

NetworkRail

Network Rail (Huddersfield to Westtown (Dewsbury) Improvements) Order

Condition 6: Construction Traffic Management and Travel Plan – Stage 4

Document reference: 151667-TSA-00-TRU-REP-W-EN-001245

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1. INTRODUCTION

1.1 Background

- 1.1.1 The Scheme is part of a wider programme of works under the Transpennine Route Upgrade (TRU) which will improve the Transpennine railway between Manchester, Huddersfield, Leeds and York and improve connections between key towns and cities across the north of England.
- 1.1.2 Planning Direction for the Huddersfield to Westtown (Dewsbury) section of the TRU was received from the Department for Transport referenced TWA/21/APP/03, dated 13 October 2022.
- 1.1.3 This Construction Traffic Management Plan (CTMP) sets out details in relation to Condition 6 of the Deemed Planning Permission.

2. STAGED APPROACH TO DISCHARGE AND STAGE DESCRIPTION

- 2.1.1 As set out in document ref: 151667-TSA-00-TRU-REP-W-EN-001189 version 3 (submitted in relation to Condition 3 of the Deemed Planning) a staged approach is proposed in relation to discharge of the deemed planning conditions.
- 2.1.2 This document sets out details in relation to Stage 4 of the works for the Huddersfield to Westtown (Dewsbury) Scheme.
- 2.1.3 Stage 4 comprises the main civils works at Huddersfield Station and Huddersfield Viaduct (Stage 4 limits are set as Westgate Overbridge and Hillhouse Lane Underbridge) and are set out in Table 2-1, as well as links to the relevant planning drawings. Figure 1 in Appendix A shows the geographical locations of the works. Route drawings relevant to Stage 4 are [NR13 Planning Drawing - Route Drawing 3.pdf \(windows.net\)](#) and [NR13 Planning Drawing - Route Drawing 4.pdf \(windows.net\)](#).
- 2.1.4 Works within the Huddersfield and Gledholt tunnels, to the west of the station, will be detailed in the Stage 5 submission.
- 2.1.5 The entire Scheme will be subject to electrification; details of the electrification works will be detailed in the Stage 5 submission.
- 2.1.6 At Huddersfield station remodelling works are required with alterations to platforms and roof structures required to facilitate the delivery of a four track railway. East of Huddersfield Station the four-track railway is reinstated across the viaduct.
- 2.1.7 There are existing earthworks throughout the Scheme area associated with the existing operational railway. Earthworks allow the track to stay relatively level through a varied topography and allows trains to operate more efficiently by reducing the need for additional acceleration and deceleration to climb and descend climbs.
- 2.1.8 As set out in Figure 2-1¹ in Volume 4 of the Environmental Statement (ES) and Table 2-4 in Chapter 2: Scheme Description (Route Section 1)² in Volume 2i of the ES, a 55m length retaining structure (0.5m high) was proposed along the eastern end of Huddersfield Viaduct (MVL3/92) near to Hillhouse Lane Underbridge (MVL3/94). This was proposed as either king post wall or soil nailing. Following design iteration, soil nailing is planned in this area of existing embankment to the south of the railway (E0).
- 2.1.9 Earthworks (new and where they have been reworked) will generally be covered in topsoil and landscaped as appropriate. Any exceptions to this will be detailed within the Landscape and Ecological Management Plan (LEMP) Stage 8.
- 2.1.10 The Scheme impacts on various existing transmission and distribution utility networks. Conflicts with utility services may occur in Stage 4 where the Scheme crosses highways and. Works within the highway will be carried out in compliance with the Highways Agreement and any impacts on the highways network will be discussed through the Highway Network Management Group.

¹ [Ch02 Scheme Description - Fig 2-1 Scheme drawings.pdf \(windows.net\)](#)

² [Ch02 Huddersfield - Scheme Description.pdf \(windows.net\)](#)

Table 2-1 Works description

Location	Structure/works	Summary description	Deemed Planning Drawing Title and Reference
Huddersfield Station	Passenger Footbridge	A covered footbridge (Huddersfield Station Footbridge (MVL3/91AA), with stairs and a lift, to be constructed to the eastern end of the station. This will provide step free access to the central platforms.	<ul style="list-style-type: none"> • Footbridge - Proposed Elevations - 151667-TSA-30-MVL3-DRG-T-LP-168053 • Footbridge - Proposed Plan Deck Level - 151667-TSA-30-MVL3-DRG-T-LP-168051 • Footbridge - Proposed General Arrangement Platform Level - 151667-TSA-30-MVL3-DRG-T-LP-168050 • Footbridge - Proposed Roof Level General Arrangement - 151667-TSA-30-MVL3-DRG-T-LP-168052 • Footbridge - Proposed Sections - 151667-TSA-30-MVL3-DRG-T-LP-168054 • Existing and Proposed Long Sections (A-A) - 151667-TSA-30-MVL3-DRG-T-LP-168003 • Existing and Proposed Long Sections (B-B) - 151667-TSA-30-MVL3-DRG-T-LP-168005
Huddersfield Station	Passenger Subway	Extension to existing Subway (MVL3/91) (of 12.5m) required to service the new island platform to the north of the station.	<ul style="list-style-type: none"> • Existing Plan and Sections - 151667-TSA-30-MVL3-DRG-T-LP-168064 • Proposed Plan and Section - 151667-TSA-30-MVL3-DRG-T-LP-168065
Huddersfield Station	Parcel Subway (MVL3/91A)	Utilising existing subway for utilities ducting and signalling equipment. Concrete infill.	<ul style="list-style-type: none"> • Existing Plan and Sections - 151667-TSA-30-MVL3-DRG-T-LP-168067 • Proposed Plan and Section - 151667-TSA-30-MVL3-DRG-T-LP-168068
Huddersfield Station	Tearooms	Existing Tea Rooms - Timber structure to be carefully dismantled and relocated within island platform. To be dismantled and reconstructed.	<ul style="list-style-type: none"> • Existing Floor Plan and Elevations - 151667-TSA-30-MVL3-DRG-T-LP-168016 • Existing and Proposed Elevations - 151667-TSA-30-MVL3-DRG-T-LP-168015 • Proposed Floor Plan and Elevations - 151667-TSA-30-MVL3-DRG-T-LP-168017
Huddersfield Station	Proposed Platforms	Works to railway lines including provision of new platforms and removal of existing railway sidings.	<ul style="list-style-type: none"> • Existing Plan - 151667-TSA-30-MVL3-DRG-T-LP-168060 • Existing and Proposed Platform General Arrangement - 151667-TSA-30-MVL3-DRG-T-LP-168001 • Proposed Plan and Section - 151667-TSA-30-MVL3-DRG-T-LP-168061 • Proposed Plan and Section - 151667-TSA-30-MVL3-DRG-T-LP-168062 • Proposed Plan and Section - 151667-TSA-30-MVL3-DRG-T-LP-168063

