



Prickleden Mill, Holmfirth,

Transport Statement

May 2023

Job no.544

Paragon Highway Consultants
Office 20/21 The Rear Walled Garden
Nostell Estate
Wakefield WF4 1AB

☎ 01924 291536

✉ mail@paragonhighways.com
paragonhighways.com

Quality Management

	First Issue	Revision 1	Revision 2	Revision 3
Remarks	Architects Amendments	Additional Council Requirements	Access by third parties	Amendment to parking
Date	20.11.2020	December 2021	January 2022	May 2023
Prepared by	PAH	PAH	PAH	LJO
Checked by	CHS	CHMS	CHMS	AH

This document is issued for the party which commissioned it and for specific purposes connected with the above-captioned project only. It should not be relied upon by any other party or used for any other purpose.

We accept no responsibility for the consequences of this document being relied upon by any other party, or being used for any other purpose, or containing any error or omission which is due to an error or omission in data supplied to us by other parties.

This document should not be shown to other parties without consent from us and from the party which commissioned it.

APPENDICES

- Appendix A Site Location
- Appendix B Pedestrian and Cycle Catchments
- Appendix C TRICs data
- Appendix D Proposed Site Layout
- Appendix E Accident Data

1 INTRODUCTION

- 1.1.1 Paragon Highway Consultants has been appointed to prepare this Transport Statement relating to the proposed residential development at a site at the former Prickleden Mill, Holmfirth in the district of Kirklees. Appendix A shows the site location in relation to the regional and local highway network.
- 1.1.2 The proposals are to erect a development of 61 no. dwellings (apartments) served off the Lower Mill Lane, Holmfirth in the district of Kirklees.
- 1.1.3 The proposed site has planning approval for 46no. age restricted apartments with associated residents and visitor car parking, new bridge access plus related engineering and landscape works – Planning approval 2012 – 90738 refers.
- 1.1.4 Drainage, lighting, parking provision and highway alignment will generally conform to Kirklees Councils Highways Design Guide. Turning areas within the site are proposed to allow all vehicles likely to enter the site, including a private refuse vehicle, to exit in a forward gear.
- 1.1.5 This Transport Statement considers such matters as access, sustainability, car parking, traffic impact, accident data and servicing and presents the proposals in relation to current guidance and data. The traffic impact associated with the development proposals is also presented.

2 **EXISTING CONDITIONS**

2.1 **Site Description**

2.1.1 The site previously accommodated the Prickleden Mill which was demolished several years ago. Access to the mill was gained via a narrow (3.8-metre-wide) and steeply sloping access from the A6024 Woodhead Road. Sight lines at this junction onto the class 1 highway were well below standards. The previous use of the mill would have generated staff, customers and heavy goods vehicles throughout the day utilising this access point. Access will still be retained along this route for the plumbing company who have rights of access via this route and through the site.

2.1.2 The site is approximately triangular in shape bounded by properties fronting Woodhead Road to the north, the River Holme to the south, the former mill dam to the west and the access to the site to the east.

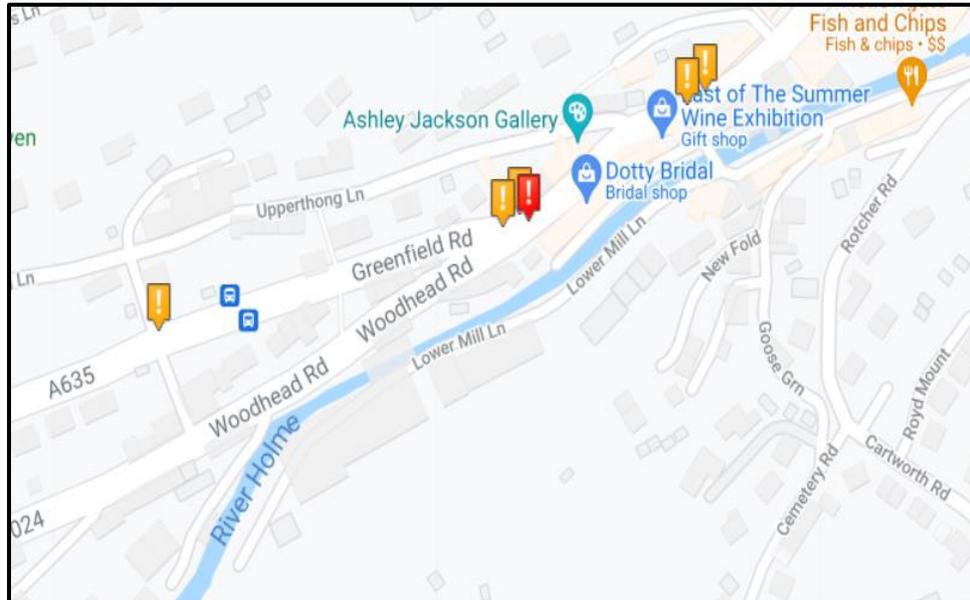
2.1.3 The site lies just over 400 metres west of Holmfirth town centre and some 8.5km south west of the large town of Huddersfield.

