped access to people with visual impairments so that they can travel safely and independently. The station currently has no such provision. Proposed works to the entrance will include the implementation of tactile paving to provide adequate facilities for those with visual impairments. We are aware that any implementation of such improvements must be considerate to the building and its architectural / historical significance.
- 1.7.20 Tactile paving is proposed to the bottom of stepped access in the ‘newer’ paving on St. George’s Square. The square had previously undergone major renovation works in 2009. We do not consider that implementing tactile paving to this surface will impact the stations architectural or historic significance. Corduroy paving tiles are to be specified by Johnson Wellfield from a local quarry and to be of a stone that matches existing.
- 1.7.21 Corduroy warning strips are proposed to be implemented on stone flags positioned at the top of the stations entrance steps. Strips are to be held in place with a mechanical fixing to the stone surface.

Summary

- 1.7.22 Developing our designs has included numerous discussions and meetings with TransPennine Express and Kirklees Council. This has allowed us to consider each parties requirements and wishes to ensure a proposal is put forward that meets as many of these requirements as possible.

1.8.0 Planning Policies

- 1.8.1 This section summarises the relevant policy and guidance in consideration of this application. This comprises:
- National Guidance – The NPPF – December 2023
 - Planning (Listed Buildings and Conservation Areas) Act 1990
 - Local Policy Guidance – Kirklees Local Plan

The National Planning Policy Framework (NPPF)

- 1.8.2 The revised NPPF was published in December 2023 and is a material consideration in planning decisions. The NPPF sets out the Government’s planning policies for England and how these are expected to be applied.
- 1.8.3 Relevant paragraphs to the proposal set out within the NPPF are listed below:
- High Quality Design

Section 12 of the NPPF states that the creation of high quality places is fundamental to what the planning and development process should achieve.

Paragraph 130 advises that planning policies and decisions should ensure that developments meet a number of design characteristics including:

- Functioning well and add to the overall quality of the area, not just for the short term but over the lifetime of the development;
- Be visually attractive as a result of good architecture, layout and appropriate and effective landscaping;
- Being sympathetic to local character and history, including the surrounding built environment and landscape setting;
- Establish or maintain a strong sense of place, using the arrangements of

- streets, spaces, building types and materials to create, welcoming and distinctive places to live, work and visit;
 - Optimising the potential of the site to accommodate and sustain an appropriate amount of mixed development (including green and other public space) and support local facilities and transport networks.
 - Create places that are safe, inclusive and accessible, which will promote health and well-being, with a high standard of amenity for existing and future users.
- Heritage

Section 16 of the NPPF sets out the relevant policies in relation to conserving and enhancing the historic environment.

Paragraph 200 of the framework sets out the need for applicants to describe the significance of the heritage asset affected including any contribution made by the setting of the asset. The level of detail should be proportionate to the assets' importance, with an understanding of the proposals impact on the significance of said asset.

Huddersfield Railway Station is a Grade I Listed Building and any proposed works require the upmost regard to the significance of the asset and its surroundings. Our proposals have been developed through a thorough assessment of the station, with all access improvement elements chosen on the basis of a uniform heritage design sympathetic with that of the existing site and area, negating any adverse impact of interventions.

Paragraph 205 states that when assessing the impact of proposed development on significance, great weight should be given to the asset's conservation; irrespective of whether any potential harm amounts to substantial harm, total loss or less than substantial harm to its significance.

Proposed access improvements have been selected with the intention of conserving the significance of the station whilst addressing the need for an inclusive, accessible entrance.

Paragraphs 206 and 208 state that any harm to the significance of a designated heritage asset should require clear and convincing justification, with potential harm weighed against the public benefits of the proposal.

Any potential harm to the significance of the station as a result of proposed works will be justified by the improved accessibility to the station. The station serves the public and as of current does not provide a level of accessibility that can be considered inclusive and safe upon entrance. Our works, formed with the intention of preservation will drastically improve access to the station and any potential harm should be weighed against the much needed improvement in accessibility.