Location	Structure/works	Summary description	Deemed Planning Drawing Title and Reference
Huddersfield Station	Canopy A	<p>Works to the main train shed within Huddersfield Station includes:</p> <ul style="list-style-type: none"> • Structural works to maintain and strengthen; • Demolition of two canopy bays at Manchester end of the station; • Construction of two new bays at Leeds end; • Grit blasting of structure; and • Reinstatement of main train shed lantern. 	<ul style="list-style-type: none"> • Existing and Proposed Roof General Arrangement - 151667-TSA-30-MVL3-DRG-T-LP-1680000 • Existing and Proposed Short Sections (A-A) - 151667-TSA-30-MVL3-DRG-T-LP-168002 • Existing and Proposed Short Sections (B-B) - 151667-TSA-30-MVL3-DRG-T-LP-168004 • Existing Roof A Structural Plan (Roof Level) - 151667-TSA-30-MVL3-DRG-T-LP-168010; • Existing Roof A Structural Sections Sheet (1) - 151667-TSA-30-MVL3-DRG-T-LP-168011 • Existing Roof A Structural Sections Sheet (2) - 151667-TSA-30-MVL3-DRG-T-LP-168012; • Existing Roof A OLE Support Details - 151667-TSA-30-MVL3-DRG-T-LP-168013
Huddersfield Station	Canopy B & C	<p>Canopy B&C are to be demolished with new replacements to be constructed to cover platforms to north.</p>	<ul style="list-style-type: none"> • Existing and Proposed Roof General Arrangement - 151667-TSA-30-MVL3-DRG-T-LP-168000 • Existing and Proposed Short Sections (A-A) - 151667-TSA-30-MVL3-DRG-T-LP-168002 • Existing and Proposed Short Sections (B-B) - 151667-TSA-30-MVL3-DRG-T-LP-168004 • Proposed Roof B (Shed Roof) Structural Plan (Roof Level) - 151667-TSA-30-MVL3-DRG-T-LP-168020 • Proposed Roof B (Shed Roof) Structural Plan (Platform Level) - 151667-TSA-30-MVL3-DRG-T-LP-168021 • Proposed Roof B (Shed Roof) Structural Sections (1) - 151667-TSA-30-MVL3-DRG-T-LP-168022 • Proposed Roof B (Shed Roof) Structural Sections (2) - 151667-TSA-30-MVL3-DRG-T-LP-168023 • Proposed Roof B (Shed Roof) Structural Sections (3) - 151667-TSA-30-MVL3-DRG-T-LP-168024
Huddersfield Station	Platform free standing canopies	<p>Free standing canopies to be constructed over island platforms to eastern end of station.</p>	<ul style="list-style-type: none"> • Existing and Proposed Roof General Arrangement - 151667-TSA-30-MVL3-DRG-T-LP-168000 • Proposed Platform General Arrangement - 151667-TSA-30-MVL3-DRG-T-LP-168030

Location	Structure/works	Summary description	Deemed Planning Drawing Title and Reference
			<ul style="list-style-type: none"> Proposed Platform Canopies Elevation (1) - 151667-TSA-30-MVL3-DRG-T-LP-168034 Proposed Platform Canopies Elevation (2) - 151667-TSA-30-MVL3-DRG-T-LP-168035 Proposed Platform Canopies Structural Plan (Platform Level) - 151667-TSA-30-MVL3-DRG-T-LP-168031 Proposed Platform Canopies Structural Sections - 151667-TSA-30-MVL3-DRG-T-LP-168032 Proposed Platform Canopies Structural Sections - 151667-TSA-30-MVL3-DRG-T-LP-168033
Huddersfield Station	Canopies – Penistone Line	Extension of Penistone Line canopies.	<ul style="list-style-type: none"> Proposed Penistone Line Canopy Platform Level Plan General Arrangement - 151667-TSA-30-MVL3-DRG-T-LP-168036 Proposed Platform Penistone Canopies Structural Sections - 151667-TSA-30-MVL3-DRG-T-LP-168037 Proposed Platform Penistone Canopies Proposed Roof Covering Plans - 151667-TSA-30-MVL3-DRG-T-LP-168038 Proposed Platform Penistone Canopies Elevation (1) - 151667-TSA-30-MVL3-DRG-T-LP-168039
Huddersfield Station	Relay Room	Existing relay room to be demolished.	<ul style="list-style-type: none"> Existing and Proposed Platform General Arrangement - 151667-TSA-30-MVL3-DRG-T-LP-168001
Huddersfield Station	Drainage Works	Fitzwilliam Street sewer outfall (New) SE 1430 1707. Proposed new storm water drainage outfall for the re-modelled areas of Huddersfield Station. It will be a piped outfall from the drainage system, either directly into the sewer in the highway, or into an existing culvert within Network Rail land which connects into this sewer. A new manhole will be provided at the outfall.	<ul style="list-style-type: none"> No relevant planning drawings

Location	Structure/works	Summary description	Deemed Planning Drawing Title and Reference
Huddersfield Viaduct (MVL3/92)		Works across Huddersfield Viaduct includes general strengthening works along the length of viaduct together with localised repairs to arches where necessary. These works include pinning and grouting, shear anchors and spandrel strengthening with tie bar and pattress plates.	<ul style="list-style-type: none"> Existing and Proposed East Elevation (Sheet 1) - 151667-TSA-30-MVL3-DRG-T-LP-168075 Existing and Proposed East Elevation (Sheet 2) - 151667-TSA-30-MVL3-DRG-T-LP-168076 Existing and Proposed East Elevation (Sheet 3) - 151667-TSA-30-MVL3-DRG-T-LP-168077 Existing and Proposed East Elevation (Sheet 4) - 151667-TSA-30-MVL3-DRG-T-LP-168078 Existing and Proposed East Elevation (Sheet 5) - 151667-TSA-30-MVL3-DRG-T-LP-168079
Huddersfield Viaduct	Span 1 – John William Street Underbridge (MVL3/92(2))	The existing Span 1 bridge deck will be removed and replaced with a new single span bridge deck due to the current structural arrangement of the bridge clashing with the proposed track and platform works.	<ul style="list-style-type: none"> Existing Plan and Proposed Plan (Sheet 1) - 151667-TSA-30-MVL3-DRG-T-LP-168070 Existing Plan and Proposed Plan (Sheet 2) - 151667-TSA-30-MVL3-DRG-T-LP-168071 Existing Plan and Proposed Plan (Sheet 3) - 151667-TSA-30-MVL3-DRG-T-LP-168072
Huddersfield Viaduct	Span 4 – Fitzwilliam Street Underbridge (MVL3/92(3))	Substructure repairs including removal and reinstatement of the pier to the south-western corner of the structure. Strengthening to cross girders. Removal and replacement of parapet.	<ul style="list-style-type: none"> Existing Plan and Proposed Plan (Sheet 4) - 151667-TSA-30-MVL3-DRG-T-LP-168073 Existing Plan and Proposed Plan (Sheet 5) - 151667-TSA-30-MVL3-DRG-T-LP-168074 John William Street Bridge - Existing Highways General Arrangement - 151667-TSA-30-MVL3-DRG-T-LP-168081 John William Street Bridge - Proposed Highways General Arrangement - 1667-TSA-30-MVL3-DRG-T-LP-168082
Huddersfield Viaduct	Span 29 Bradford Road Underbridge (MVL3/92(9))	Existing bridge deck to be removed and replaced. Additional masonry pilasters/buttresses will be formed at the corners of the new abutments into which the concrete parapet will join. New concrete abutments to be built with stone facing in front of existing sandstone at both ends and, on both elevations.	

3. INFORMATION INCLUDED IN THIS SUBMISSION

3.1.1 The wording of Condition 6 is reproduced in Table 3-1. Based on the scope of works detailed in Section 2, Table 3-1 sets out how this document details the information required in conjunction with Condition 6.

Table 3-1 Details of Condition 6

Condition reference	Details of condition	Section reference
Condition 6a (Sub-section i)	<p>The package of interventions and mitigation outlined in Volume 2i, Chapter 23, Page 5, paragraph 23.2.14 of the Environmental Statement including an implementation timetable for each stage.</p> <ul style="list-style-type: none"> • Traffic signage • Signing and guarding of compound access and egress; • Co-ordination of road closures • The need for diversions as well as any route timing or volume restrictions and • Rail replacement strategy 	<p>Details relating to traffic signage and guarding is contained within Section 4.2.</p> <p>Road closures are outlined in Section 6.</p> <p>Diversions are detailed in Section 6.</p> <p>Route timing is detailed in Section 4.3.</p> <p>Rail replacement strategy is included in Section 8.</p>
Condition 6a (Sub-section ii)	<p>Specific details on arrangements for temporary car parking provision for train users as appropriate at each station including temporary parking at Huddersfield and Mirfield stations and mobility impaired set down/pick up points at Ravensthorpe and Deighton stations.</p>	<p>Information is included in Section 4.6 relating to temporary parking at Huddersfield railway station.</p>
Condition 6a (Sub-section iii)	<p>A travel plan for construction staff, outlining the methods by which they shall be transported to the relevant sites and including the provision of non-motorised facilities to encourage walking and cycling.</p>	<p>Details are provided in Section 5 in relation to the construction staff required to deliver Stage 4.</p>
Condition 6a (Sub-section iv)	<p>Details on temporary diversions of both highways and rights of way required as part of the Scheme.</p>	<p>Highway diversions required to facilitate the works in Stage 4 are included in Section 6.</p> <p>Information relating to works to the Public Right of Way (PRoW) network is set out in Section 7.</p>

4. TRAFFIC MANAGEMENT MEASURES

4.1 Site compounds

4.1.1 Works during Stage 4 will utilise the following site compounds:

- Compound 4 – Fitzwilliam Street
- Compound 5 – Network Rail Existing Maintainers Yard
- Compound 6 – Huddersfield Station Car Park
- Compound 7 – Castlegate
- Compound 10a – Halfords/Bradford Road

4.1.2 All works associated with the construction compounds between Huddersfield and Bradley Junction are detailed in the Stage 2 documentation (previously submitted planning application ref: 2022/44/93945/W).

