

Network Rail (Huddersfield to Westtown (Dewsbury) Improvements) Order

Condition 5b(ii): Pollution Prevention and Incident Control Plan – Stage 4

Document reference: 151667-TSA-00-TRU-REP-W-EN-001240

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1. INTRODUCTION

1.1 Background

- 1.1.1 The Scheme is part of a wider programme of works under the Transpennine Route Upgrade (TRU) which will improve the Transpennine railway between Manchester, Huddersfield, Leeds and York and improve connections between key towns and cities across the north of England.
- 1.1.2 Planning Direction for the Huddersfield to Westtown (Dewsbury) section of the TRU was received from the Department for Transport referenced TWA/21/APP/03, dated 13 October 2022.
- 1.1.3 This Pollution Prevention and Incident Control Plan (PPICP) sets out details in relation to Condition 5b(ii) of the Deemed Planning Permission.

2. STAGED APPROACH TO DISCHARGE AND STAGE DESCRIPTION

- 2.1.1 As set out in document ref: 151667-TSA-00-TRU-REP-W-EN-001189 version 3 (submitted in relation to Condition 3 of the Deemed Planning) a staged approach is proposed in relation to discharge of the deemed planning conditions.
- 2.1.2 This document sets out details in relation to Stage 4 of the works for the Huddersfield to Westtown (Dewsbury) Scheme.
- 2.1.3 Stage 4 comprises the main civils works at Huddersfield Station and Huddersfield Viaduct (Stage 4 limits are set as Westgate Overbridge and Hillhouse Lane Underbridge) and are set out in Table 2-1, as well as links to the relevant planning drawings. Figure 1 in Appendix A shows the geographical locations of the works. Route drawings relevant to Stage 4 are [NR13 Planning Drawing - Route Drawing 3.pdf \(windows.net\)](#) and [NR13 Planning Drawing - Route Drawing 4.pdf \(windows.net\)](#).
- 2.1.4 Works within the Huddersfield and Gledholt tunnels, to the west of the station, will be detailed in the Stage 5 submission.
- 2.1.5 The entire Scheme will be subject to electrification; details of the electrification works will be detailed in the Stage 5 submission.
- 2.1.6 At Huddersfield station remodelling works are required with alterations to platforms and roof structures required to facilitate the delivery of a four track railway. East of Huddersfield Station the four-track railway is reinstated across the viaduct.
- 2.1.7 There are existing earthworks throughout the Scheme area associated with the existing operational railway. Earthworks allow the track to stay relatively level through a varied topography and allows trains to operate more efficiently by reducing the need for additional acceleration and deceleration to climb and descend climbs.
- 2.1.8 As set out in Figure 2-1¹ in Volume 4 of the Environmental Statement (ES) and Table 2-4 in Chapter 2: Scheme Description (Route Section 1)² in Volume 2i of the ES, a 55m length retaining structure (0.5m high) was proposed along the eastern end of Huddersfield Viaduct (MVL3/92) near to Hillhouse Lane Underbridge (MVL3/94). This was proposed as either king post wall or soil nailing. Following design iteration, soil nailing is planned in this area of existing embankment to the south of the railway (E0).
- 2.1.9 Earthworks (new and where they have been reworked) will generally be covered in topsoil and landscaped as appropriate. Any exceptions to this will be detailed within the Landscape and Ecological Management Plan (LEMP) Stage 8.
- 2.1.10 The Scheme impacts on various existing transmission and distribution utility networks. Conflicts with utility services may occur in Stage 4 where the Scheme crosses highways and. Works within the highway will be carried out in compliance with the Highways Agreement and any impacts on the highways network will be discussed through the Highway Network Management Group.

¹ [Ch02 Scheme Description - Fig 2-1 Scheme drawings.pdf \(windows.net\)](#)

² [Ch02 Huddersfield - Scheme Description.pdf \(windows.net\)](#)

Table 2-1 Works description

Location	Structure/works	Summary description	Deemed Planning Drawing Title and Reference
Huddersfield Station	Passenger Footbridge	A covered footbridge (Huddersfield Station Footbridge (MVL3/91AA), with stairs and a lift, to be constructed to the eastern end of the station. This will provide step free access to the central platforms.	<ul style="list-style-type: none"> • Footbridge - Proposed Elevations - 151667-TSA-30-MVL3-DRG-T-LP-168053 • Footbridge - Proposed Plan Deck Level - 151667-TSA-30-MVL3-DRG-T-LP-168051 • Footbridge - Proposed General Arrangement Platform Level - 151667-TSA-30-MVL3-DRG-T-LP-168050 • Footbridge - Proposed Roof Level General Arrangement - 151667-TSA-30-MVL3-DRG-T-LP-168052 • Footbridge - Proposed Sections - 151667-TSA-30-MVL3-DRG-T-LP-168054 • Existing and Proposed Long Sections (A-A) - 151667-TSA-30-MVL3-DRG-T-LP-168003 • Existing and Proposed Long Sections (B-B) - 151667-TSA-30-MVL3-DRG-T-LP-168005
Huddersfield Station	Passenger Subway	Extension to existing Subway (MVL3/91) (of 12.5m) required to service the new island platform to the north of the station.	<ul style="list-style-type: none"> • Existing Plan and Sections - 151667-TSA-30-MVL3-DRG-T-LP-168064 • Proposed Plan and Section - 151667-TSA-30-MVL3-DRG-T-LP-168065
Huddersfield Station	Parcel Subway (MVL3/91A)	Utilising existing subway for utilities ducting and signalling equipment. Concrete infill.	<ul style="list-style-type: none"> • Existing Plan and Sections - 151667-TSA-30-MVL3-DRG-T-LP-168067 • Proposed Plan and Section - 151667-TSA-30-MVL3-DRG-T-LP-168068
Huddersfield Station	Tearooms	Existing Tea Rooms - Timber structure to be carefully dismantled and relocated within island platform. To be dismantled and reconstructed.	<ul style="list-style-type: none"> • Existing Floor Plan and Elevations - 151667-TSA-30-MVL3-DRG-T-LP-168016 • Existing and Proposed Elevations - 151667-TSA-30-MVL3-DRG-T-LP-168015 • Proposed Floor Plan and Elevations - 151667-TSA-30-MVL3-DRG-T-LP-168017
Huddersfield Station	Proposed Platforms	Works to railway lines including provision of new platforms and removal of existing railway sidings.	<ul style="list-style-type: none"> • Existing Plan - 151667-TSA-30-MVL3-DRG-T-LP-168060 • Existing and Proposed Platform General Arrangement - 151667-TSA-30-MVL3-DRG-T-LP-168001 • Proposed Plan and Section - 151667-TSA-30-MVL3-DRG-T-LP-168061 • Proposed Plan and Section - 151667-TSA-30-MVL3-DRG-T-LP-168062 • Proposed Plan and Section - 151667-TSA-30-MVL3-DRG-T-LP-168063

