

Issued  
November 2023

Revision - A

HALLIDAY CLARK  
ARCHITECTS

# DESIGN & ACCESS STATEMENT

Ref: 333.19

Prepared for:

## Globe Innovation Centre

### Multi-storey Car Park at

Globe Mills II  
Bridge Street  
Slaithwaite  
Huddersfield  
HD7 5JN

### Halliday Clark Architects

1 Lower Railway Road,  
Ilkley, LS298FL

T: 01943 604123

[www.hallidayclark.co.uk](http://www.hallidayclark.co.uk)  
[info@hallidayclark.co.uk](mailto:info@hallidayclark.co.uk)

### Prepared by

Jessica Ing





# TABLE OF CONTENTS

1	Introduction.....	5
2	Site Context.....	6
4	Use & Amount.....	8
5	Design, Appearance and Ethos.....	10
6	Heritage Statement.....	13
7	Landscape.....	14
8	Access.....	14
9	Crime Reduction Statement.....	15
10	Priority Employment Areas Statement.....	15



Figure 1 - Existing West Elevation

# INTRODUCTION

**This document has been created to support the Full Planning Application for the redevelopment of Globe Mills II, Slaithwaite, Huddersfield.**

**The scheme has been carefully considered regarding local and national policy and responds to its immediate context to create much needed upgraded facilities for the school.**

The Globe Mills Complex is a former textile mill built in the 19th Century. The site comprises of 2 large mills, known as Globe I and Globe II, which sit either side of Bridge Street. Globe I has been previously developed and is occupied, this application relates to Globe II, which is situated to the south of Bridge Street.

*Figure 2 - Artist Impression view from eastern end of Brook Street*



# SITE CONTEXT

**The site is located in the centre of Slaithwaite, bordered to the north by Bridge Street and to the south by the River Colne. Along Bridge Street, to the west of the site, is a mixture of housing and retail within terraced stone buildings. To the east, are light industrial warehouses and timber yards.**

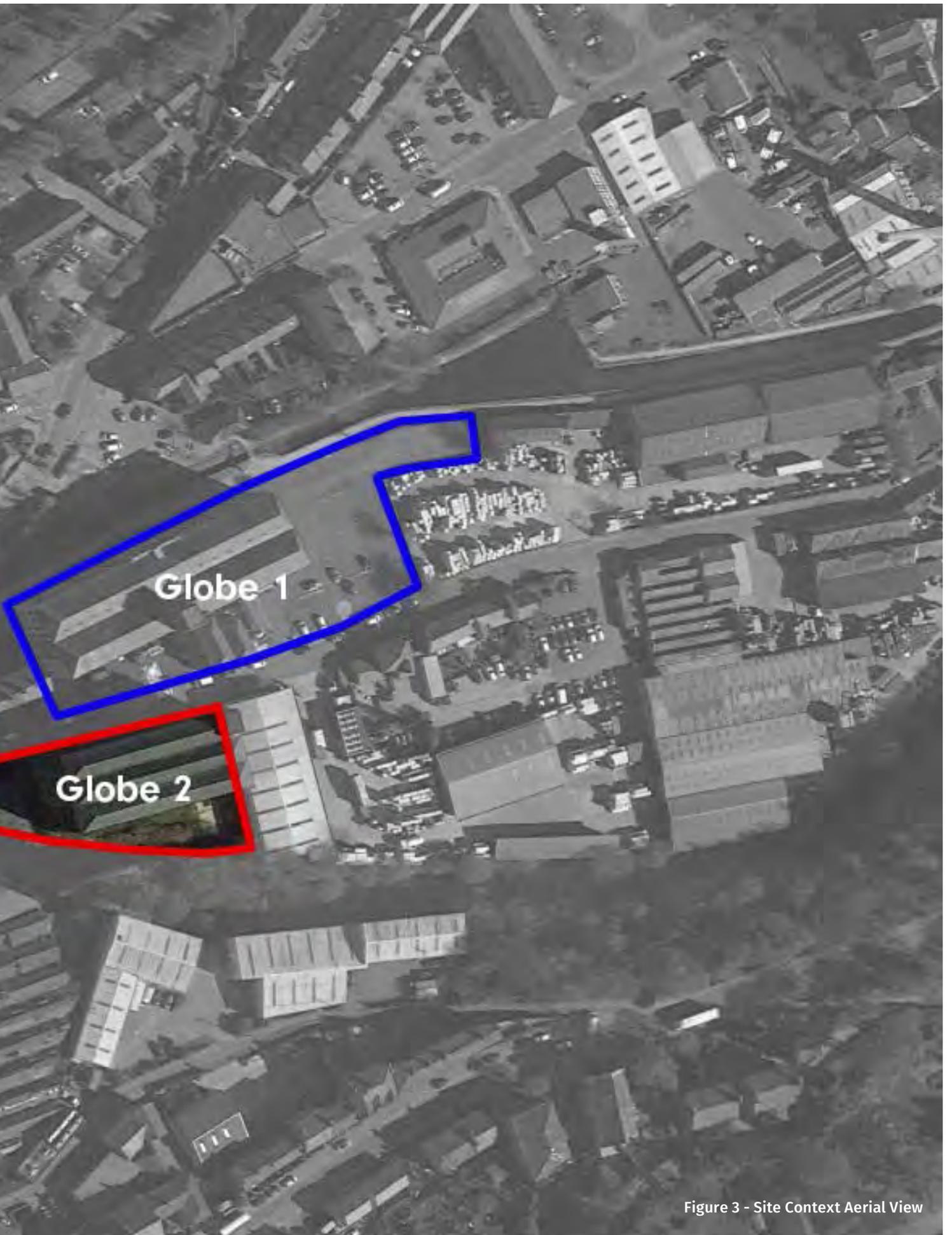
The application site consists of Globe Mill 2, a 6-storey mill building which is directly opposite the recently refurbished Globe Mill 1. The mills were originally built in stages in 1887 and operated as a worsted-spinning mill by Globe Worsted Company. They were once joined by a footbridge over Bridge Street and are examples of traditional Victorian mills, typical of the 19th century.