4.2 Signage and guarding of compound access and egress

4.2.1 Details of signage types and locations are being communicated to the local highways authority through ongoing discussions. The signage types and locations will be formally agreed through the Highways Network Management Group (to be documented in meeting minutes).

4.2.2 Any vehicles accessing the work sites will be made aware of and must use the agreed construction access routes.

4.2.3 Appropriate signage is to be utilised to limit disruption of ongoing construction work.

4.2.4 Temporary road signing will be implemented on main highway junctions signposting construction traffic to the preferred access routes. The signs will be in place throughout the duration of the works to direct construction traffic to and from the Site and to inform background traffic of the ongoing works along the Scheme.

4.2.5 Additional road signage will be placed on the local road network to slow the traffic down and to provide advance warning of the site access and egress points. In addition, a site safety sign will be provided at the entrance of the site with contact details for the site manager should anyone have any queries regarding the site.

4.2.6 Traffic signage, including those provided at the site access points, will be clearly signed and consistent with that provided elsewhere on the public highway.

4.2.7 All road signs are to be used with approved stands or erected on posts set into the ground, where permitted by the local highways authority. Where appropriate, the erection of permanent signage will be agreed through the Highways Management Network Group.

4.2.8 All signs are to be placed in the most advantageous position, having regard for the nature of the hazard and the warning being conveyed, to provide the maximum visual impact for approaching drivers.

4.2.9 All signs will be installed in accordance with the relevant chapters of the Traffic Signs

Manual Chapter 8, Parts 1 and 2³.

- 4.2.10 It will be ensured that all signs used are in good condition and are removed at the completion of the work.
- 4.2.11 Safety signals are required where, despite putting in place all other relevant measures, a significant risk to the health and safety of employees and others remains. All site-related works shall not commence until all signage is in place, to ensure safety all staff, road and footpath users.
- 4.2.12 Signs will be clear and legible, and used to identify actions that are prohibited (e.g. no access), safeguards that must be followed (e.g. ear protection must be worn), warning of a hazard (e.g. corrosive material) and to direct towards fire exits/equipment or first-aid equipment.
- 4.2.13 Signs will be placed within the driver's line of sight and at the same time not obscuring other traffic devices from the driver's line of sight.
- 4.2.14 Signs and devices will be placed in a manner and position so as not to become a possible hazard to construction personnel, pedestrians or vehicles (e.g. diverting traffic into an undesirable path).
- 4.2.15 Signs and devices will be regularly checked for effectiveness and maintained in a satisfactory condition.
- 4.2.16 Signs and devices will be removed from the site when practical and once the hazard ceases to exist, restoring the road/footway to previous condition.
- 4.2.17 Details of the construction compound locations, the proposed signage locations and type for each compound area are provided in the submitted Stage 1 and Stage 2 documentation.
- 4.2.18 Table 4-1 details the proposed signage locations and type for each worksite is provided in Appendix B.
- 4.2.19 Access to compounds will be from the existing highways network. To provide assurance to Kirklees Council's Planning and Highways departments, Table 4-1 also details the access/egress arrangements to the compound sites and identifies whether any improvement is required. Details are also provided in the drawings in Appendix B.

Table 4-1 Details of drawings showing signage details for Stage 4

Location	Site location	Details of access and egress	Figure number
Huddersfield Station	Compound 4 – Fitzwilliam Street	Existing access to be utilised for works	B2
	Compound 5 – Network Rail Existing Maintainers Yard	Existing access to be utilised for works	B2
	Compound 6 – Huddersfield Station Car Park	Existing access to be utilised for works	B2

³ [Traffic Signs Manual - Chapter 1 Introduction \(publishing.service.gov.uk\)](https://www.gov.uk/publishing/traffic-signs-manual)

Location	Site location	Details of access and egress	Figure number
Huddersfield Viaduct (MVL3/92)	Compound 7 – Castlegate	Existing access to be utilised for works	B2
Huddersfield Viaduct - Span 29 Bradford Road Underbridge (MVL3/92(9))	Compound 10a – Huddersfield/Bradford Road	Existing access to be utilised for works	B3

4.3 Route timings

- 4.3.1 Deliveries for construction works will generally be made by road and will be in standard working hours.
- 4.3.2 Deliveries and collections will be limited to normal site working hours:
- 08:00 to 18:00 Monday to Friday, with 30 minutes either side for setting up and organising/cleaning the site; and
 - 08:00 to 13:00 on Saturdays, with 30 minutes either side for setting up and organising/cleaning the site.
- 4.3.3 Where practicable, deliveries will be staggered and delivered to Site as required. Heavy Good Vehicles (HGV) will not be permitted to wait outside the construction sites. The use of a just in time delivery system will ensure that deliveries are programmed in, and vehicles are directed to the loading and unloading areas as required. Should vehicles be required to wait, this will occur within the site confines.
- 4.3.4 Measures to manage any noise impacts from traffic and deliveries are detailed in the Noise and Vibration Management Plan (document ref: 151667-TSA-00-TRU-REP-W-EN-001243) for Stage 4.
- 4.3.5 A 5 week notice period of abnormal load movements will be provided to the Local Highways Authority. Requirements will also be presented to the established Highways Network Management Group for information.

4.4 Traffic generation

- 4.4.1 Traffic generation and its potential effects on junctions etc. was considered through the Transport Assessment submitted and accepted under the Transport and Works Act Order (TWAO). The Transport Assessment is provided in Appendix 14-1 in Volume 3 of the ES⁴.
- 4.4.2 During Stage 4 the following types of vehicles will be used:
- Light Goods Vehicles (LGV) comprises vehicles up to 7.5 tonnes excluding cars:
 - Two 8-seat minibuses to transport staff to work sites;
 - One 4x4;
 - Four electric pool cars;

⁴ [W3 Environmental Statement Volume 3: Appendix 14-1 Transport Assessment](#)

- Three electric vans;
- Six transit van crew cabs;
- HGV:
 - 6-8 wheeler rigid tippers;
 - 6-8 wheeler rigid flatbed hiab;
 - Tractor and trailer;
 - 8-wheel concrete mixers.
- One 17m forklift
- Two 50-seater coaches

4.4.3 These vehicles all routinely use the local highway network.

4.5 Plant and equipment logistics

4.5.1 A Scheme-wide logistics hub will be established. Plant will be transported throughout the Scheme as required from that central location. Three plant managers will be based at the site to manage the plant requirements across the Scheme.

4.5.2 In general, Just-In-Time (JIT) deliveries will be planned, but due to blockades staging areas will be required, for Stage 4 works, the staging area will be located at the Hillhouse compound site.

4.5.3 Operated Plant Review Meetings will be held weekly between all the plant managers to ensure the most efficient model and delivery is achieved. This will also minimise the impact on the local highway network.

4.6 Alternative parking at Huddersfield Station

4.6.1 The formal station car park at Huddersfield Station is needed as a construction compound to serve works at the station. From November 2023, the current staff car park will be possessed for the Stage 6 W3A works, as well as access across the short stay and long stay car parks. Customer parking will be suspended at the station for the duration of the Stage 4 works, a period of circa 2.5 years.

4.6.2 Replacement public car parking is available in proximity to the station. Some examples are provided below in Table 4-2.