3 LOCAL HIGHWAY NETWORK

- 3.1.1 The A6024 Woodhead Road is a class 1 highway and connects locally Holmfirth with the large town of Huddersfield to the north east and to the south west towards Manchester. Close to the point of access to the site it is the subject of a 30mph speed limit.
- 3.1.2 In the vicinity of the site access, Woodhead Road is a two-way single carriageway with footways to both sides and street lighting to main road standards. On street parking is restricted on both sides when travelling towards the town centre but away from the centre parking is permitted in certain locations particularly in front of residential properties.
- 3.1.3 The site will gain access from the Lower Mill Lane which lies to the south side of the River Holme. It is a residential cul-de-sac with a varying carriageway width but typically around 4.8 metres or so. There is a raised footway to its northern side for almost its full length which leads to a flush footway to the south side of the carriageway at its eastern end. Approximately half way along the street there is a priority give way system at a point narrowing of the carriageway.
- 3.1.4 There is a turning head at the head of the cul-de-sac which is bounded on its western side by 4no.parking spaces for visitors to the properties along Lower Mill Lane.
- 3.1.5 At the eastern end of Lower Mill Lane it forms a simple priority junction on the outside of a sharp bend in Hollowgate. Hollowgate is a two-way single carriageway to the east of the junction but becomes one way over the narrow bridge (westerly) up to the junction with the A6024.
- 3.1.6 On street parking is controlled along the full length of Lower Mill Lane to one side or the other. However, apart from some indiscriminate parking that occurs close to the Hollowgate junction, the parking restrictions appear to operate in an effective manner at the times of our site visits.

3.2 Road Traffic Accidents

3.2.1 A review of personal injury accidents for the last five-year period up to December 2020 has taken place using www.crashmap.co.uk. The search area is shown on the area highlighted on the drawing taken from the Crahsmap website below:



3.2.2 As can be noted from the above there have been no recorded injury accidents along the full length of Lower Mill Lane or at its junction with Hollowgate. However there has been 2no.recorded injury accidents at the junction of Hollwogate with the A6024 which are dealt with in the paragraphs immediately below.

3.2.3 The first incident occurred in September 2016 during daylight hours with a dry carriageway surface and during the morning. The accident involved a private car and a light goods vehicle and involved the car emerging from the Hollowgate junction into the path of the lgv resulting in a slight injury to a passenger within the car.

3.2.4 The second incident occurred in February 2018 on an evening and with a wet road surface. The accident involved 2 vehicles. The incident occurred when one of the vehicles performed a U turn and a vehicle collied with the rear of this vehicle, resulting in slight injuries to 3 of the occupants of the vehicle performing the U turn. The accident data can be found in Appendix E.

3.2.5 Based on the above the adjacent highway network and local junctions do not identify any road safety problems or trends of any significance and appear to operate in a safe and practicable manner.

3.2.6 It is anticipated that the road safety record would not warrant any further treatment given the slight change in the network peak traffic flows as a result of the development proposals.

3.3 Transport Sustainability

3.3.1 The current revised National Planning Policy Framework was published in July 2021 and sets out the government’s planning policies for England and how these are expected to be applied. It recommends that new development should only be prevented or refused on highways grounds if there would be an unacceptable impact on highway safety, or the residual cumulative impacts on the road network would be severe. Within this context, applications for development with regards to Transport should:

Considerations	Proposals
Give Priority first to pedestrians and cycle movements both within the scheme and the surrounding neighborhood	Cycle parking facilities are proposed, and a return footway is proposed into the development to aid pedestrian access to the site
Address the needs of people with disabilities and reduced mobility in relation to all modes of transport	On site disabled parking spaces are proposed. Access along the existing network to bus facilities is considered acceptable for people with reduced mobility
Create places that are safe, secure and attractive and minimise the scope for conflict between all users.	The architects have acknowledged these issues within the overall design
Allow for the efficient delivery of goods, and access by service and emergency vehicles	The site access and internal circulation area will allow for safe access within the site and suitable access and egress onto the major road
Allow within the design for the charging of plug-in and ultra-low emission vehicles in safe and convenient locations	Charging points for plug-in vehicles will be provided as part of the overall parking scheme