The character of Bridge Street is defined by the mix of small and large sandstone buildings close to the road edge on both sides. The road gradually slopes down from Britannia Road and the views down the street are dominated by the mills. In the area, the urban form is given interest due to the relationship between the large mills and the smaller scale buildings surrounding them.

The River Colne, to the south of the site, is mostly enclosed by the backs of houses through the village, typically 3 storeys from the water level. Where the river is not fully enclosed it opens out directly behind Globe 2; here it supports riparian vegetation and draws wildlife to the immediate area, however this area of the bank is generally hidden from view and inaccessible to the public. There is little planting on or around the site, with the majority of trees across the far bank of the River Colne and overgrown areas of shrubbery to the rear and sides of the mill.

The majority of Slaithwaite village centre sits within the Slaithwaite Conservation Area and Globe Mills is one of the principal landmarks. Signage throughout the village is kept traditional style cast iron, sympathetic to the existing buildings and materials in the Conservation Area.





Globe 1

Globe 2

Figure 3 - Site Context Aerial View

# USE & AMOUNT

## Use

The proposed use of the building is for a multi-storey car park to provide parking for the users of Globe 1. This will be facilitated by the internal demolition of the mill, leaving all 4 external walls in place, a self supporting steel and concrete car park structure will then be inserted into the mill. The building will provide pedestrian access to ground level.

## Amount

Site Area -	2,495sqm/ 0.24ha
GIFA (Demolition) -	6,600sqm/71,041sqft
GIFA (New) -	8,250sqm/ 88,802sqft
GIFA (Net Gain) -	1,650sqm/ 17,760sqft

## Parking Schedule

Level 1	13 spaces
Level 2	9 spaces (9 disabled)
Level 3	13 spaces
Level 4	12 spaces (3 disabled)
Level 5	13 spaces
Level 6	13 spaces
Level 7	13 spaces
Level 8	13 spaces
Level 9	13 spaces
Level 10	13 spaces
Level 11	13 spaces
Level 12	13 spaces
Level 13	13 spaces
Level 14	13 spaces
Level 15	16 spaces