Table 4-2 Details of alternative public car parking

Location	Type	Address	No. of parking spaces
Huddersfield Albion Street Roof RingGo: 84178	Car park Up to 10 hours	Albion Street HD1 2NW	41
Huddersfield Bath Street RingGo: 84181	Car park Up to 10 hours 3.0 metres max height	Bath Street HD1 5BQ	54
Huddersfield Huddersfield Bus Station RingGo: 84172	Car park Up to 14 hours 2.06 metres max height	Upperhead Row/Bus Station HD1 2JN	225

- 4.6.3 Four accessible parking spaces are currently provided at the station and this provision will be maintained during the works. A 2-3m access route will be provided around the external of the car park to facilitate drop off access for mobility impaired passengers.
- 4.6.4 In line with the External Communications Programme (document ref: 151667-TSA-00-TRU-CNT-W-LP-000552) submitted against Condition 5b(i) (Stage 1), signage and advertising of any changes to the parking provision will be undertaken prior to the temporary closure of the formal station car park to inform users of the alternative arrangements and likely duration of the works (at least one month's notice will be given of such changes). Formal parking at the station will resume upon completion of the construction works.

5. TRAVEL PLAN FOR CONSTRUCTION STAFF

5.1 Staff levels

5.1.1 Staff levels for Stage 4 will comprise a total of around 195 staff and 425 operatives at the compounds set out at paragraph 4.1.1. The majority of staff will be based at the Fitzwilliam Street site.

5.1.2 The workforce required for the main track works will be detailed in the Stage 5 submission.

5.2 Sustainable modes of travel

5.2.1 The Scheme will seek to reduce dependence on the private car as far as is practicably possible. The primary objective is to minimise the number of single occupancy car journeys by employees, especially during the peak hours, through actively encouraging the use of more sustainable modes of travel such as walking, cycling, public transport and car sharing to reflect current policy at both a national and local level.

5.2.2 The benefits of using sustainable modes of travel include:

- Delivery of a long-term commitment to changing travel habits, thus delivering environmental health and road safety improvements;
- Reduction in unnecessary travel to and from the site and ensure that those that do have to travel do so in a sustainable manner; and
- Minimising the traffic impact of the development on the wider highway network within the Kirklees area.

5.2.3 The majority of the Stage 4 works are located close to the existing railway station at Huddersfield, and so staff will be encouraged to travel to this point where possible.

5.2.4 Every encouragement, including incentives, will be offered to those travelling to and from the site to use sustainable modes of transport; to be able to walk, cycle or use public transport in a safe and secure manner. Incentivisation for car sharing, use of public transport and walking will be made available to staff.

5.2.5 Cycle racks will be provided at all compound locations to encourage cycling to work. Walking from the parking location will also be encouraged to minimise vehicles on the highway network.

5.2.6 Site vehicles will be electric where practicable and Electric Vehicle Charging Points for site vehicles will be installed where possible.

5.3 Communication

5.3.1 Relevant information will be made available in a 'Travel Information Pack' which will be provided to each employee. Relevant information will be also placed on centrally-located notice boards/screens for the benefit of staff and visitors.

5.3.2 The information will include:

- Pedestrian access routes to the compound sites;
- Details of bus services (timetable information and updates) and bus stop locations;
- Details of train services (timetables and updates);
- Public transport fare information;
- Bicycle route maps;

- Information on local footpaths; and
- Information on car-share schemes (including incentives provided by the TRU West Alliance).

5.3.3 The site manager will be responsible for production of a Travel Questionnaire which will be issued to all employees within three months of start on site, ensuring sufficient time is allowed for employees to become accustomed to their new location and to adapt their travel habits accordingly. The questionnaire will aim to gather information regarding employees' existing travel habits and any measures that they would like to see introduced to further facilitate sustainable travel from the site. The results of the Travel Questionnaire will be recorded in a database which will allow for the identification (and updating) of suitable targets as well as new measures to be introduced in future stages of the development.

5.4 Parking

- 5.4.1 The logistics surrounding parking have been designed to encourage staff to utilise public transport and car sharing.
- 5.4.2 Where car journeys on the local road network are made, staff will be encouraged to arrive on site before 07:30.
- 5.4.3 Parking locations are to be kept away from busy urban environments where possible. During blockades, alternative transport and parking will be provided away from the busy urban areas at peak times.
- 5.4.4 At a minimum all personnel parking will be contained within construction compounds designed for the purpose.
- 5.4.5 The main parking locations for the Stage 4 works will be at Compound 4 - Fitzwilliam Street.
- 5.4.6 All staff will be briefed on the parking location for their chosen work prior to starting on site. Parking spaces will be allocated to staff and no staff parking will be allowed without a permit.
- 5.4.7 Transport will be provided from the parking locations to the work sites in the form of a 52-seat coach. Minibuses will also be available 24 hours a day, 7 days a week. All site transport will be provided free of charge to staff and visitors.
- 5.4.8 Localised signage will be installed to discourage on-street parking by staff.
- 5.4.9 The procedure to manage parking for the Scheme comprises:
- Establish system and procedure;
 - Inform all staff of process at induction;
 - Monitor compliance; and
 - Discipline action taken if required for non-compliance.

6. HIGHWAY CLOSURES AND DIVERSIONS

- 6.1.1 Every effort has been made to avoid temporary closures of roads during construction works, however there are some cases where this is unavoidable to complete the required works during Stage 4.
- 6.1.2 Further details of these temporary road closures are provided as follows, details in relation to the dates, durations, proposed diversion routes and appropriate signage will be discussed and agreed via the Highways Network Management Group:

John William Street

- 6.1.3 Various closures are required for the highway and footways at John William Street to allow works to the John William Street Underbridge (MVL3/92(1)). Full closures of the highway are planned during March 2025 and October 2025 to January 2026 (dates to be confirmed through the Highways Network Management Group). Temporary closure of single lanes of the highway and the footway will be in place at various times between July 2023 and February 2026, again to be confirmed through the Highways Network Management Group and appropriate signage will be implemented to warn users of any required closures. The proposed diversion route is via Castlegate, Southgate and Kirkgate into the town centre.

Fitzwilliam Street

- 6.1.4 Various closures are required for the highway and footways at Fitzwilliam Street to allow works to the John William Street Underbridge (MVL3/92(1)). Full closures of the highway are planned during May 2025, July 2025, September 2025 and October to December 2025 (dates to be confirmed through the Highways Network Management Group). Temporary closure of single lanes of the highway and the footway will be in place at various times between June 2023 and December 2025, again to be confirmed through the Highways Network Management Group and appropriate signage will be implemented to warn users of any required closures. The proposed diversion route is via Castlegate, Southgate and Kirkgate into the town centre.

Castlegate

- 6.1.5 Works to Huddersfield Viaduct (MVL3/92) will result in the closure of Castlegate for a 3-month period (December 2024 to March 2025, with exact dates to be confirmed through the Highways Network Management Group). The diversion route will be Castlegate to Trinity Street and Westgate then back up John William Street.

Northgate

- 6.1.6 Various closures are required for the highway and footways at Northgate to allow works to Huddersfield Viaduct Span 29 – Bradford Road Underbridge (MVL3/92(29)). Full closures of the highway are planned during February 2024 to July 2024 (dates to be confirmed through the Highways Network Management Group). Temporary closure of single lanes of the highway and the footway will be in place at various times between June 2023 and January 2026, again to be confirmed through the Highways Network Management Group and appropriate signage will be implemented to warn users of any required closures. A diversion route will be put in place via Bradford Road at Willow Lane East and then via Hillhouse Lane to the A62, re-joining the ring road via Lower Fitzwilliam Street. There is a local diversion from Bradford Road via Ray Street to Great Northern Street to Lower Fitzwilliam Street and then onto the ring road (Southgate).

Oxford Street

- 6.1.7 Works to Huddersfield Viaduct (MVL3/92) will result in the closure of Oxford Street for a 3-

month period (December 2024 to March 2025, with exact dates to be confirmed through the Highways Network Management Group). The diversion route will be via Castlegate to St John's Road.