Table 1: Transport Considerations

3.3.2 Paragraph 113 of the NPPF states, “All developments that will generate significant amounts of movement should be required to provide a Travel Plan, and the application should be supported by Transport Statement or Transport Assessment so that the likely impacts of the proposal can be assessed.” A separate Travel Plan was prepared in this case.

3.3.3 The national transport policy relating to promoting sustainable development that was formerly set out in PPG 13 'Transport' is now replaced by Section 9 of the NPPF. However, the former guidance within PPG 13 is still useful as a reference.

Pedestrians

3.3.4 PPG13 stated that *'walking is the most important mode of travel at the local level and offers the greatest potential to replace short car trips, particularly under 2km'*. As can be seen from the plan in Appendix B, the catchment includes the whole of Holmfirth, its bus station and its local retail services, health services and amenities. The pedestrian catchment also includes parts of Upperthong and Holmbridge and the majority of the commercial areas within Holmfirth.

3.3.5 An additional pedestrian link will be provided to the Woodhead Road utilising the current vehicular access to the site.

3.3.6 As such it can be seen that the site is well placed in relation to the public footway network. This provides residents with the opportunity to access the various local amenities in the surrounding area on foot and similarly the opportunity for residents' visitors to make their journey to the site on foot or via multi modal travel.

Cycling

3.3.7 With regards to cycling, PPG 13: Transport stated that *"Cycling also has the potential to substitute for short car trips, particularly those under 5km, and to form part of a longer journey by public transport"*. The plan at Appendix B also shows the 5km cycling catchment area from the development site, which includes the settlements of Brockholes, Honley, Thurstonland, New Mill, Scholes, Jackson Bridge, Hepworth, Hade Edge and Holme, as well as their numerous amenities and services. This demonstrates that there are significant levels of commercial and service centre areas within the catchment area, providing existing potential for residents and visitors to cycle to and from the development site.

3.3.8 It is acknowledged that the local topography does limit cycling opportunities for the occasional cyclist, however, to the experienced cyclist the terrain will not prevent the day-to-day commute which is usually more weather dependent.

Bus Travel

3.3.9 Opportunities to access bus services exist within Holmfirth at the bus station. The bus station has the benefit of passenger shelters. The main bus services available from the bus station are shown in Table 2 below:

Service No	From – To	Frequency Mon – Sat	Late evenings and Sundays
H1 & H2	Holmfirth – Cinderhills – Scholes – Wooldale – Totties loop	60 mins	-
308	Huddersfield – Primrose Hill - Newsome – Honley – Holmfirth	60 mins	60 mins
310	Huddersfield – Holmfirth – Scholes – Hepworth	30 mins	60 mins
314	Huddersfield – Brockholes – Holmfirth – Holme	60 mins	60 mins
316	Huddersfield – Brockholes – Holmfirth – Parkhead	60 mins	-
335	Holmfirth – Netherthong – Meltham – Slaitwaite	60 mins	-
X1	Holmfirth – Wakefield	60 mins	120 mins

Table 2 - Bus Services

3.3.10 As can be identified from Table 2 above there are frequent bus services that provide access to many local settlements and to the major town of Huddersfield and the city of Wakefield. Therefore, it is considered that these bus services will provide a very good alternative to the private car in line with current Government guidelines.

Train Travel

3.3.11 Brockholes Rial station is located to the northeast of the application site and is just within the recognised cycling distance for commuting. The station is on the Penistone Line and provides a regular hourly service to Sheffield, Barnsley, Penistone and Huddersfield plus many stations in between the major towns / cities.

- 3.3.12 As can be identified from the train routes and services summarised above there are good train links available with frequent train services.
- 3.3.13 From the above it is evident that the site benefits from being in proximity to a good frequency of public transport links for travelling around the area with opportunities for multi modal travel.

4 THE DEVELOPMENT PROPOSALS

4.1 Site History

4.1.1 The site received planning approval for 46no. age restricted apartments - planning approval ref: 2012/90738. The scheme also included for a new bridge access, residents and visitor parking space and associated engineering and landscaping.