**Total**                    **192 spaces (inc. 12 disabled)**

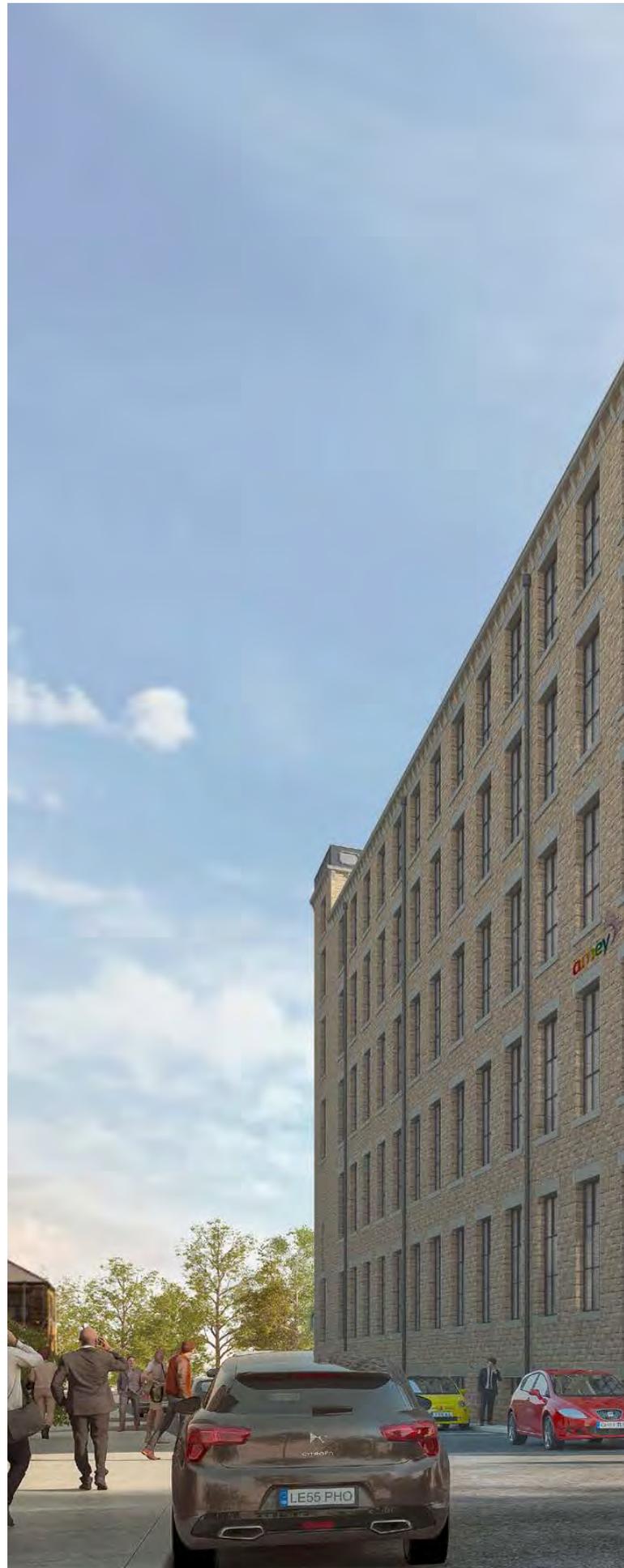




Figure 4 - Artists Impression view from western end of Brook Street

# DESIGN, APPEARANCE & ETHOS

**All 4 façades of the existing building will be retained as part of the development. The existing building is a key building within the Globe Mills complex, and therefore it is important to retain this to preserve the character of the conservation area.**

## Concept

The concept is driven by the need to provide adequate parking around the village of Slaithwaite, partly to address parking needs for Globe I, and partly to provide parking for the village itself, this will reduce the pressure on parking in the village as a whole, and reduce the number of parked vehicles on the surrounding streets. This is beneficial for both the character of the conservation area, and also for the safety of pedestrians and other road users.

The retention of the façades of the existing building ensures that the character of the area is retained, and that the multi-storey structure and parked vehicles are not visible from the street.

The existing geometry of the mill is used for the main multi-storey car park structure, with the structure being housed entirely within the existing walls of the mill.

In order to maximise space for car parking, and to ensure adequate space is allowed for vehicles, the pedestrian access to the building will be via two new external cores located at either end of the building.

## Design and Appearance

The exterior of the mill will remain as existing, with the masonry cleaned and restored, rainwater pipes will be retained and repainted and the steel tower feature restored and repainted.

In order to facilitate access for construction the roof structure will require replacement, however the existing tiles will be retained to be reused where possible.