Other works

- 6.1.8 Temporary lane closures or footway closures may also be required at the following locations over the period July 2023 to February 2026.
- Westgate
 - Viaduct Street
 - St John's Road
 - Green Street
 - Unna Way
 - Bradford Road
 - Lower Viaduct Street
- 6.1.9 Dates and durations will be confirmed through the Highways Network Management Group and appropriate signage will be implemented to warn users of any required closures

7. RIGHTS OF WAY IMPACTS

- 7.1.1 This section provides details in relation to the PRoW which will temporarily require stopping up whilst construction works during Stage 4 are being undertaken.
- 7.1.2 A temporary closure of footpath HUD/338/10 will be required during construction works on Huddersfield Viaduct (MVL3/92), as shown in Figure C1 in Appendix C. The diversion route will be via Lower Viaduct Street, onto Ray Street and then up Alder Street.
- 7.1.3 The temporary closure will commence on 1 August 2023 and at present it is anticipated that this temporary stopping up will be required for the duration of construction works in this area, i.e. for a period of circa 2.5 years.
- 7.1.4 Details of the required temporary closure will be submitted to the Highways Network Management Group including any update to closure periods/durations.
- 7.1.5 The TRU West Alliance will place signs up at appropriate locations informing users of the PRoW network of any closures or diversions, the form of which will be agreed with the local authority.

8. RAIL REPLACEMENT STRATEGY

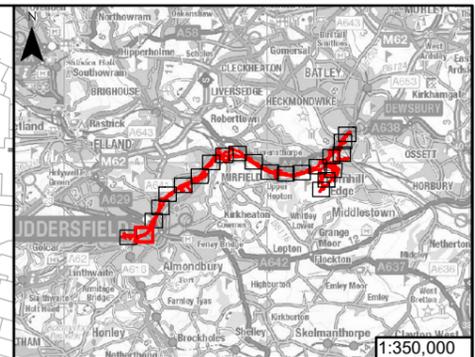
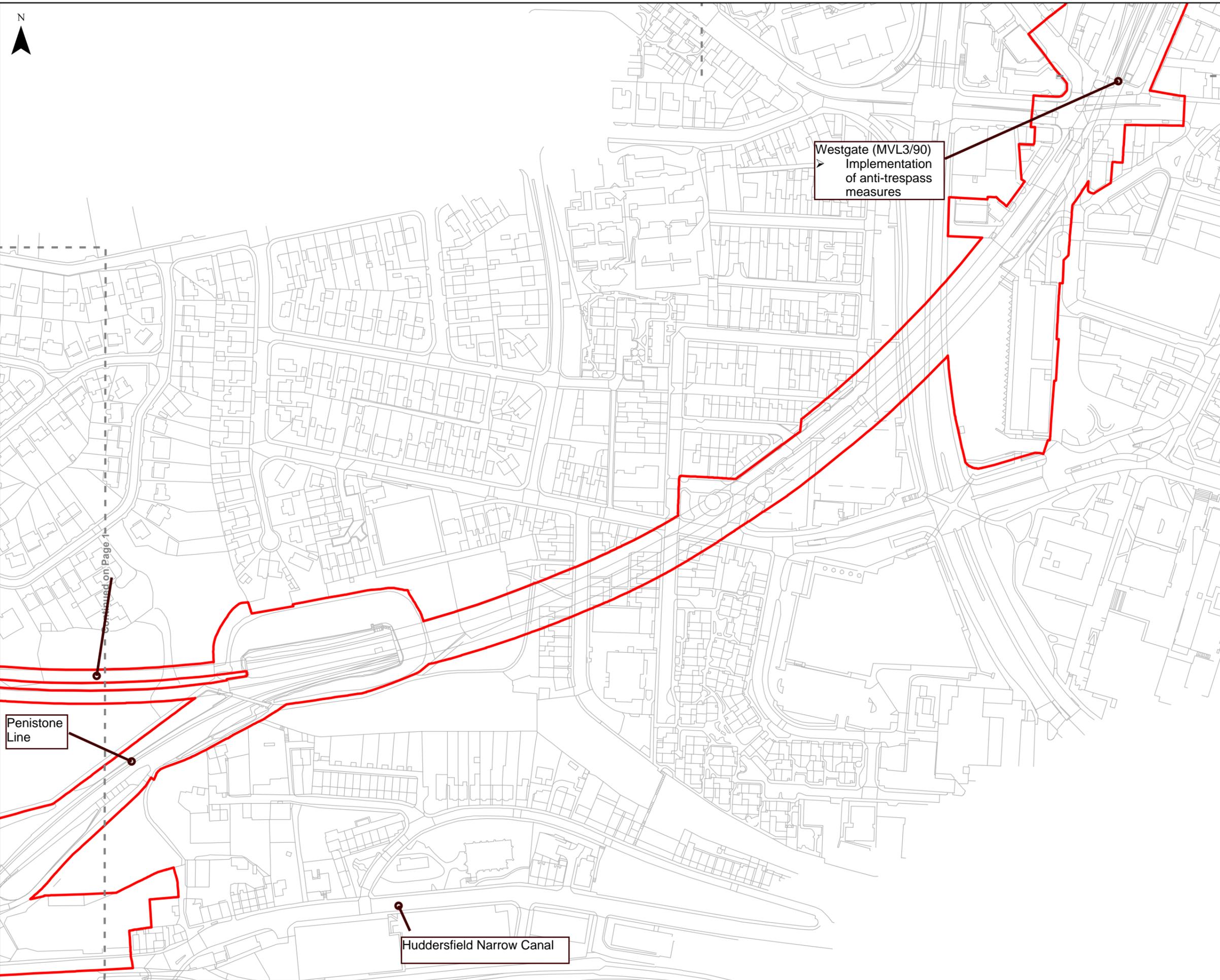
8.1 Rail Replacement Services at Huddersfield Station

Railway closures

- 8.1.1 During works within the operational railway boundary, there will be a need to temporarily close the railway to passenger services overnight. During this time services may be diverted to keep passengers moving by train where possible and where necessary rail replacement bus services will be provided. The location for the rail replacement services at Huddersfield Station is the Bus stop on St George's Square outside the station front.
- 8.1.2 Provision will be made within the Hillhouse Compound Site for a temporary railway platform for use while Huddersfield Station is being remodelled. This 150m length temporary platform will be constructed within the eastern extent of the Site to enable passenger train services to Hillhouse Construction Compound continue to operate during the planned closures (blockades) at Huddersfield Station during the construction phase of TRU. The temporary platform will include a Public Address (PA) system, welfare facilities for customers and staff and an emergency egress.
- 8.1.3 During its use, the temporary platform will be served by a bus replacement service. Passengers would purchase tickets at Huddersfield Station and would be transported directly from Huddersfield Station by replacement bus to the Hillhouse compound site, being dropped off at the temporary platform; this would ensure that there is no unauthorised access onto the Hillhouse site by the public, which may conflict with the construction works/traffic.
- 8.1.4 These alternatives are led by the Train Operating Companies and information will be advertised via the TRU website [Upcoming Closures - Transpennine Route Upgrade \(thetrupgrade.co.uk\)](http://thetrupgrade.co.uk) as well as through articles in the local press and via messaging on trains and at railway stations affected.

Appendices

APPENDIX A – WORKS LOCATION PLAN



Scheme Boundary
 Adjacent Map Sheet



P01	12/02/21	FIRST ISSUE	RB	PB	PB
Rev	Date	Description of Revisions	Drwn	Chkd	Appr
Status	SHARED				Suitability



Project
TRANSPENNINE ROUTE UPGRADE
 Contract No.
151667
 Scheme Title
THE NETWORK RAIL (HUDDERSFIELD TO WESTTOWN (DEWSBURY) IMPROVEMENTS) ORDER

Drawing Title

Figure 1

Stage 4 works

Designed	R.Bowes	Signed Electronically	Date	12/02/2021
Drawn	R.Bowes	Signed Electronically	Date	12/02/2021
Checked	P.Butler	Signed Electronically	Date	12/02/2021
Approved	P.Butler	Signed Electronically	Date	12/02/2021

Scale(s)
 1:2,500
 ELR & Project Chainage

Alternative Reference

 Sheet
 2 of 22

Drawing Number
 151667-TSA-00-TRU-REP-W-EN-001370
 Revision
 P01.1

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Railway works include

- 4 tracking
- Horizontal track realignment
- Relaying of ballast and tracks

Continued on Page 4

Halfords - access to business to be maintained

Huddersfield Viaduct (MVL3/92) Bradford Road

- Existing bridge deck to be removed and replaced
- Additional masonry pilasters/butresses will be formed at the corners of the new abutments into which the concrete parapet will join.
- New concrete abutments to be built with stone facing in front of existing sandstone at both ends and, on both elevations.