- 1.8.4 The proposals meet these key objectives of the National Planning Policy Guidance, with further discussion on the proposals detailed in section 1.7.0 of this document.

Planning (Listed Buildings and Conservation Areas) Act 1990

- 1.8.5 The Planning (Listed Buildings and Conservation Areas) Act 1990 is an act of Parliament with a particular focus on listed buildings and conservation areas. The act provides control for the demolition, alteration or extension of buildings, objects or structures of particular architectural or historic interest, as well as conservation areas.
- 1.8.6 Section 16(II) of the act provides the objectives in forming a decision on applications. In considering whether to grant listed building consent for any works the local planning authority or the Secretary of State shall have special regard to the desirability of preserving the building or

its setting or any features of special architectural or historic interest which it possesses.

- 1.8.7 Section 72(1) of the act presents the duty that in respect of any building or land within a conservation area, special attention shall be paid to the desirability of preserving or enhancing the character or appearance of that area.
- 1.8.8 Huddersfield Railway Station is located within the Huddersfield Town Centre conservation area. Proposed works incorporate additional lighting units and handrails to the station entrance, these are to be of a heritage style to match the existing units and fixtures currently in place. By continuing the heritage design of elements the proposals will preserve the character of the area whilst enhancing the accessibility and functionality of the entrance.
- 1.8.9 The proposals consider the principles set out in the act, providing accessible provisions that allow for inclusivity, with minimal impact to the architectural and historical features of the building.

Local Planning Policy

- 1.8.10 With respect to the proposed development, the Local Plan for Kirklees Council was adopted in February 2019 and guides the future development of the area in the period up to 2031. The policies below are considered relevant to this application.
- Policy LP24 – Design: good design is fundamental to making places more attractive, sustainable, safe and accessible. The way building and spaces are designed improves the built and natural environment. There is a note within the plan ‘*the needs of a range of different users are met, including disabled people, older people and families with small children to create accessible and inclusive places;*’
 - Policy LP35 – Historic Environment: which sets out a strategy for the conservation and enjoyment of the historic environment. Consideration will be had where the benefits of the proposal outweigh the effect on the heritage asset.
 - Policy LP47 – Healthy, active, and safe lifestyles: which aims to create an environment that supports healthy, active, and safe communities and reduces inequality. It is specifically noted that healthy active and safe lifestyles will be enabled by: ‘creating high-quality and inclusive environments incorporating active design and the creation of safe, accessible and green environments’.

1.9.0 Consultation – Accessibility Support Statements

- 1.9.1 In recognition of the Grade I Listed status and the importance of the station to Huddersfield and the surrounding areas, TransPennine Express and AHR sought early consultation with the Conservation Officer for Kirklees Council. The initial proposals were issued for comment and a meeting was held on site to review in more detail. The feedback received has been incorporated into the application drawings and this document, prior to submission of an application for Listed Building consent.
- 1.9.2 During the site meeting several discussions were had in relation to the proposals. The minutes for which can be found in Appendix B.
- 1.9.3 The following statements of support have been provided from the TransPennine and Northern’s accessibility and integration departments:

“Making accessibility improvements to the front entrance at Huddersfield railway station is a priority for TransPennine Express because there are currently several challenges which customers currently face when using it. Whilst there is a ramp available, it isn’t visible when exiting the station or approaching from the car park or Railway Street so many customers are unaware of it. Regarding the steps, there is no tactile paving in place making them difficult to use for people with visual impairments, and many of the markings on the edges of the steps are missing. There are also no handrails to assist people with steadying themselves, and there is damage to several step nosings. The lighting is also very dim, increasing the difficulties outside

of daylight hours.

There have been several accidents reported on the steps, and we have received feedback from several customers and through our Customer & Professional's Accessibility Panel about the challenges that the entrance poses for older and disabled customers, as well as those travelling with luggage, prams, and push chairs.