The existing timber windows will be removed and replaced with new aluminium framed louvres to facilitate ventilation in the building. The frame structure will replicate the current window pane arrangement. The frames will be in-filled with a woven steel mesh to facilitate air movement. The woven mesh also references the heritage of the textile industry associated with this mill. This will replicate the existing window arrangement whilst also providing ventilation and screening the parked cars from view.

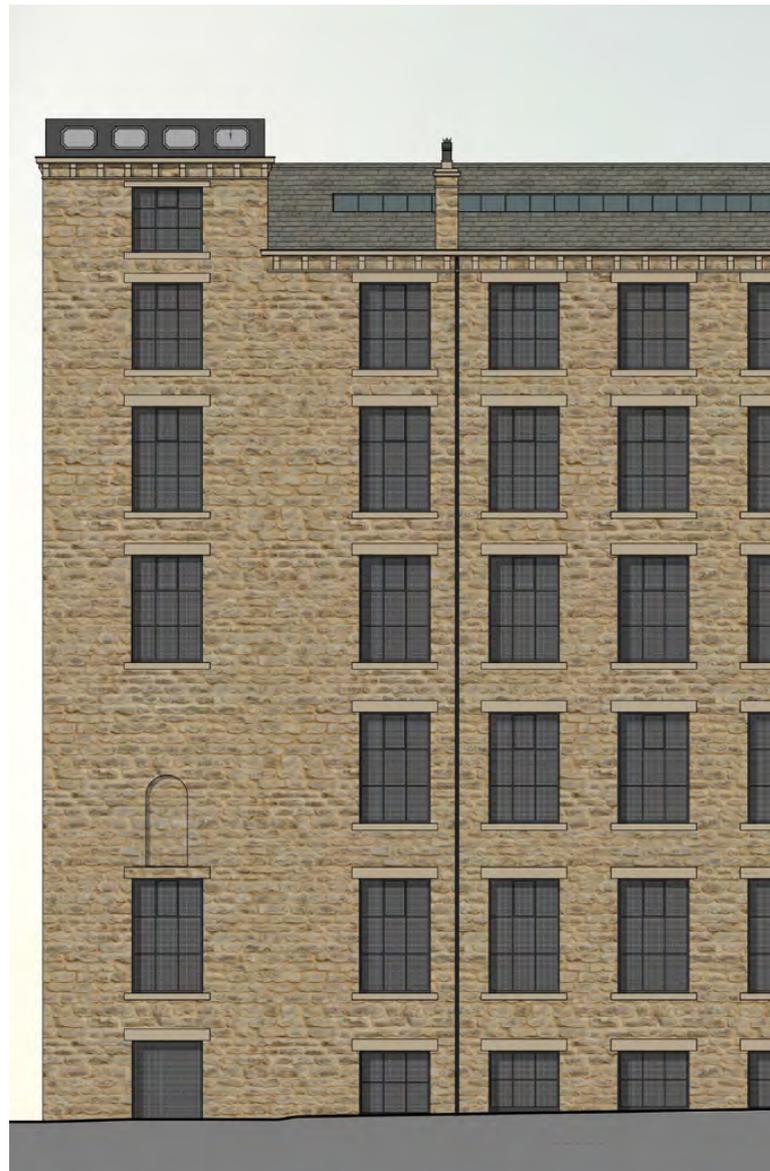




Figure 5 - Proposed Elevations

### Staircores

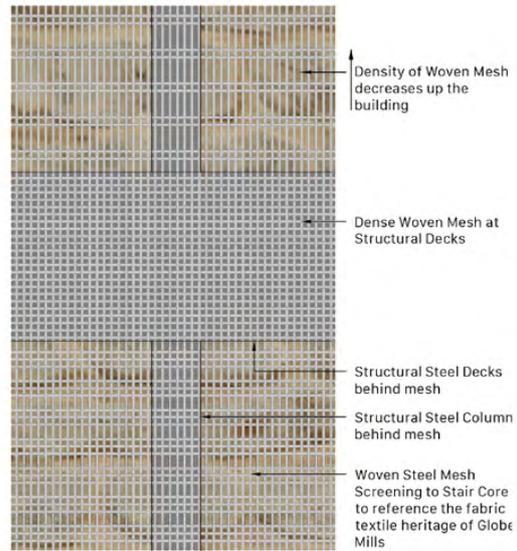
Similarly the two stair cores will be open external spaces. The western stair will have central lift core and wrap around concrete stair arrangement to provide the primary pedestrian access from the street. The eastern core will be staircase only and provide emergency exit only. This staircase is not intended for everyday use.