4.2 Proposed Development

4.2.1 It is intended to construct a development of 61no. apartments consisting of 59no. 2 bed roomed dwellings and 2no. 3 bed roomed units served off the Lower Mill Lane. The development will be age restricted to the over 55's.

4.2.2 The road layout and internal network will be laid out to generally comply with the current guidance contained within the Kirklees Council Street Design Guide in terms of construction, however, the new access and internal arrangements will not be offered for adoption as this will be a gated community. A separate access to Woodhead Road will be provided, following the line of the existing vehicular access to the mills, for pedestrians and cyclists as well as the plumbing company who have rights of access along same. This route could be utilised as an emergency access if necessary. There is also an emergency pedestrian escape route proposed off Woodhead Road located at the far northwest side of the site.

4.2.3 Drawings illustrating the potential internal proposals can be found in Appendix D.

4.2.4 2no. footbridges over the Rover Holme will be formed as part of the development and also a section of footpath, running between the mill dam and the adjacent watercourse, to compliment the riverside walk ambitions of the Kirklees Council.

4.3 Vehicular Access

4.3.1 The existing access off the Woodhead Road will be permanently closed to vehicular traffic (apart from emergency vehicles, the plumbing company and parking to the rear of Numbers 15 to 23 Woodhead Road) and be retained for general use by pedestrians and cyclists. A new vehicular access will be formed via a new bridge over the River Holme onto Lower Mill Lane. The four existing public visitor parking bays will be relocated to allow access to the bridge and the application site. This part of the access, fronting the four visitor spaces, will be offered for adoption by the LHA.

4.3.2 The new bridge will be wide enough to accommodate all anticipated vehicle movements including the private refuse collection service and a fire tender. The new bridge will also cater for pedestrians and cyclists wishing to access the proposed development via Lower Mill Lane.

4.3.3 Vehicle swept path analysis has been undertaken as requested by the Local Highway Authority and this information can be found on the drawings at Appendix D.

4.4 Parking Provision

4.4.1 The Councils car parking requirements for residential developments including apartments is shown below which is an extract from the Council current residential design guide.

20 **KEY DESIGN DRIVER**

Kirklees Council has not set local parking standards for residential and non-residential development. However, as an initial point of reference for residential developments (unless otherwise evidenced using the criteria in Para. 5.1), it is considered that new:

- 2 to 3 bedroom dwellings provide a minimum of two off-street car parking spaces
- 4+ bedroom dwellings provide three off-street spaces.
- 1-2 bedroom apartments provide one space (3+ bed two spaces)

In most circumstances, one visitor space per 4 dwellings is considered appropriate. One cycle space per unit is recommended. The council can advise on provision for other uses. Further guidance can be found [here](#):

4.4.2 The parking provision for the 61no. apartments will be 78no. spaces in total with 31no. spaces on the surface level and 47no. spaces within the basement car park. This is generally in accordance with the Councils current parking guidelines.

4.4.3 The Councils Highway Officer has stated that 78no. spaces would fully meet the recommended standards. The development meets this requirement.

4.4.4 EVC points will be provided as part of the development to ensure that some of the car journeys that are necessary are made by less polluting vehicles.

4.4.5 As mentioned above the 4no. displaced visitor parking bays at the head of Lower Mill Lane will be replaced close to the point of access to the site. The spaces will be offered for adoption to the Local Highway Authority together with the relevant part of the access road fronting same.

4.5 Traffic Impact

4.5.1 The TRICs database (currently version 7.8.4) has been used to derive the peak hour generation rates for the proposed development – 61 no. age restricted apartments – See Table 3 below. A copy of the output can be found in Appendix C.

61 no. apartments Calc Factor: Per dwelling	Morning Peak			Evening Peak		
	ARRIVE	DEPART	TOTAL	ARRIVE	DEPART	TOTAL
Trip Rates	0.063	0.059	0.122	0.059	0.048	0.107
Traffic Generations	3.843	3.593	7.442	3.599	2.928	6.527

Table 3: Peak Hour Trip Rates and Traffic Generations

4.5.2 Based upon the information contained within the TRICS database the development would be a very low traffic generator at the network peaks and generally overall throughout the day with only between 7 trips at the busiest times. This number of additional trips would have little or no impact on surrounding junctions including the site access points with all priority junctions operating within normal requirements. The traffic impact would not be significant or severe as per the test within the latest NPPF.