Location	Structure/works	Summary description	Deemed Planning Drawing Title and Reference
Huddersfield Station	Canopy A	<p>Works to the main train shed within Huddersfield Station includes:</p> <ul style="list-style-type: none"> • Structural works to maintain and strengthen; • Demolition of two canopy bays at Manchester end of the station; • Construction of two new bays at Leeds end; • Grit blasting of structure; and • Reinstatement of main train shed lantern. 	<ul style="list-style-type: none"> • Existing and Proposed Roof General Arrangement - 151667-TSA-30-MVL3-DRG-T-LP-1680000 • Existing and Proposed Short Sections (A-A) - 151667-TSA-30-MVL3-DRG-T-LP-168002 • Existing and Proposed Short Sections (B-B) - 151667-TSA-30-MVL3-DRG-T-LP-168004 • Existing Roof A Structural Plan (Roof Level) - 151667-TSA-30-MVL3-DRG-T-LP-168010; • Existing Roof A Structural Sections Sheet (1) - 151667-TSA-30-MVL3-DRG-T-LP-168011 • Existing Roof A Structural Sections Sheet (2) - 151667-TSA-30-MVL3-DRG-T-LP-168012; • Existing Roof A OLE Support Details - 151667-TSA-30-MVL3-DRG-T-LP-168013
Huddersfield Station	Canopy B & C	<p>Canopy B&C are to be demolished with new replacements to be constructed to cover platforms to north.</p>	<ul style="list-style-type: none"> • Existing and Proposed Roof General Arrangement - 151667-TSA-30-MVL3-DRG-T-LP-168000 • Existing and Proposed Short Sections (A-A) - 151667-TSA-30-MVL3-DRG-T-LP-168002 • Existing and Proposed Short Sections (B-B) - 151667-TSA-30-MVL3-DRG-T-LP-168004 • Proposed Roof B (Shed Roof) Structural Plan (Roof Level) - 151667-TSA-30-MVL3-DRG-T-LP-168020 • Proposed Roof B (Shed Roof) Structural Plan (Platform Level) - 151667-TSA-30-MVL3-DRG-T-LP-168021 • Proposed Roof B (Shed Roof) Structural Sections (1) - 151667-TSA-30-MVL3-DRG-T-LP-168022 • Proposed Roof B (Shed Roof) Structural Sections (2) - 151667-TSA-30-MVL3-DRG-T-LP-168023 • Proposed Roof B (Shed Roof) Structural Sections (3) - 151667-TSA-30-MVL3-DRG-T-LP-168024
Huddersfield Station	Platform free standing canopies	<p>Free standing canopies to be constructed over island platforms to eastern end of station.</p>	<ul style="list-style-type: none"> • Existing and Proposed Roof General Arrangement - 151667-TSA-30-MVL3-DRG-T-LP-168000 • Proposed Platform General Arrangement - 151667-TSA-30-MVL3-DRG-T-LP-168030

Location	Structure/works	Summary description	Deemed Planning Drawing Title and Reference
			<ul style="list-style-type: none"> Proposed Platform Canopies Elevation (1) - 151667-TSA-30-MVL3-DRG-T-LP-168034 Proposed Platform Canopies Elevation (2) - 151667-TSA-30-MVL3-DRG-T-LP-168035 Proposed Platform Canopies Structural Plan (Platform Level) - 151667-TSA-30-MVL3-DRG-T-LP-168031 Proposed Platform Canopies Structural Sections - 151667-TSA-30-MVL3-DRG-T-LP-168032 Proposed Platform Canopies Structural Sections - 151667-TSA-30-MVL3-DRG-T-LP-168033
Huddersfield Station	Canopies – Penistone Line	Extension of Penistone Line canopies.	<ul style="list-style-type: none"> Proposed Penistone Line Canopy Platform Level Plan General Arrangement - 151667-TSA-30-MVL3-DRG-T-LP-168036 Proposed Platform Penistone Canopies Structural Sections - 151667-TSA-30-MVL3-DRG-T-LP-168037 Proposed Platform Penistone Canopies Proposed Roof Covering Plans - 151667-TSA-30-MVL3-DRG-T-LP-168038 Proposed Platform Penistone Canopies Elevation (1) - 151667-TSA-30-MVL3-DRG-T-LP-168039
Huddersfield Station	Relay Room	Existing relay room to be demolished.	<ul style="list-style-type: none"> Existing and Proposed Platform General Arrangement - 151667-TSA-30-MVL3-DRG-T-LP-168001
Huddersfield Station	Drainage Works	Fitzwilliam Street sewer outfall (New) SE 1430 1707. Proposed new storm water drainage outfall for the re-modelled areas of Huddersfield Station. It will be a piped outfall from the drainage system, either directly into the sewer in the highway, or into an existing culvert within Network Rail land which connects into this sewer. A new manhole will be provided at the outfall.	<ul style="list-style-type: none"> No relevant planning drawings