The external treatment of the staircores is also proposed as a woven steel mesh system, surrounding a steel and concrete structure. The woven mesh references the heritage of the textile industry associated with Globe Mills and the textiles produced within it. The mesh provides shelter whilst providing an interesting and tactile surface both internally and externally. The density of the mesh will diminish as the height of the building increases. This helps to reduce the visual impact of the stair cores as their density is decreased and they blend further into the surrounding context. This will be achieved by a mesh design that utilises less 'strings' in the weaving pattern whilst keeping the steel diameter the same, so delicacy and detail is achieved at all levels.

### Internal Carpark Structure

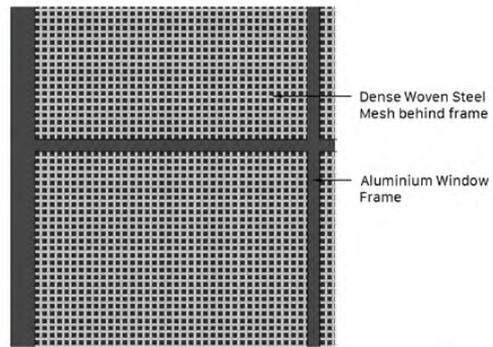
The internal car park structure will be primarily of concrete and steel, designed to sit within the existing structure and not interact with it. The existing façades will subsequently be supported by the new structure, rather than the other way round.

Internally the existing brick masonry will be left exposed and the woven mesh ventilation grilles will be visible from inside. Crash barriers will be installed where appropriate against the external facade.



### Stair Core Mesh Detail

1:10



### Window Screen Mesh Detail

1:10



Figure 6 - Artists Impression view from inside carpark

# HERITAGE STATEMENT

**The site is within the Slaithwaite Conservation Area and Globe Mills is one of the principal landmarks within that Conservation Area. The importance of this character and the opportunity to create a high quality mixed use scheme; which fits within the scale, form and materials, was assessed and approved under the existing consent (App no. 2013/62/91452/W), reflecting the context of the area.**

The revised proposals aim to maintain the same sensitive and considered approach to the redevelopment of Globe 2 as was initially proposed under the existing consent. The deployment of minimal interventions to the existing external facade will ensure that the character of this area is maintained in line with the completed redevelopment of Globe 1.

Where new additions are proposed we have been careful to consider the use of sensitive materials and low impact design. The use of steel mesh is in line with the industrial character of the area, and its woven design is a deliberate reference to the woven textiles that used to be manufactured in the mill. The use of this material in a vertically diminishing way on the stair cores ensures this feature is partially transparent, getting more so at higher level, which further reduces its visual impact.

The lack of glass in the proposal also avoids large areas of reflective surface, which is a very common contemporary characteristic we were keen to keep away from, as this would be alien to this part of Slaithwaite and in particular the mill itself.

The stair cores are also of similar design to the fire escape staircases already present on Globe 1.

The vehicle entry point is designed to utilise a modified existing vehicle access point, and placing the vehicle entry point to the building below street level ensures it has virtually no impact on the streetscape.

The use of the rear part of the site for landscape and biodiversity improvements ensures the protection of the character of this part of the site. Particularly when viewed from the bridge over the River Colne at Britannia Road, where currently the view of the river disappears into trees and riverside vegetation.

## Executive Structural Summary

The aim of the structural proposals are to remove the corroding and rotting elements of the existing building, and to construct a new structure within the existing masonry walls.

The existing walls will be kept maintaining the character of the existing building and therefore preventing any loss. The proposed frame within the existing masonry walls will be constructed in a sequenced manner where no façade retention is required. The frame will tie to the existing walls providing strength and stability. The frame therefore will be designed to serve its function whilst maximising the available space.