Huddersfield Viaduct – Span 4 – Fitzwilliam Street Underbridge

- Substructure repairs including removal and reinstatement of the pier to the south-western corner of the structure
- Strengthening to cross girders
- Removal of parapet and replacement with replica in cast iron

Huddersfield Viaduct - general strengthening works along the length of viaduct together with localised repairs to arches where necessary. These works include pinning and grouting, shear anchors and spandrel strengthening with tie bar and pattress plates.

DNO - Fitzwilliam Street

Huddersfield Viaduct Span 1 (MVL3/929 (1)) - John William Street

- existing bridge deck will be removed and replaced with a new single span bridge deck

Fitzwilliam Street Construction Compound

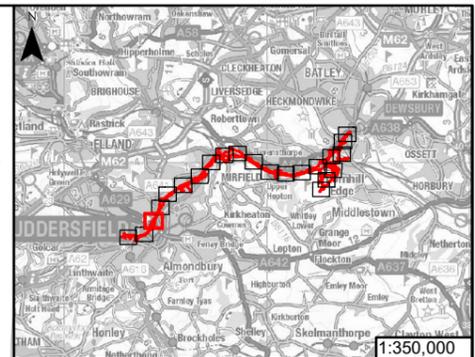
Huddersfield Station - construction compound

Brian Jackson House

Huddersfield Station

- main train shed - structural works, two bays at Manchester end of canopy to be demolished, two new bays at Leeds end of platform 1; reinstatement of main train shed lantern;
- smaller train shed (canopies B&C) to be demolished, new roof to be constructed;
- free standing canopies over island platforms
- extension to canopy to service Penistone line;
- tea rooms to be deconstructed and relocated;
- a covered footbridge (with lift and stairs) to be constructed (step free access)
- concrete infill of existing parcel subway
- extension to existing passenger subway to service new island platform to north of station
- Grit blasting of main train shed
- Works to railway lines including provision of new platforms and removal of existing railway sidings
- Demolition of existing relay room

Continued on Page 2



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Scheme Boundary

Adjacent Map Sheet



P01	12/02/21	FIRST ISSUE	RB	PB	PB
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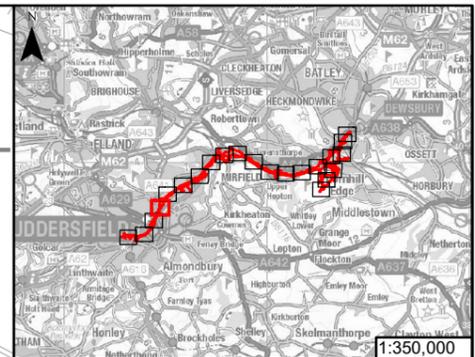
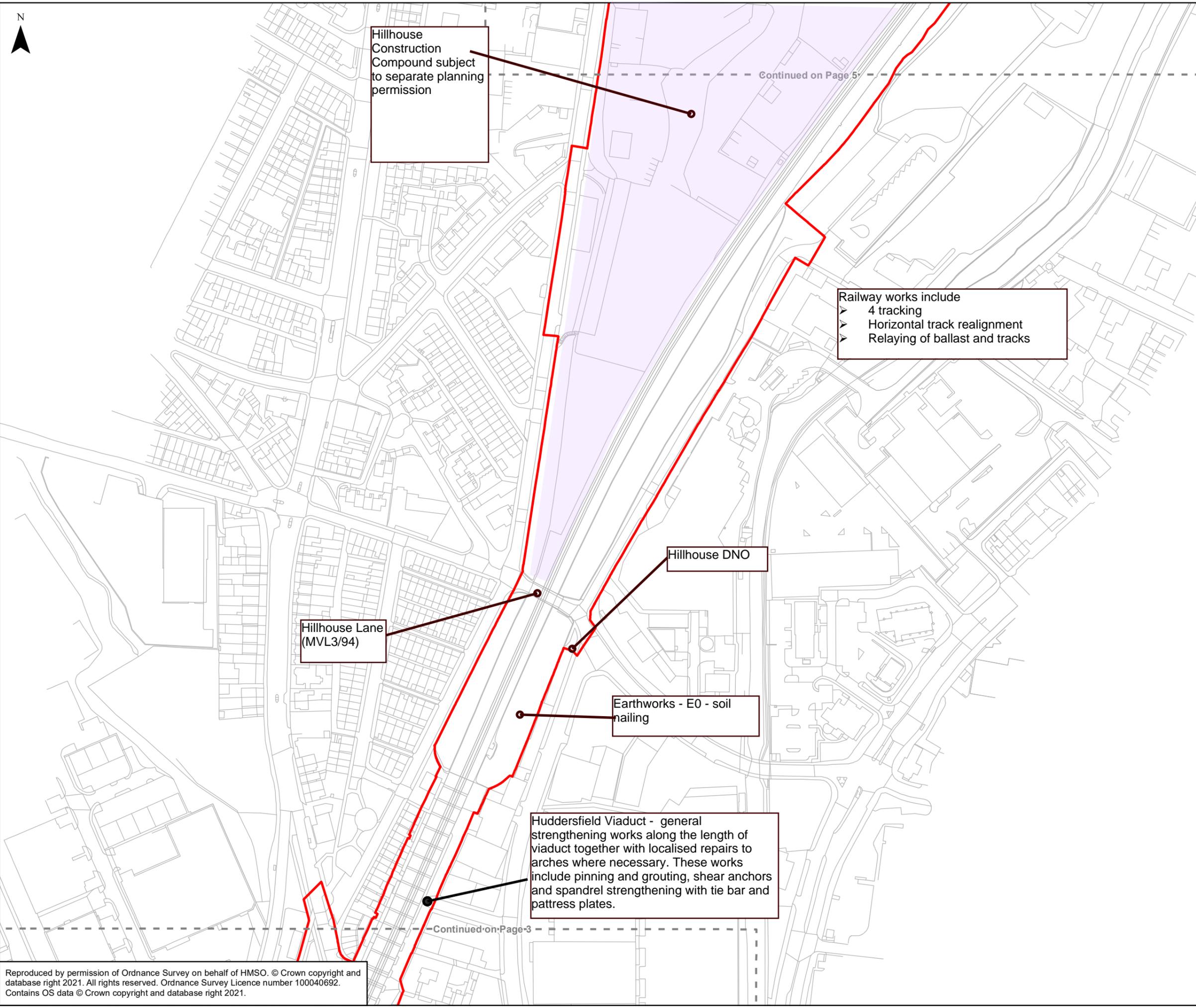
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THE NETWORK RAIL (HUDDERSFIELD TO WESTTOWN (DEWSBURY) IMPROVEMENTS) ORDER

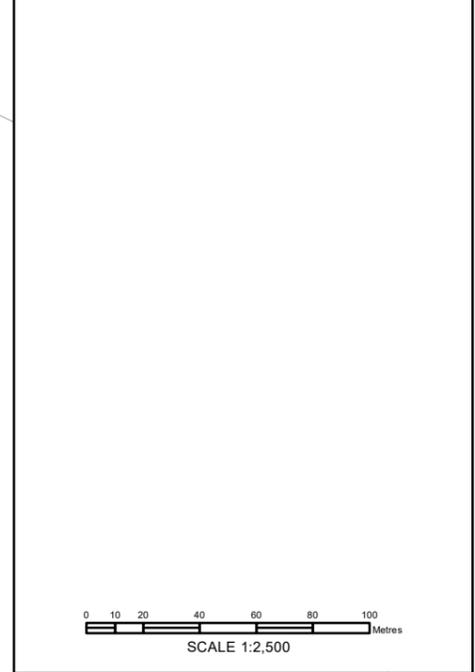
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**Figure 1
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Drawn	R.Bowes	Signed Electronically	Date	12/02/2021
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Approved	P.Butler	Signed Electronically	Date	12/02/2021
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				3 of 22
Drawing Number	151667-TSA-00-REP-W-EN-001370	Revision	P01.1	