Location	Structure/works	Summary description	Deemed Planning Drawing Title and Reference
Huddersfield Viaduct (MVL3/92)		Works across Huddersfield Viaduct includes general strengthening works along the length of viaduct together with localised repairs to arches where necessary. These works include pinning and grouting, shear anchors and spandrel strengthening with tie bar and pattress plates.	<ul style="list-style-type: none"> Existing and Proposed East Elevation (Sheet 1) - 151667-TSA-30-MVL3-DRG-T-LP-168075 Existing and Proposed East Elevation (Sheet 2) - 151667-TSA-30-MVL3-DRG-T-LP-168076 Existing and Proposed East Elevation (Sheet 3) - 151667-TSA-30-MVL3-DRG-T-LP-168077 Existing and Proposed East Elevation (Sheet 4) - 151667-TSA-30-MVL3-DRG-T-LP-168078 Existing and Proposed East Elevation (Sheet 5) - 151667-TSA-30-MVL3-DRG-T-LP-168079
Huddersfield Viaduct	Span 1 – John William Street Underbridge (MVL3/92(2))	The existing Span 1 bridge deck will be removed and replaced with a new single span bridge deck due to the current structural arrangement of the bridge clashing with the proposed track and platform works.	<ul style="list-style-type: none"> Existing Plan and Proposed Plan (Sheet 1) - 151667-TSA-30-MVL3-DRG-T-LP-168070 Existing Plan and Proposed Plan (Sheet 2) - 151667-TSA-30-MVL3-DRG-T-LP-168071 Existing Plan and Proposed Plan (Sheet 3) - 151667-TSA-30-MVL3-DRG-T-LP-168072
Huddersfield Viaduct	Span 4 – Fitzwilliam Street Underbridge (MVL3/92(3))	Substructure repairs including removal and reinstatement of the pier to the south-western corner of the structure. Strengthening to cross girders. Removal and replacement of parapet.	<ul style="list-style-type: none"> Existing Plan and Proposed Plan (Sheet 4) - 151667-TSA-30-MVL3-DRG-T-LP-168073 Existing Plan and Proposed Plan (Sheet 5) - 151667-TSA-30-MVL3-DRG-T-LP-168074 John William Street Bridge - Existing Highways General Arrangement - 151667-TSA-30-MVL3-DRG-T-LP-168081 John William Street Bridge - Proposed Highways General Arrangement - 151667-TSA-30-MVL3-DRG-T-LP-168082
Huddersfield Viaduct	Span 29 Bradford Road Underbridge (MVL3/92(9))	Existing bridge deck to be removed and replaced. Additional masonry pilasters/buttresses will be formed at the corners of the new abutments into which the concrete parapet will join. New concrete abutments to be built with stone facing in front of existing sandstone at both ends and, on both elevations.	

3. RELEVANT PLANNING CONDITION

3.1.1 The wording of Condition 5 sub-section (ii) is reproduced as follows:

5.a) No stage of the development (including preliminary works) is to commence until a Code of Construction Practice (CoCP) Part B for that stage, including the relevant plans and programmes referred to in (b) below (which incorporates the means to mitigate the construction impacts identified by the Environmental Statement), has been submitted to and approved in writing by the local planning authority. For the avoidance of doubt this does not include approval for Part A of the CoCP (a general overview and framework of environmental principles and management practice to be applied to the scheme along with all construction-led mitigation identified in the Environmental Statement) which has been submitted as part of the Order.

5.b) Part B of the CoCP (as defined in the Environmental Statement: Volume 3, Appendix 2-1 Code of Construction Practice (Part A), Section 1.2.5) must include the following plans and programmes, for each stage as defined in condition 3:-

ii) A pollution prevention and incident control plan;

The development must be implemented in accordance with the approved CoCP and the relevant plans or programmes unless otherwise agreed in writing with the local planning authority shall be implemented in full throughout the period of the works.

Reason: *To mitigate expected construction impacts arising from the development and to protect local and residential amenity and to ensure the development is carried out in accordance with Kirklees Local Plan policies LP51 and 52.*

4. POLLUTION PREVENTION AND INCIDENT CONTROL PLAN

4.1 Overview

- 4.1.1 This section includes information on the pollution prevention and incident control measures for the Stage 4 works as detailed in Section 2.
- 4.1.2 General pollution prevention and incident control measures applicable to all Stage 4 works are outlined in Section 4.3 and details pertaining to emergency response in Section 4.4.
- 4.1.3 The works in Table 4-1 have been identified as being adjacent to surface watercourses and as such will require specific water environment related pollution prevention measures to be in place during construction activities. Where applicable, specific measures in relation to these works are outlined in Section 4.5.

Table 4-1 Works adjacent to watercourses

Stage 4 works	Watercourse(s)
Earthworks E0	<ul style="list-style-type: none"> • Earthworks situated over the culverted section of Hebble Beck (Grimescar Dike), a Main River, where it flows beneath the railway and then under Great Northern Street. A large extent of Hebble Beck is highly modified with concrete/brick bank protection, blocked trash screens and sediment choking. The exposed section of Hebble Beck is located approximately 30 m southwest of Earthworks E0. • Earthworks E0 are also located around 45 m from the Huddersfield Broad Canal at its nearest point.

4.2 Management framework

Legislation, regulations and other requirements

- 4.2.1 The Stage 4 works will comply with all relevant legislation and regulations to ensure legal construction works.
- 4.2.2 Other requirements and regulations from the Local Authority, Highways Authority or other Statutory Bodies will be reviewed by the contractor and applied where applicable.
- 4.2.3 All work carried out will be conducted with due cognisance of client standards, obligations and best practice.
- 4.2.4 A legislation register will be maintained and updated following any changes to applicable legislation. Any applicable changes will be evaluated and communicated to the relevant personnel through environmental alerts, newsletters, staff briefings or toolbox talks. Site-specific procedures will also provide guidance to activity specific legislation.