*Figure 7 - Historic view of Slaithwaite, with Globe Mills prominent in centre, 1930 - Copyright: Historic England*



# LANDSCAPE

# ACCESS

**The Landscaping proposals have been designed to be as least intrusive and extensive as possible in order to maintain the character of the Conservation Area**

The site is a industrial site in the centre of a village, therefore the existing landscaping is minimal. The proposals aim to utilise the remaining areas of land on the site for landscape and biodiversity improvement. Further details of this can be found in the Landscape Design documents accompanying this application.

The area to the south of the site that site between the mill and the River Colne has been designated as an area of Biodiversity improvement. The location next to the river makes it a particularly beneficial area for this. Alongside this, the rear area of the mill is relatively inaccessible for pedestrians an passers-by, therefore the landscape and biodiversity will be left alone to flourish and develop naturally. The additional flora and trees onsite will also help to offset pollution and carbon emissions emanating from the vehicles parked in the adjacent car park.

New vehicular access is proposed from Bridge Street, further details can be found in the plans accompanying this application. A full size junction with associated junction radii and visibility splays will lead to a 1:16 two way ramp providing access to the lowest level of parking. A sloping floor with ramp arrangement then allows for 15 'half level' floors of parking. The car park provides two-way circulation and vehicles enter and exit the car park via a barrier system on the lowest level.

Pedestrian access will be via a new stair core to the western elevation adjacent to Bridge Street, this core will provide stair and lift access to even number levels (Levels 2,4,6,8,10,12&14), internal stairs in the car park will provide pedestrian access from the even numbered levels to the odd numbered levels, e.g Level 2 to Level 1, or Level 8 to Level 7. Disabled spaces are located on levels with direct level access to the stair core. The majority of disabled spaces are located on Level 2 which has direct street level access.

A further stair core to the east end of the building provides fire escape access only from odd numbered levels, e.g. 1,3,5,7,9,11,13&15.

Internal stairs between levels ensure that pedestrians are not required to use vehicle ramps to access parking levels.