Resolving the accessibility issues with the front entrance would help older and disabled people in particular travel more independently and with greater confidence. Resolving the accessibility issues also aligns with the National Rail Accessibility Strategy, supports with our compliance of The Equality Act 2010, and our obligations under the Public Sector Equality Duty. Furthermore, it presents us with an opportunity to work collaboratively with stakeholders including the local authority, Network Rail, and Northern Trains Limited (who also operate train services at Huddersfield railway station) in working towards an outcome where accessibility and heritage are both presented in harmony."

Chris Jeffery
Accessibility & Transport Integration Manager
TransPennine Express

"I would offer my full support for your application on behalf of Northern. The work you are proposing is very much in line with the work we are delivering to achieve accessibility standards across our estate, and it would be a shame if Huddersfield, a key location for us both, were to lag behind the wider industry work to make our estates more accessible. I understand that the heritage status of the station needs to be respected, but this shouldn't be at the expense of making the station as accessible as possible."

Matt Wilson
Travel Integration & Accessibility Manager
Northern Trains

1.9.4 From these statements it is clear Huddersfield Railway Station is lacking adequate provisions for access. Our proposals aim to address these issues whilst being sympathetic of the station's heritage.

1.10.0 Design Statement

1.10.1 AHR Building Consultancy have undertaken previous design works at a number of stations occupied by TransPennine Express. We have also undertaken adaptations of other Listed Buildings across the rail network and are aware of the historic status of the railway station and overall network. The proposals detailed within this report have been developed in conjunction with Kirklees Council and TransPennine Express.

1.11.0 Access Statement

Approach to Inclusion

1.11.1 Within the services sector, integration and accessibility for visitors with disabilities is firm government policy. This policy places a duty on service providers and the aspiration and expectations of the general public. Implementation of this policy requires barrier free environments and support mechanisms to deliver services and manage resources.

1.11.2 The implications of the Equality Act are that service providers should make 'reasonable adjustments' to change the physical features of their premises so that there are no physical barriers which prevent the services from being used or make it unreasonably difficult to do so.

1.11.3 Improvement works will make reasonable provision to ensure that the building is accessible and usable. People, regardless of disability, age or gender, should be able to gain access to the building and to gain access within buildings and use their facilities. This will, however, be

subject to the restrictions of the existing building layout and fabric.

The Equality Act 2010

- 1.11.4 The Equality Act 2010 provides a legal framework that protects the rights of individuals and advances the equality of opportunity for all. People and organisations who own, manage and occupy historic buildings in England and have duties under the service provider, employer, education and other provisions of the act, need to ensure that they do not discriminate against people with protected characteristics.
- 1.11.5 The act provides a duty to service providers to take positive steps ensure that disabled people can access services at a standard that is as close as possible to that offered to the public at large.
- 1.11.6 Proposals to the station entrance ensure that the station is easily accessible to all by providing the reasonable facilities to the required standard for access.
- 1.11.7 Access improvements will include accessible handrails to ramped and stepped access, repointing and stonework repairs to stepped access, improved entrance lighting, additional signage to the station entrance, and additional handrails to north and south elevations.
- Summary
- 1.11.8 The management of access within the building is understood to be an ongoing process which does not end when the building is in use. To support the day to day management of the building in operation, robust policies will be put in place to support inclusion in accordance with the rationale of the Equality Act. Examples of such measures include carrying out regular building and user audits to identify obstructions, ensuring signs are regularly checked and updated and maintaining door furniture.
- 1.11.9 Accessibility legislation with respect to railway stations places a great deal of responsibility on those providing, running and maintaining the facilities. This access statement is intended to clarify the interpretation that the design team has placed on the access requirements in relation to planning issues.
- 1.11.10 Accessibility legislation makes it clear that reasonableness is a key ingredient in determining what is to be done in each circumstance. This strategy and statement embody what is considered to be reasonable in terms of providing the services, facilities and support at the railway station.
- 1.11.11 As time passes, the interpretation of what is deemed reasonable changes and the access strategy will need to be reviewed in order to reflect this, together with any changes to legislation. However, in the current circumstances we consider that these proposals will provide a flexible, accessible, affordable environment for all.