Environmental and social policies

- 4.2.5 The Stage 4 works will be carried out in accordance with the relevant Network Rail standards and procedures including, but not limited to, the following:
- Network Rail Environmental Policy (NR/L2/ENV/015);
 - Procedure ENV01 Ecology and Biodiversity;
 - Procedure ENV02 Water Management;
 - Procedure ENV03 Oil Storage and Refuelling;
 - Procedure ENV04 Waste Management;

- Procedure ENV05 Control of Site Nuisance; and
- Procedure ENV06 Environmental Incident Response Plan.

4.2.6 It will be ensured that the policies, procedures and their requirements are made known to all relevant personnel. This will be undertaken through a number of methods including site inductions, Work Package Plans (otherwise known as method statements) and risk assessment briefings, and toolbox talks.

Roles and responsibilities

4.2.7 Staff, operatives and subcontractors have the authority and responsibility to protect the environment at all times during execution of the works; the responsibilities outlined in this section will be highlighted during the site induction. All personnel will be trained in the necessary skills to fulfil their role.

4.2.8 Key personnel for specific job roles are set out in the Table 4-2. The roles outlined may be substituted as required providing that the key environmental responsibilities are clearly and appropriately allocated.

4.2.9 Contact details for all key contractors will be displayed on notice boards in the site offices.

Table 4-2 Roles and responsibilities

Role	Key environmental responsibilities
Work Area Project Manager	<ul style="list-style-type: none"> • Responsible for ensuring the Environmental Management System for the project is implemented. • Ensures that the Network Rail Environmental Policy is drawn to the notice of all employees under his control. • Establishes effective lines of communication with all employees under their control. • Promotes the continuous improvement of environmental performance • Monitors and reviews the implementation of environmental objectives and targets on the project.
Principal Environment Manager	<ul style="list-style-type: none"> • Carries overall responsibility for delivery of the design stage of the project and for meeting environmental performance objectives and targets. • Audits the performance of the Environmental Social Management Plan • Ensures adequately trained and competent resources are provided to implement the Pollution Prevention and Incident Control Plan (PPICP). • Ensures that environmental risks are evaluated and considered during the planning stage of the project. • Interfaces between various design disciplines to ensure that environmental considerations have been taken account of in final design output. • Approves all specific or specialist environmental procedures that are required. • Responsible for setting and meeting project objectives and targets.
Site Environment Manager(s)	<ul style="list-style-type: none"> • Responsible for providing a focal point for all communications between the construction team and outside environmental bodies. • Responsible for the production of the ESMP and ensuring works are carried out in accordance with the measures set out in the document • Responsible for ensuring works are carried out in accordance with NR Standards see Section 4.2.5 and Parts A and B of the CoCP

Role	Key environmental responsibilities
	<ul style="list-style-type: none"> • Maintaining and revising the PPICP and all specific or specialist environmental procedures that are required. • All measures in the PPICP are implemented on Site. This includes ensuring that adequate resources are allocated to environmental management on site. • Collecting and collating the project’s environmental performance records. • Collating reportable environmental incident and NCR data, establishing cause and implementing actions to prevent reoccurrence • Reviews and approves risk assessments and Work Package Plans (RAMS) for environmental content. • Ensuring that internal environmental audits are undertaken and reported. • Drawing up measures for emergency preparedness and response procedures. • Environmental issues in risk assessments are communicated effectively on site and that appropriate training is delivered. • Producing monthly environmental reports and forwarding them to the Site Manager.
Work Site Manager(s)	<ul style="list-style-type: none"> • Responsible for management of the construction phase, and ensuring compliance with all relevant legal requirements, commitments and targets. • Ensures that site-specific training needs are identified, and training programmes are effectively undertaken. • Establishes and implements comprehensive environmental inductions, training awareness and education programmes for all level of site staff and operatives.
Resident liaison officer	<ul style="list-style-type: none"> • Ensures any enquiries or complaints directed to site staff are submitted to the Network Rail helpline. • Is the first point of contact for the Network Rail Community Relations team for enquiries or complaints that have been submitted to the Network Rail helpline. • Role will link directly with the Network Rail Community Relations team
All site staff	<ul style="list-style-type: none"> • Protect the environment and act sustainably. • Report any environmental concerns to their supervisors. • Comply with specified systems of work. • Promote and communicate newly developed best practice. • Ensure only staff who have the required understanding, qualifications, and where necessary certification, carry out the specialised tasks.

Training, awareness and competence

4.2.10 All personnel will receive specific and targeted information during site inductions.

4.2.11 All personnel, whose work may cause an impact on the environment, will receive environmental training specific to their task. This will be appropriate to their level and role and will include subcontractors and the wider supply chain, as appropriate.

4.3 General pollution prevention and incident control measures

4.3.1 Runoff may become contaminated via:

- Vehicles accessing compounds, refuelling, oil/fuel storage tanks and accidental spillage;

- Storage and use of construction materials/chemicals (e.g., solvents, degreasers) on site; and
- Runoff from construction compounds – stockpiles, drainage, wheel washings and material movement.

4.3.2 The following paragraphs detail preventative and response measures that will be in place to reduce the risk of pollution entering highway drainage as far as is reasonably practicable.

4.3.3 Additional pollution prevention measures are detailed in the Nuisance Management Plan (document ref: 151667-TSA-00-TRU-REP-W-EN-001242) and Noise and Vibration Management Plan (document ref: 151667-TSA-00-TRU-REP-W-EN-001243) for Stage 4.