- 4.5.3 Given the sites town centre location and the age restriction on occupation of the apartments it is anticipated that the trips generated especially at the peak times will generally be in accordance with the derived rates set out above. Notwithstanding this even a 50% increase above that anticipated would still have little impact on the Lower Mill Lane / Hollowgate junction or at the Hollowgate / Huddersfield Road junction.
- 4.5.4 It is anticipated that the majority of the trips to and from the site will be made on foot or potentially by cycle.
- 4.5.5 As a worst-case scenario a trip rate of 0.4 trips per apartment could be utilised as part of the assessment. However, this trip rate level would generally only be used for apartments / flats occupied by the general public i.e. not age restricted. Utilising this higher trip rate then the development could have the potential to generate 24 trips at the peak times. Notwithstanding the above, these trip rates relate to occupation with no age restriction, however, should the actual trip rates from the development be 50% of the worst case scenario trip rate, this would still have little or no impact on the surrounding network and junctions.

5 TRANSPORT POLICY

5.5.1 When considering transport policy compliance for planning applications, the main thrust of local, regional and national policy is that new development should be conveniently accessible by a range of sustainable transport modes, including public transport, cycling and walking. This policy therefore sets out the framework for this Transport Assessment and the project's compliance with the policy objectives. Further details of the relevant policy documents are set out below.

National Planning Policy Framework – Promoting Sustainable Transport

5.5.2 The recently revised National Planning Policy Framework was published in July 2021 and sets out the government's planning policies for England and how these are expected to be applied.

5.5.3 It recommends that development should only be prevented or refused on highways grounds if there would be an unacceptable impact on highway safety, or the residual cumulative impacts on the road network would be severe.

5.5.4 This revised Framework replaces the previous National Planning Policy Framework published in 2019. In assessing sites that may be allocated for development in plans, or specific applications for development, it should be ensured that:

- Give priority first to pedestrian and cycle movements, both within the scheme and neighbouring areas. And second, so far as possible, to facilitating access to high quality public transport to encourage public transport use.
- Address the needs of people with disabilities and reduced mobility in relation to all modes of transport.
- Create places that are safe, secure and attractive which minimise the scope for conflict between all road users. This is together with reducing street clutter etc.
- Allow for the efficient delivery of goods, and access by service and emergency vehicles.

- Include within the design for the charging of plug in and ultra-low emission vehicles in safe and convenient locations.

5.5.5 Paragraph 111 of the current NPPF suggests that development should only be prevented or refused on highways grounds if there would be an unacceptable impact on highway safety, or the residual cumulative impacts on the road network would be severe.

5.5.6 Paragraph 113 of the NPPF goes on to say that “All developments that will generate significant amounts of movement should be required to provide a Travel Plan.”

5.5.7 Section 9 (para 104) of the NPPF states that “transport issues should be considered from the earliest stages of plan making and development proposals so that”: -

- *The potential impacts of development on the transport networks can be addressed;*
- *Opportunities from existing or proposed transport infrastructure and changing technology and usage, are realised;*
- *Opportunities to promote walking, cycling and public transport are identified and pursued;*
- *The environmental impacts of traffic and transport infrastructure can be identified, assessed and taken into account;*
- *Patterns of movement, streets, parking and other transport considerations are integral to the design of schemes.*

Local Transport Plan

5.5.8 The current Local Transport Plan is the third West Yorkshire Local Transport Plan (LTP3) which covers the period 2011 to 2026. The key objectives of the LTP3 include:

- *To improve access to jobs, education and other key services for everyone;*
- *To reduce delays to the movement of people and goods;*
- *To improve safety for all highway users;*
- *To limit transport emissions of air pollutants, greenhouse gases and noise;*
- *To improve the condition of the transport infrastructure.*

5.5.9 The LTP sets out the walking and cycling strategy for West Yorkshire to encourage more people to use these modes of travel to help reduce the dependency on private cars. With regards to cycling provision within development proposals, the WYCS seeks to 'ensure that new development proposals are located and designed to be cycle friendly and adopt guidelines for cycle parking standards. With regards to walking, the LTP seeks to improve the local environment to make walking more attractive by enhancing safety, security and environmental quality.

5.5.10 The LTP also sets out a bus strategy West Yorkshire and seeks to increase patronage for all categories of bus passenger and modal shift towards the bus and away from the car.