1.12.0 Construction Waste

- 1.12.1 Contractors and sub-contractors are required to make all reasonable efforts to effectively recycle strip-out and waste materials arising from this project. As a minimum, TransPennine Express expects to see the following recycled to either a Local Authority or a private local recycling centre located within the district or authority where the project is being carried out:

- Plastics
- Wood
- Plasterboard
- Glass
- Bricks, blocks and tiles
- Waste packaging material
- Concrete, demolition rubble and soils
- Metals

1.12.2 Contractors are also directed to the WRAP (Waste and Resources Action Programme) guides.

1.13.0 Drawing Register

1.13.1 The following drawings should be read in conjunction with this report:

AHR Building Consultancy drawings:

- HUD-001 – Site Plan
- HUD-002 – Existing Entrance Plan
- HUD-003 – Existing Elevations
- HUD-004 – Proposed Works
- HUD-005 – Existing Images and Proposed Lighting
- HUD-006 – Proposed Elevations
- HUD-007 – Proposed Handrails to South of Entrance
- HUD-008 – Proposed Handrails to North of Entrance
- HUD-009 – Schedule of Repairs

A Appendix A: Photograph Schedule



Photograph 1 – Station entrance



Photograph 2 – St George Hotel adjacent to the station



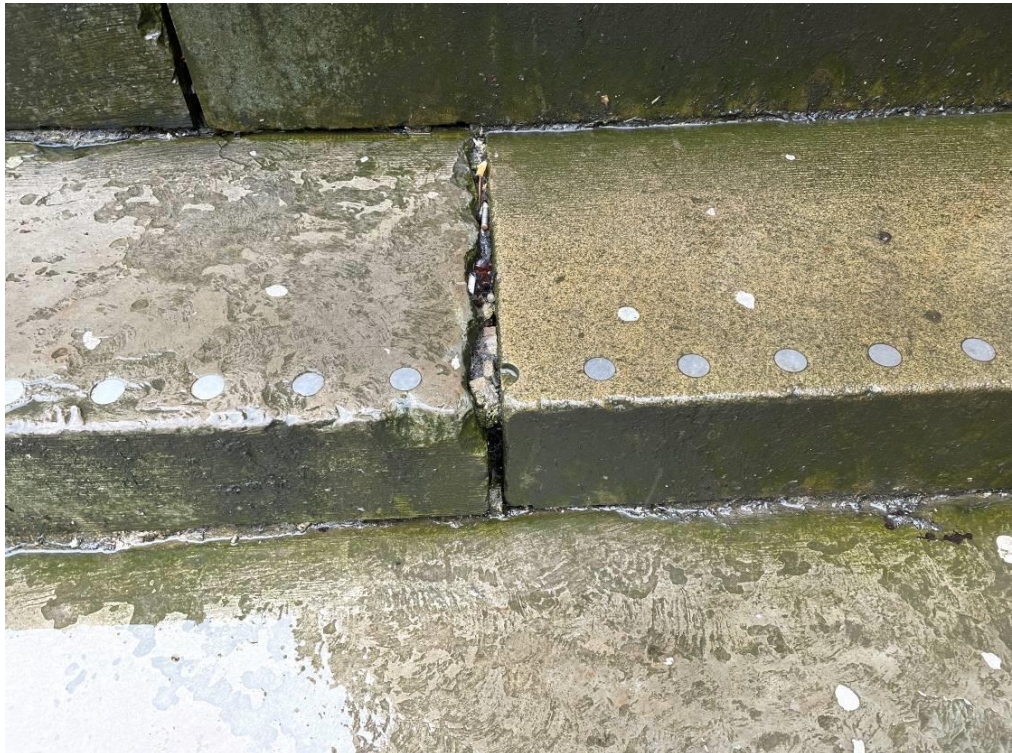
Photograph 3 – View of entrance from the south (taxi drop off / pick up area)