4.3.4 The closest controlled water is Hebble Beck which rises to the north-west of the study area and flows in a south/south-easterly direction within the study area for approximately 2.2km before flowing into the River Colne close to Bradley Mills. It is mainly culverted in its lower sections, including where it transects the Scheme boundary, flows beneath the railway and crosses Huddersfield Broad Canal

4.3.5 Measures to prevent pollution to controlled waters will be employed, and all work will be completed in line with the following best practice guidance:

- Environment Agency's (EA) Pollution Prevention Guidance (PPG)³ including:
 - General Guide to Prevention of Pollution (PPG1)⁴. This guidance provides an introduction to the five main activities which should be looked at to help construction activities avoid pollution incidents and comply with the law.
 - Works and maintenance in or near water (PPG5)⁵. This guidance provides advice on how to make sure construction activities protect the environment and meet legal requirements when you are working near, in or over water.
 - Working at Construction and Demolition Sites (PPG 6)⁶. This guidance was developed in partnership with industry to provide guidance to those working at construction and demolition sites to prevent pollution.
- Construction Industry Research and Information Association (CIRIA) technical guidance:
 - CIRIA 532 - Control of water pollution from construction sites⁷
 - CIRIA 649 - Control of water pollution from linear construction projects. Site guide⁸.

4.3.6 The PPGs contain a mix of regulatory requirements and good practice advice. They have been withdrawn by the EA but are still considered good practice advice to avoid pollution of watercourses.

4.3.7 Pollution prevention measures include:

- Any storage of hydrocarbons, oils and chemicals be stored within bunded and lined areas to contain pollutants in the event of leakage.
- Any new car parks and associated access/haul roads will incorporate, at least, oil interceptors before discharging to the receiving body of water to intercept any oily

³ [\[ARCHIVED CONTENT\] Environment Agency - Pollution prevention advice and guidance \(PPG\) \(nationalarchives.gov.uk\)](https://www.nationalarchives.gov.uk)

⁴ [\[Withdrawn\] Basic good environmental practices, PPG1: prevent pollution - GOV.UK \(www.gov.uk\)](http://www.gov.uk)

⁵ Including PPG23 (maintenance of structures over water)

⁶ [ppg-6.pdf \(netregs.org.uk\)](http://netregs.org.uk)

⁷ https://www.ciria.org/CIRIA/CIRIA/Item_Detail.aspx?iProductCode=C532&Category=BOOK

⁸ <https://www.ciria.org/ItemDetail?iProductCode=C649&Category=BOOK&WebsiteKey=3f18c87a-d62b-4eca-8ef4-9b09309c1c91>

- substances that may build up on surfaces. Due to the measures being employed in line with this document and the CoCP Parts A and B, any material build up is unlikely.
- Appropriate spill kits will be put in place to deal with accidental leakages or spillages of hydrocarbons and oils, or accidental spillages of hazardous substances and will be available at all times to all areas of work activity.
 - All site workers will be trained in the use of spill kits and informed of their location.
 - Emergency controls, including cessation of pumping, deployment of spill kits and informing the EA, for all works adjacent to or over the river and canal network will be in place.
 - All refuelling and any maintenance oiling will take place in designated and bunded area and on an impermeable surface.
 - Refuelling will be mandatory attended by a site representative.
 - Plant nappies will be placed below static mechanical and mobile plant.
 - Vehicles will be washed down only in designated areas.
 - Site-specific responses will be prepared for potential pollution incidents (e.g., spillages) or extreme weather events (e.g. storms) which may cause an increase in sediment runoff.
 - Adherence to Scheme-wide drainage strategy included as Appendix A in Appendix 11-1 Flood Risk Assessment in Volume 3 of the ES⁹).
 - Use of appropriate measures to ensure contaminants (including fine sediments) from construction activities do not enter the watercourse.
 - If required, silt fences at the toe of slopes, made from geotextiles, to reduce silt transport will be employed. Exposed ground and soil stockpiles will be minimised to prevent runoff to watercourses and drains.
 - Any measures in place to limit adverse dust and air pollution effects will also apply to reducing the likelihood of polluted surface water run off being generated.
 - Potential flooding effects, as per the approved Flood Risk Assessment, will be considered when planning remediation activity sites and storage of materials.
 - Identification of site-specific drainage requirements.
 - A nominated specialist emergency spill response contractor in the event of an emergency/major pollution incident.

4.4 Emergency response

- 4.4.1 In accordance with the Network Rail Memorandum of Understanding and in line with Network Rail's internal environmental compliance and report procedures, the TRU West Alliance will report all major pollution incidents to the EA hotline.
- 4.4.2 The following list is included as an example of the criteria and is not meant to be exhaustive. It may also be influenced by factors such as the environmental sensitivity of the site. Discussions with the EA will set out the parameters of when they should be informed of environmental incidents where they deviate from the below or where further clarification of the criteria is required:
- Spillages of HAZCHEM listed chemicals (unless otherwise stated) (See Note 1);
 - Spillages of Low Hazard products with polluting potential (see Note 2);
 - Petrol spillages greater than 100 litres;

⁹ [W3 Environmental Statement Volume 3: Appendix 11-1 Flood Risk Assessment](#)

- Hydrocarbon spillages greater than 20 litres (Inc. hydraulic oils and cutting oils);
- Any spillages in or near watercourses;
- Incidents at EA-regulated Network Rail sites in accordance with permit conditions;
- Incidents involving hazardous fly tipped waste;
- Loss of cable or transformer oil in accordance with the incident notification thresholds contained in Annex 2 of the Memorandum of Understanding;
- Incidents involving flooding from main river watercourses or where actions or incidents have occurred that could increase the risk of future flooding (see Note 3); and
- Significant releases of silt/sands/cement slurry (see Note 4).

Note 1: Incidents involving UN classified Dangerous Goods should be notified to the Environment Agency unless the incident involves only small quantities of mineral oils (under 20 litres). Incidents involving most gases are unlikely to be of interest to the Environment Agency unless large quantities of water or foam are used. Ammonia and Chlorine are notable exceptions.

Note 2: Substances such as milk and beer are highly polluting if discharged into the aquatic environment. Many other seemingly low hazard substances can also pose a risk. A list of 'Low Hazard Products with Polluting Potential' is included in Table 4-3.

Note 3: Examples could include blockage to a main river or damage to a flood defence or control structure and any works in the floodplain that may have an impact on the effectiveness of the floodplain

Note 4: Discharges of silty/sandy water can be highly polluting and should be avoided, as should discharges of water contaminated with cement. Typical sources of such pollution can include dewatering operations, dredging, and surface run-off during construction activities.