- Scheme Boundary
- Adjacent Map Sheet



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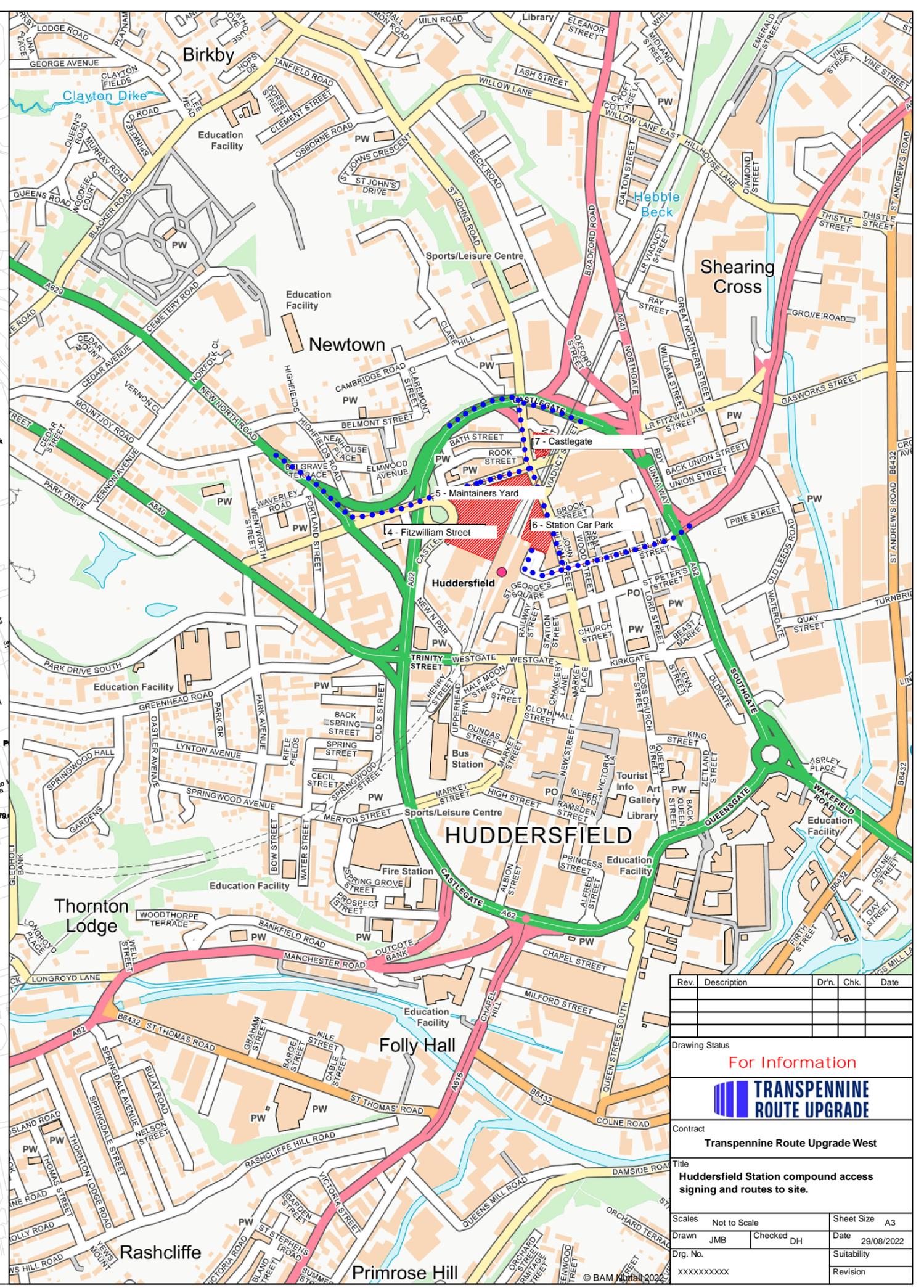
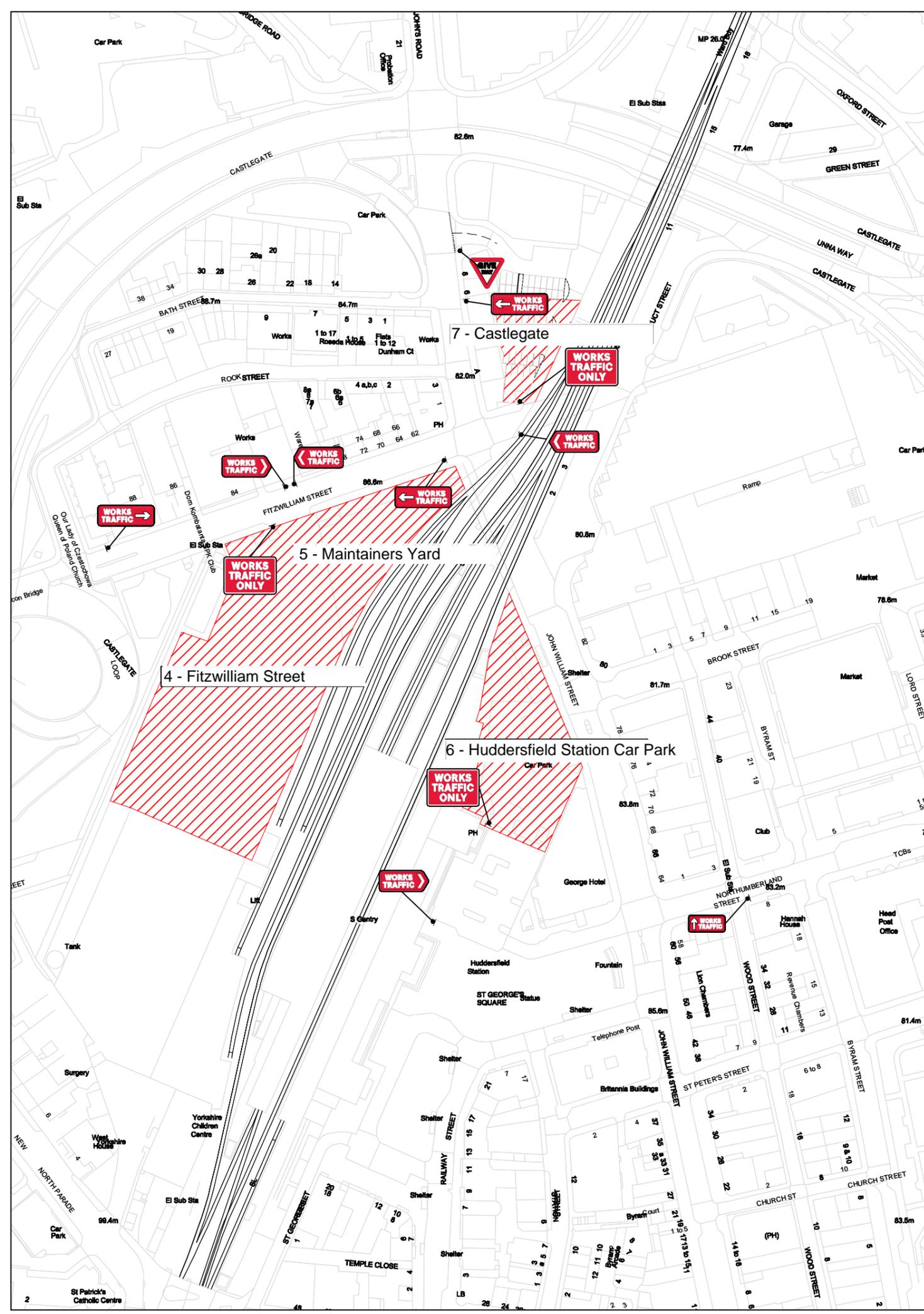
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APPENDIX B – DETAILS OF CONSTRUCTION COMPOUND SIGNAGE AND ACCESS / EGRESS POINTS

FIGURE B2 – COMPOUNDS 4 – 7 (FITZWILLIAM STREET, MAINTAINERS YARD, HUDDERSFIELD STATION CAR PARK AND CASTLEGATE)



Rev.	Description	Dr'n.	Chk.	Date

Drawing Status

For Information

TRANSPENNINE ROUTE UPGRADE

Contract
Transpennine Route Upgrade West

Title
Huddersfield Station compound access signing and routes to site.