Photograph 4 – View from the station entrance to the opposite Lion Building's



Photograph 5 – Build-up of debris between eroded mortar joints to steps



Photograph 6 – Build-up of debris between eroded mortar joints to steps



Photograph 7 – Missing nosing marker to entrance steps



Photograph 8 – Existing globe lights to entrance



Photograph 9 – Handrail to ramped access



Photograph 10 – Areas of historic pointing repairs



Photograph 11 – Areas of historic fixings to façade



Photograph 11 – Areas of historic fixings to façade

B Appendix B: Pre-application Meeting Minutes

The following points are from an early state pre-application site meeting at the station with TransPennine and a representative from Kirklees Council. The meeting was attended by: Neil Dixon, James Smith, Austin Nwadike, Paul Purcell, Chris Jeffery, and Sue Brooks.

- Neil welcomed everyone to the meeting which took place around the front entrance steps, ramp and forecourt area. The two attached drawings were used as a basis for the discussions and Neil explained that these had been prepared to show what should be provided to a new build, but with the heritage status of the building it was important to carefully consider the impact on the building and respect the setting.
- Sue mentioned that previous proposals and options had been prepared by Strzala Architects for this part of the station as part of a masterplan. Neil agreed to request these drawings from TPE but explained that AHR's remit was to look at simpler proposals that could be implemented relatively soon in order to improve the accessibility, whilst minimising the impact on the heritage.
- Chris and Paul explained that there had been recent incidents including an elderly traveller who had fallen on the stairs – this had further highlighted the need to these works.
- The first item looked at was lighting and the possibility of providing additional wall mounted lights to either side of the entrance and in a style to match the existing globe lights and brackets. Sue suggested that it would be preferable for lights to be free standing rather than affixed to the building. We also looked at introducing a low height additional handrail to the ramp and perhaps including lights within the handrails to illuminate the ramp. This may be acceptable as long as the handrails are in a style to match the existing handrails.
- We also looked at an option for providing handrails to the steps and it was agreed that these should only be on the end elevations as shown on the attached drawing, in order to minimise the impact on the front elevation view of the building. These should also be to both sides in order to maintain symmetry, and again be in a style to match the other handrails to the ramp.
- It was agreed that the existing ramp to the side was well placed for taxi users and also the accessible parking spaces, however, there was little facility for passengers being dropped off at the opposite side. Neil explained that passengers being dropped off at the opposite side and who may be unfamiliar with the location or availability of the ramp, may struggle. Neil mentioned that with the changes in level and natural fall away from the building at this side, a ramp here may require a significant length and take up space in the car park, so at present this had been discounted.
- There is a passenger assist service in place whereby staff can aid passengers from the drop off side of the entrance and into the station. It was agreed that handrails to the steps at this side would be a benefit to passengers entering and leaving here.
- Paul pointed out that new signs had been provided inside the station to direct passengers to the exit and ramp, but these did not continue to the outside. The attached drawings detailed some additional wall mounted adhesive signs on the column bases to guide people towards the ramp when leaving through the entrance doors. A further post mounted sign was suggested around the drop off area to highlight the availability of a ramp at the opposite side.
- The steps themselves were discussed and the impact of tactile paving and highlighted nosings. Chris explained that both of these would significantly improve the accessibility and that the presence of tactile paving was of more importance than colour contrast to the paving. It was agreed that the introduction of tactile paving could be done at the bottom of the steps in the 'newer' paving, but that it would be harder to implement at the top of the steps. It was also felt that any tactile surfacing, if installed, should be to the top and bottom and not just in isolation.
- There are existing round insets to the steps to try and highlight the step edges – these offer some minimal contrast when walking down the steps, but none when going up the steps. The proposal for contrasting white painted nosings was discussed but it was felt these would detract from the building and there are other examples around the town which have not worked well. It was agreed that these should not be considered further.



- In addition to the access works Neil also highlighted some stone and pointing repairs that could be implemented at the same time to enhance the area and remove potential trip hazards.
- Sue suggested that it would be beneficial to see other examples of such installations at other similar sites for consideration.