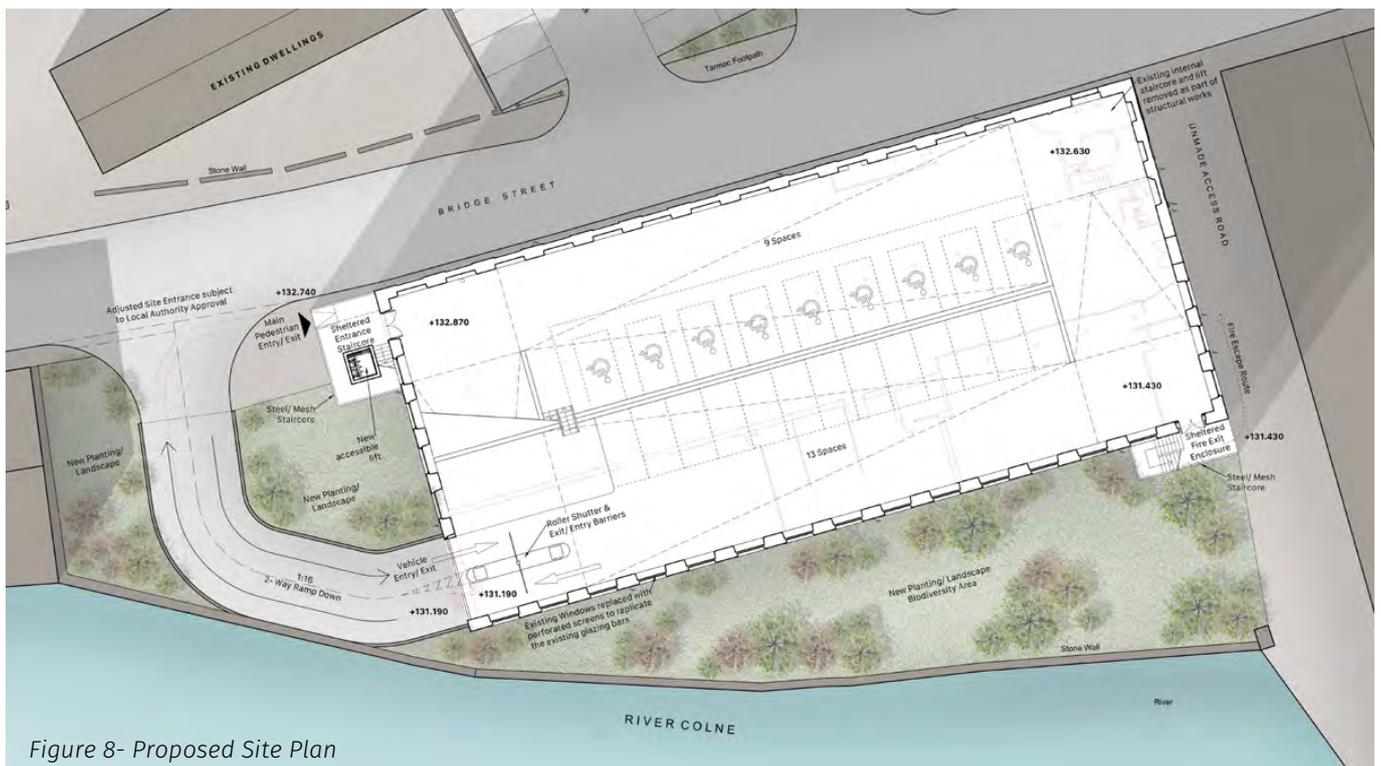


Figure 8- Proposed Site Plan

# CRIME REDUCTION STATEMENT

**The 'National Planning Policy Framework' makes clear that a key objective for new developments should be that they create safe and accessible environments where crime and disorder or fear of crime does not undermine quality of life or community cohesion.**

The proposed design will reflect the attributes of safe, sustainable places, as set out in Safer Places - The Planning System and Crime Prevention.

The proposals incorporate the principles of secured by design (SBD) and ensure crime prevention is considered an integral part of the design of this development. The proposals do this by addressing the following issues in respect to designing out crime.

- Lighting in the development is proposed to ensure that parking areas as well as entrances and footpaths are well lit with good colour rendition and uniformity levels.
- The design or any layout of pedestrian and vehicular routes into and within the sites including how they will integrate with existing patterns.
- Landscaping and planting, hiding places and dark secluded areas are avoided.
- Public spaces and car parks are well lit.
- Secure access to car parking area via barrier system to control vehicle movements both in and out of the development.
- Natural surveillance of stair cores and pedestrian access routes due to the transparent design of the external envelope treatment to the stair cores

# PRIORITY EMPLOYMENT AREAS STATEMENT

**The scheme does not lie within a Kirklees Priority Employment Area, however we acknowledge that historically the Globe Mills complex would have been a major employment provider in the town.**

The site was previously part of the wider Globe Mills complex, and functioned as a textile mill for the Globe Worsted Company. Like many other textile mills the Globe Worsted Company saw a gradual decline in trade towards the end of the 20th Century and by the early 21st Century all manufacturing and trading on the site had been ceased. Globe 2 has subsequently been vacant for around 20 years.

This application is closely related to the ongoing redevelopment of the larger of the 2 mills, Globe 1. Globe 2 currently has permission for conversion to Offices and Light Industrial with associated parking (application ref: 2015/90646) which has been implemented by the development of Globe 1. Globe 1 has been converted into a mixed-use development providing both public use and significant employment opportunity this has been completed and is currently in the process of being occupied.

The conversion of Globe 1 has created a significant amount of employment opportunity, which is being utilised and having a positive impact on the economy of the town.

Working practices have changed significantly since the original mill was providing significant employment in the town in the 19th and 20th Centuries, and the letting of Globe 1 is causing a strain on the parking provision provided as part of the Planning Consent. This could be impacting the parking provision in the town as a whole.

Therefore the development of Globe 2 aims to support the parking provision for Globe 1 and the wider town via the provision of both private and public parking spaces. This will encourage the employment opportunities within Globe 1 to continue to be utilised in a way that has a positive impact on the town both economically and strategically.

The applicant feels that to implement the current Planning Approval for Globe 2 would increase employment opportunities in the town however may exacerbate the parking pressures that are already being felt and have an overall detrimental impact on the town. Therefore the applicant is willing to sacrifice the economic investment to provide infrastructure for the benefit of the town .



HALLIDAY CLARK  
ARCHITECTS

**Halliday Clark Architects**

1 Lower Railway Road, Ilkley, LS298FL

T: 01943 604123

[www.hallidayclark.co.uk](http://www.hallidayclark.co.uk)

[info@hallidayclark.co.uk](mailto:info@hallidayclark.co.uk)