Table 4-3 Low hazard products with polluting potential

Substance	Threshold	Example
Detergents	25 litres	Washing powder, washing up liquid, shampoos, soaps and car cleaning products
Disinfectants	25 litres	Household bleach, Dettol
Food Stuffs	250 litres	Most have the potential to cause problems, but of particular concern are sauces, sugars, salt, syrups, milk, cream, yoghurt and vinegar.
Beverages	250 litres	Soft drinks, beers, lagers, wines and spirits.
Fertilisers	25 kg	All
Paint and Dye	25 kg	All
Other Organic Liquids/ Slurries	Varies	Blood, offal, farmyard slurries, fire-fighting foams, sewage sludge, antifreeze, cutting, lube and cooking oils, glycerine, alcohols, latex, water soluble polymers.

4.4.3 The effects of these products vary widely; some, such as detergents, are toxic to aquatic life. Silt and sand can smother and choke aquatic life whilst others, such as foodstuffs, beverages and blood, can deoxygenate the water. Fertilisers and detergent are both toxic and lead to deoxygenation. Every effort will be made to contain these products.

4.4.4 The threshold quantities outlined in Table 4-3 are a guide. If there is a spillage of any of the substances, the EA will be contacted and where required additional information may be

needed about the possible effects.

- 4.4.5 Other, seemingly harmless, products such as tyres, rubbish and straw can also cause problems if involved in fires, as fire-fighting run-off from incidents involving them can be highly polluting.
- 4.4.6 There might be incidents that do not fall into any of the categories above; if any doubt exists, the EA will be contacted.

4.5 Specific guidance for Stage 4 works

Earthworks E0

- 4.5.1 Earthworks E0 located to the south of the railway could result in increased risk of pollution to the Huddersfield Broad Canal and Hebble Beck due to runoff containing elevated levels of suspended solids and resuspension of potentially contaminated sediments.
- 4.5.2 To minimise the risk of pollution, methods of work will be used that do not contaminate surface water.
- 4.5.3 Areas of vegetation clearance and topsoil strip should be limited as much as practicable. Earthworks to be phased to minimise the areas of exposed ground and reduce the potential risk for runoff. Other mitigation measures include, as set out in section 4.3, consideration of the timing of excavation, in relation to the weather forecast; consideration of the proximity to the canal and Hebble Beck, and apply the mitigation as appropriate; use of sediment mitigation measures including use of small bunds and silt traps to isolate any relevant areas in close proximity to watercourses; use of straw bale traps where appropriate.
- 4.5.4 Where earthworks and vegetation clearance require the use of machinery, then fuel-related mitigation measures will be put in place. This includes, as set out in section 4.3: any refuelling and maintenance oiling taking place in designated and bunded areas and on an impermeable surface; refuelling being mandatorily attended by a site representative; plant nappies placed under any static mechanical and mobile plant.

Bulk materials storage

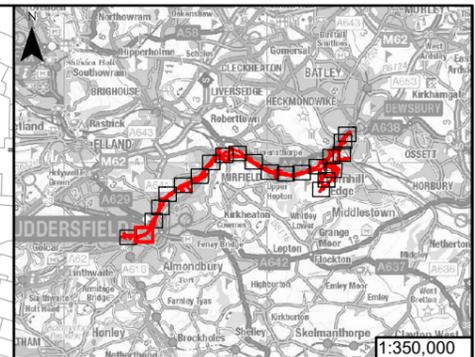
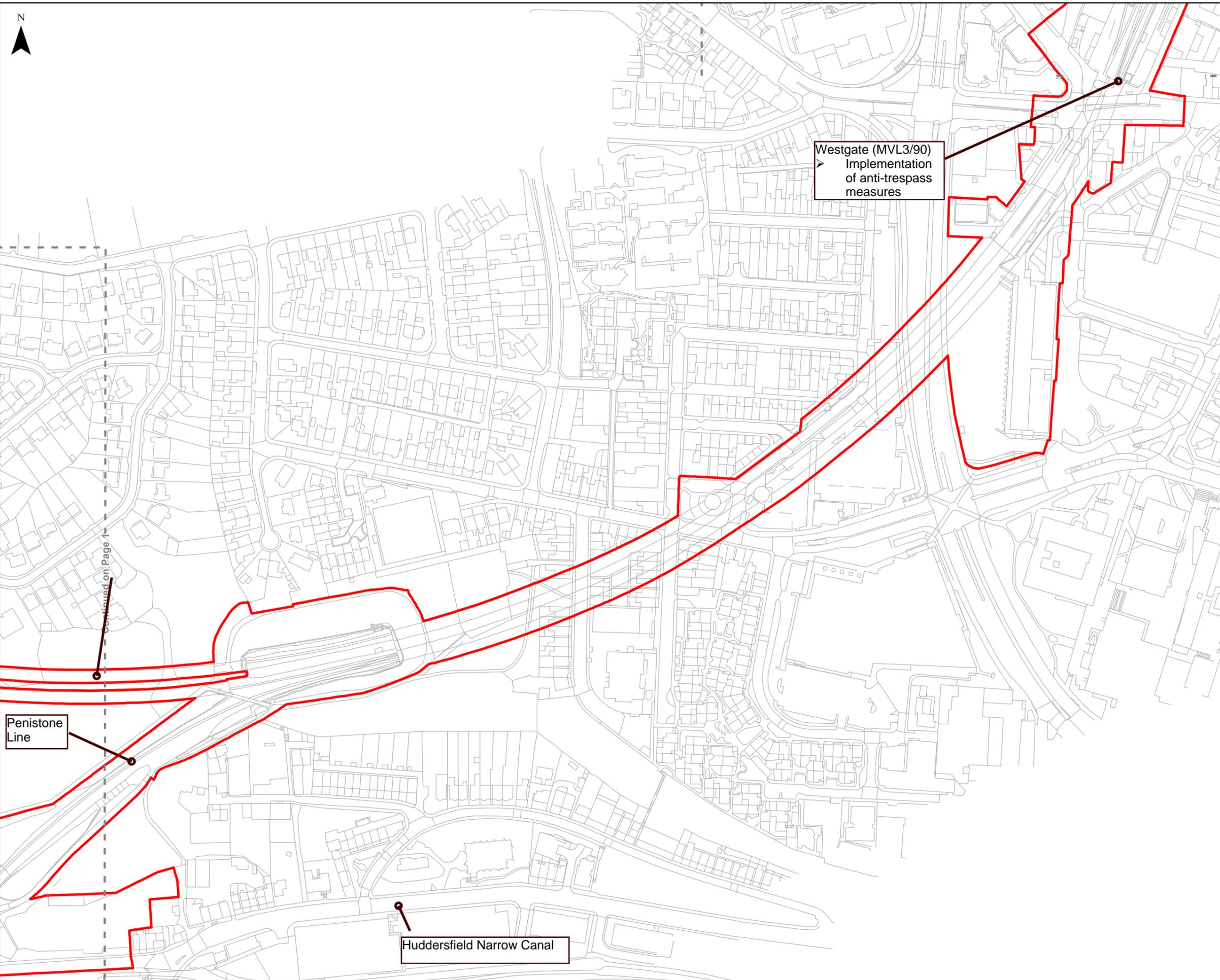
- 4.5.5 Bulk materials such as topsoil and excavation arisings must be stored in designated areas away from the nearby surface watercourses.
- 4.5.6 Where materials are required to be stored in proximity to the Huddersfield Broad Canal or Hebble Beck, containment systems are to be put in place. Stockpiles will be bunded or sheeted (using geotextiles, jute or coconut matting) to minimise the potential for runoff and any dust or air pollution.
- 4.5.7 Proprietary silt fences / matting should be installed to control pollution from surface water runoff and reduce the risk of pollution to watercourses. General mitigation measures to prevent the runoff of sediment from the stockpiles will be implemented, as set out in section 4.3: consideration of the timing of any movements from the stockpile, in relation to the weather forecast; consideration of the proximity to the canal and Hebble Beck, and apply the mitigation as appropriate; use of sediment mitigation measures including use of small bunds and silt traps to isolate any relevant areas in close proximity to watercourses; use of straw bale traps where appropriate.
- 4.5.8