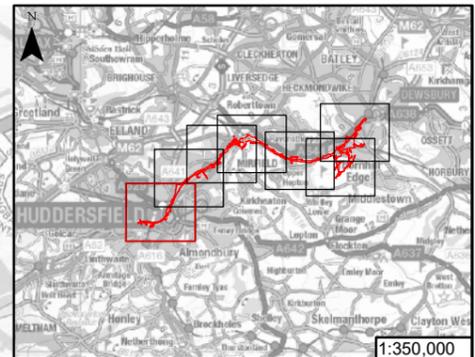
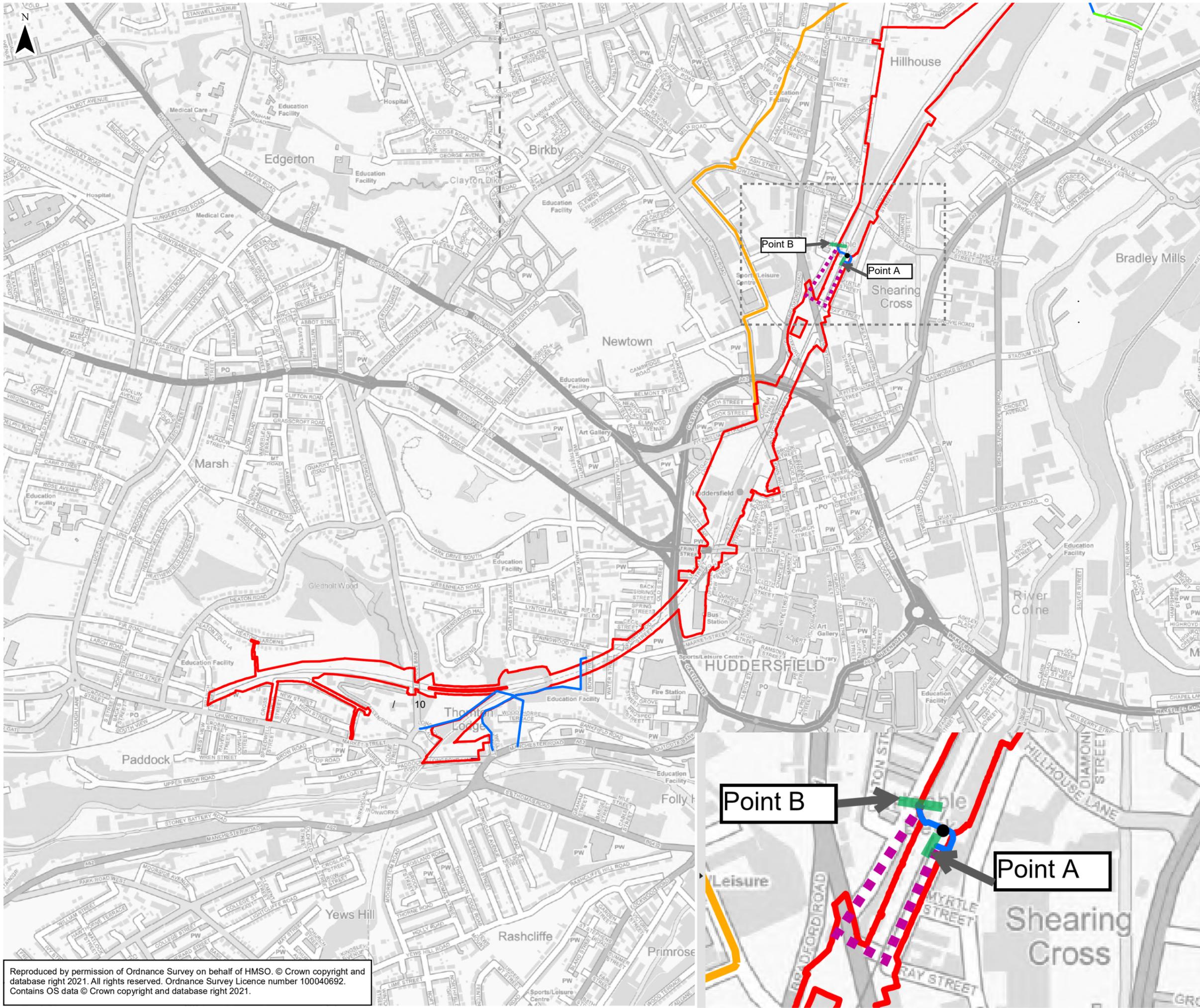
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		Date	29/08/2022
Drg. No.	XXXXXXXXXX	Suitability	
		Revision	

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FIGURE B3 – COMPOUND 10A (HALFORDS/BRADFORD ROAD)

APPENDIX C – WORKS AFFECTING PUBLIC RIGHTS OF WAY

FIGURE C1 – PROW HUD/338/10 (FOOTPATH)



- ▬ Scheme Boundary
- ▬ Proposed Temporary Diversion
- ▬ Cycleway
- ▬ Byway
- ▬ Footpath



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P01	19/03/21	FIRST ISSUE	RB	RM	PB	



Project
TRANSPENNINE ROUTE UPGRADE

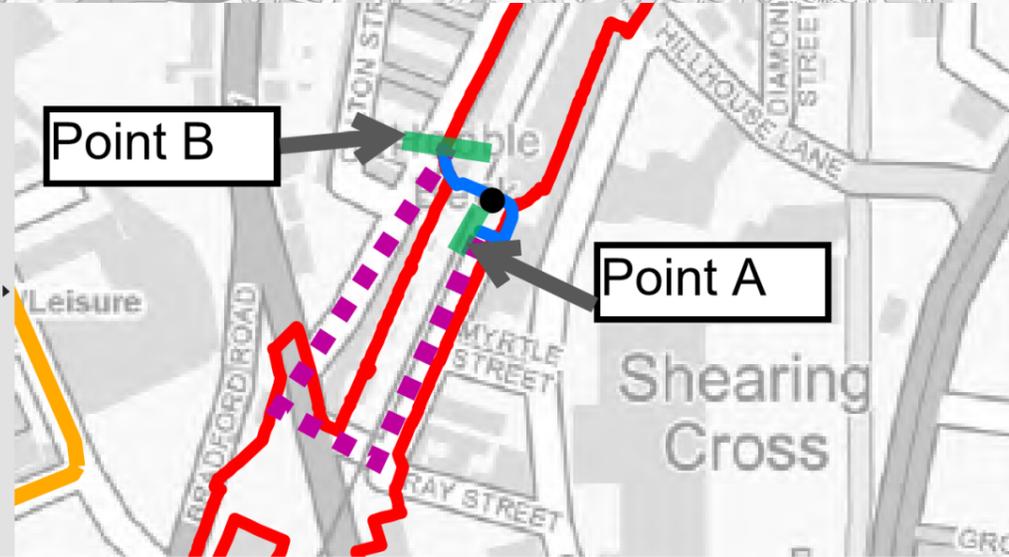
Contract No.
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Scheme Title
THE NETWORK RAIL (HUDDERSFIELD TO WESTTOWN (DEWSBURY) IMPROVEMENTS) ORDER

Drawing Title
FIGURE C1
WORKS TO PROW HUDD/338/10

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Approved	P. Butler	Signed Electronically	Date	23/03/2021

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Alternative Reference	---	Sheet	1 of 7
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