Kirklees Local Plan

5.5.11 Local transport policy is set out in the Kirklees Council's Local Plan covering the period 2013 to 2031.

5.5.12 Policies relating to Transport are contained within Section 10 of the Local Plan.

5.5.13 Policy LP20 'Sustainable Travel' states: - 'New Development will be located in accordance with the spatial development strategy to ensure the need to travel is reduced and that essential travel needs can be met by forms of sustainable transport other than the private car'.

5.5.14 Policy LP21 'Highways and Access' states: - 'Proposals shall demonstrate that they can accommodate sustainable modes of transport and be accessed effectively and safely by all users'.

5.5.15 Policies LP22, LP24 relate to parking provision and design for new developments.

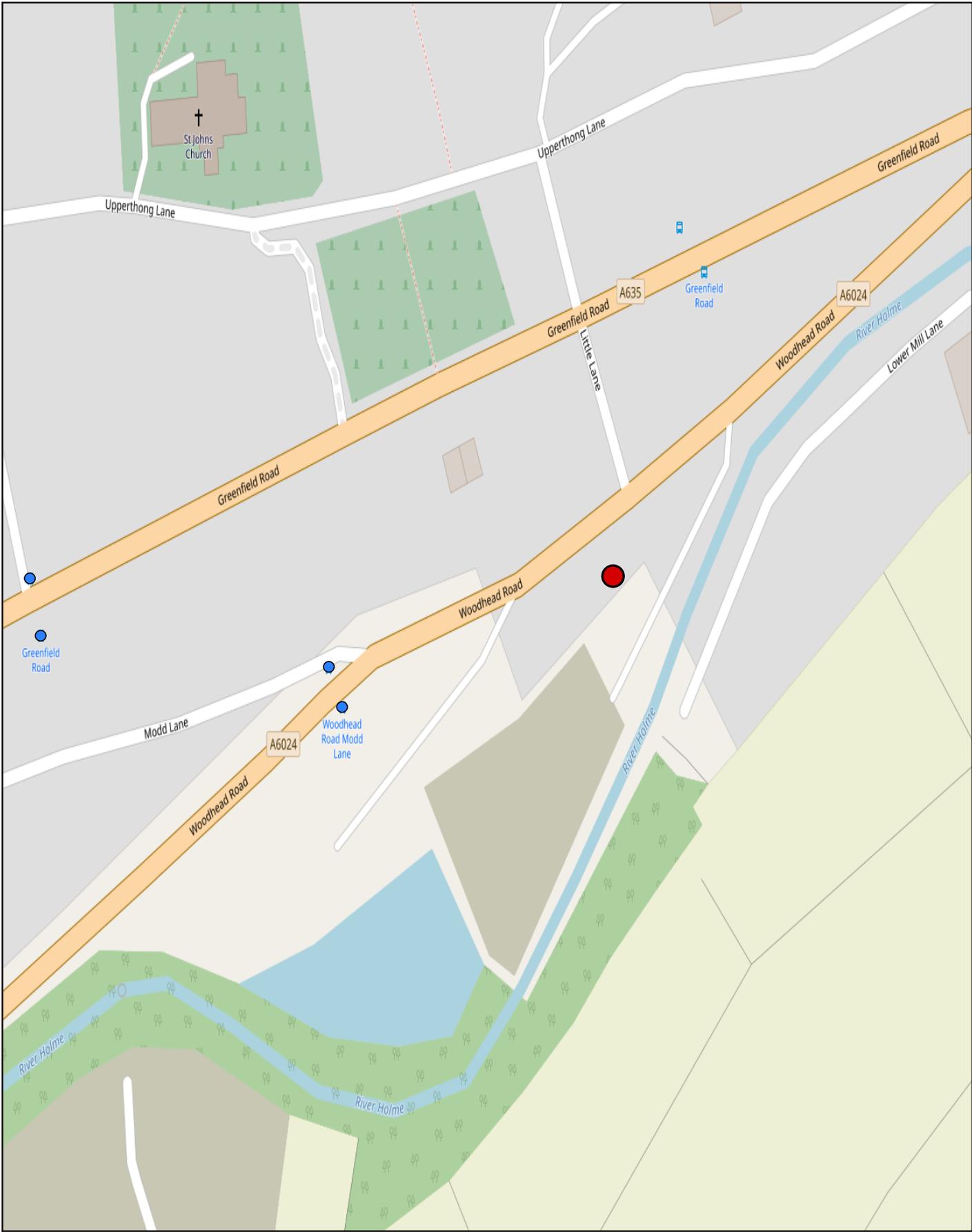
5.5.16 The location of the development in a very sustainable central position close to good bus routes, provides a suitable access arrangement, parking provision, and design. Therefore, the proposals generally meet the requirements of the Local and National Policy.