Runoff from de-vegetated ground

- 4.5.9 Runoff from completed earthworks which have yet to have become established with grasses and other ground cover are vulnerable to erosion and runoff. Consideration will be given to the use of methods to protect and stabilise unvegetated areas to reduce erosion, such as using woodchip/mulch, benching and roughening of slopes and geotextiles.
- 4.5.10 Graded slopes increase the velocity of surface water runoff, the risk of erosion and subsequent silty water runoff. Consideration should be given to more natural rough, uneven slopes as this encourages infiltration and decreases the velocity of surface water runoff. It also allows seeds and vegetation to establish more readily. Roughening can be achieved with the use of tracked machinery leaving track imprints parallel to the slope.
- 4.5.11 If necessary, general mitigation measures to prevent the runoff of sediment from the de-vegetated ground will be implemented temporarily, as set out in section 4.3: consideration of the proximity to the canal and Hebble Beck, and apply the mitigation as appropriate; use of sediment mitigation measures including use of small bunds and silt traps to isolate any relevant areas in close proximity to watercourses; use of straw bale traps where appropriate.

Appendices

APPENDIX A – WORKS LOCATION PLAN



Scheme Boundary
 Adjacent Map Sheet



P01	12/02/21	FIRST ISSUE	RB	PB	PB
Rev	Date	Description of Revisions	Drwn	Chkd	Appr
Status	SHARED				Suitability



Project
TRANSPENNINE ROUTE UPGRADE
 Contract No.
151667
 Scheme Title
THE NETWORK RAIL (HUDDERSFIELD TO WESTTOWN (DEWSBURY) IMPROVEMENTS) ORDER

Drawing Title

Figure 1

Stage 4 works

Designed	R.Bowes	Signed Electronically	Date	12/02/2021
Drawn	R.Bowes	Signed Electronically	Date	12/02/2021
Checked	P.Butler	Signed Electronically	Date	12/02/2021
Approved	P.Butler	Signed Electronically	Date	12/02/2021

Scale(s)	1:2,500	ELR & Project Chainage	---
Alternative Reference	---	Sheet	2 of 22
Drawing Number	151667-TSA-00-TRU-REP-W-EN-001370	Revision	P01.1

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Railway works include

- 4 tracking
- Horizontal track realignment
- Relaying of ballast and tracks

Continued on Page 4

Halfords - access to business to be maintained

Huddersfield Viaduct (MVL3/92) Bradford Road

- Existing bridge deck to be removed and replaced
- Additional masonry pilasters/butresses will be formed at the corners of the new abutments into which the concrete parapet will join.
- New concrete abutments to be built with stone facing in front of existing sandstone at both ends and, on both elevations.

Huddersfield Viaduct – Span 4 – Fitzwilliam Street Underbridge

- Substructure repairs including removal and reinstatement of the pier to the south-western corner of the structure
- Strengthening to cross girders
- Removal of parapet and replacement with replica in cast iron

Huddersfield Viaduct - general strengthening works along the length of viaduct together with localised repairs to arches where necessary. These works include pinning and grouting, shear anchors and spandrel strengthening with tie bar and pattress plates.

DNO - Fitzwilliam Street

Huddersfield Viaduct Span 1 (MVL3/929 (1)) - John William Street

- existing bridge deck will be removed and replaced with a new single span bridge deck

Fitzwilliam Street Construction Compound

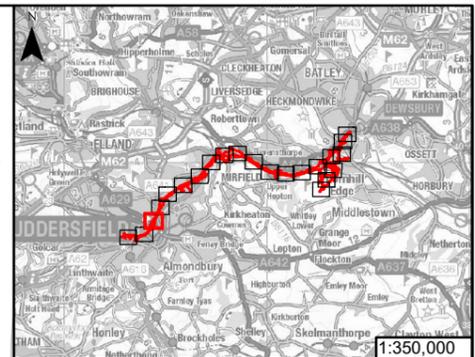
Huddersfield Station - construction compound

Brian Jackson House

Huddersfield Station

- main train shed - structural works, two bays at Manchester end of canopy to be demolished, two new bays at Leeds end of platform 1; reinstatement of main train shed lantern;
- smaller train shed (canopies B&C) to be demolished, new roof to be constructed;
- free standing canopies over island platforms
- extension to canopy to service Penistone line;
- tea rooms to be deconstructed and relocated;
- a covered footbridge (with lift and stairs) to be constructed (step free access)
- concrete infill of existing parcel subway
- extension to existing passenger subway to service new island platform to north of station
- Grit blasting of main train shed
- Works to railway lines including provision of new platforms and removal of existing railway sidings
- Demolition of existing relay room

Continued on Page 2



Scheme Boundary
Adjacent Map Sheet



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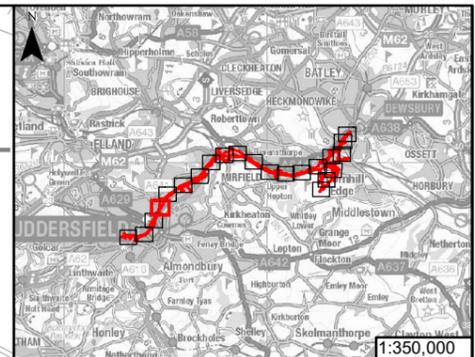
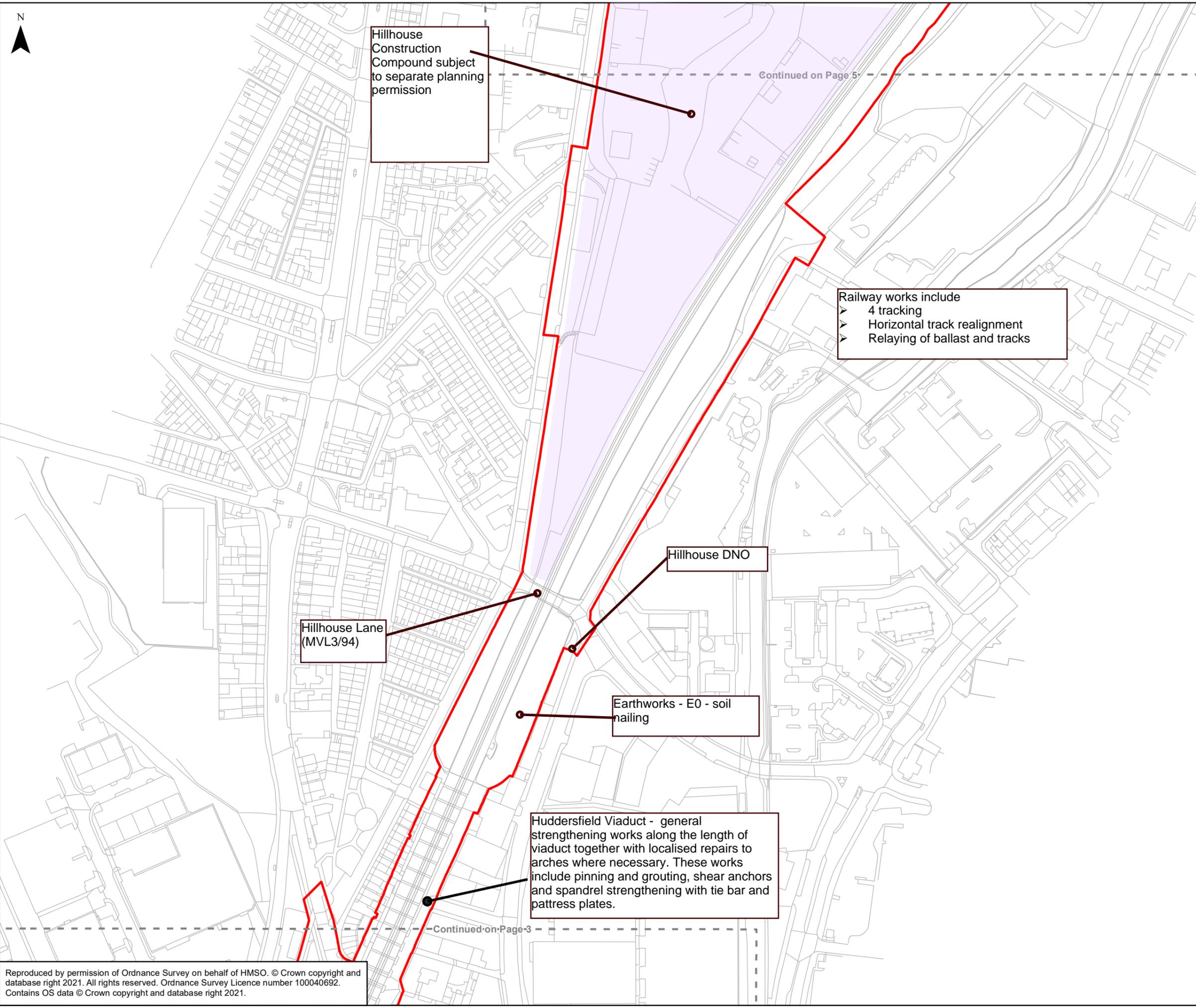
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Alternative Reference	---	Sheet	3 of 22
Drawing Number	151667-TSA-00-REP-W-EN-001370	Revision	P01.1



- Scheme Boundary
- Adjacent Map Sheet



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SHARED					



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Scale(s)
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ELR & Project Chainage

Alternative Reference

Sheet
4 of 22

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Revision
P01.1

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