6 CONCLUSIONS

- 6.1.1 This Transport Statement considers such matters as access, sustainability, car parking, traffic impact, accident data and servicing and presents the proposals in relation to current guidance and data. The traffic impact associated with the development proposals is also presented.
- 6.1.2 This report has considered the proposed access arrangements, site sustainability, presenting the proposals in relation to current guidance and data.
- 6.1.3 The impact of the development has been assessed utilising TRICs data (currently version 7.8.3) and our analyses has found that the predicted increase in vehicle trips will not have a material, significant nor severe impact on any of the local junctions this being the test within the current NPPF.
- 6.1.4 The anticipated trip rates in respect of traffic movements are extremely low for retirement type properties and this has been confirmed by the TRICs data. The proposed development, based upon current data, would suggest that this slightly larger scheme would generate similar or lower trips to the previously approved development of 46 apartments based upon current TRICs data.
- 6.1.5 Assessment of the proposed traffic impact of the scheme shows that the level of traffic and associated trips on the local highway network will therefore have no material impact on the safe operation of the local highway and nearby junctions and will not severely add to any congestion at the peak times on the local network.
- 6.1.6 It is therefore concluded that the development is considered acceptable, and that there are no highway safety, capacity or efficiency reasons why planning consent for the proposed development should not be granted.

Appendix A

Location plan

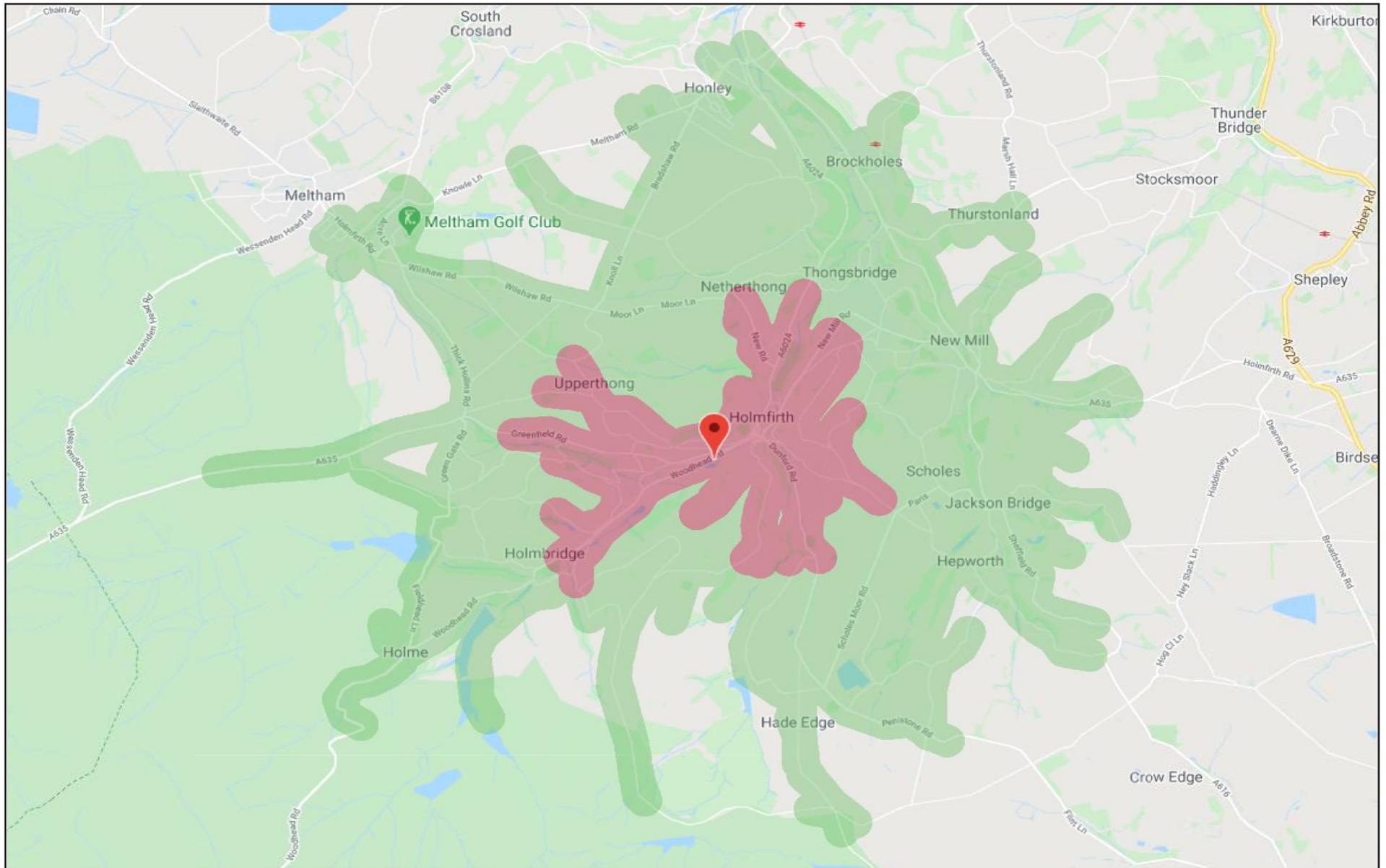


Legend:

- Site Location
- Closest Unique Bus Stops

Appendix B

Pedestrian and Cycle Catchments



Legend:

- 5km Cycle Catchment
- 2km Walking Catchment
- Site Location

Appendix C

TRICs Data

Calculation Reference: AUDIT-742101-220110-0156

TRIP RATE CALCULATION SELECTION PARAMETERS:

Land Use : 03 - RESIDENTIAL
 Category : N - RETIREMENT FLATS
 TOTAL VEHICLES

Selected regions and areas:

01	GREATER LONDON	
	WF WALTHAM FOREST	1 days
04	EAST ANGLIA	
	CA CAMBRIDGESHIRE	1 days
05	EAST MIDLANDS	
	DS DERBYSHIRE	1 days
	LN LINCOLNSHIRE	1 days
06	WEST MIDLANDS	
	WM WEST MIDLANDS	1 days
07	YORKSHIRE & NORTH LINCOLNSHIRE	
	NY NORTH YORKSHIRE	1 days
	WY WEST YORKSHIRE	1 days
08	NORTH WEST	
	CH CHESHIRE	1 days

This section displays the number of survey days per TRICS® sub-region in the selected set

Primary Filtering selection:

This data displays the chosen trip rate parameter and its selected range. Only sites that fall within the parameter range are included in the trip rate calculation.

Parameter: No of Dwellings
 Actual Range: 25 to 40 (units:)
 Range Selected by User: 25 to 62 (units:)

Parking Spaces Range: All Surveys Included

Parking Spaces per Dwelling Range: All Surveys Included

Bedrooms per Dwelling Range: All Surveys Included

Percentage of dwellings privately owned: All Surveys Included

Public Transport Provision:

Selection by: Include all surveys

Date Range: 01/01/13 to 20/11/19

This data displays the range of survey dates selected. Only surveys that were conducted within this date range are included in the trip rate calculation.

Selected survey days:

Monday	2 days
Tuesday	3 days
Wednesday	1 days
Friday	2 days

This data displays the number of selected surveys by day of the week.

Selected survey types:

Manual count	8 days
Directional ATC Count	0 days

This data displays the number of manual classified surveys and the number of unclassified ATC surveys, the total adding up to the overall number of surveys in the selected set. Manual surveys are undertaken using staff, whilst ATC surveys are undertaken using machines.

Selected Locations:

Edge of Town Centre	3
Suburban Area (PPS6 Out of Centre)	3
Edge of Town	2

This data displays the number of surveys per main location category within the selected set. The main location categories consist of Free Standing, Edge of Town, Suburban Area, Neighbourhood Centre, Edge of Town Centre, Town Centre and Not Known.

Selected Location Sub Categories:

This data displays the number of surveys per location sub-category within the selected set. The location sub-categories consist of Commercial Zone, Industrial Zone, Development Zone, Residential Zone, Retail Zone, Built-Up Zone, Village, Out of Town, High Street and No Sub Category.

Secondary Filtering selection:

Use Class:

C3 8 days

This data displays the number of surveys per Use Class classification within the selected set. The Use Classes Order 2005 has been used for this purpose, which can be found within the Library module of TRICS@.

Population within 500m Range:

All Surveys Included

Population within 1 mile:

1,001 to 5,000	1 days
10,001 to 15,000	1 days
15,001 to 20,000	1 days
25,001 to 50,000	5 days

This data displays the number of selected surveys within stated 1-mile radii of population.

Population within 5 miles:

5,001 to 25,000	1 days
75,001 to 100,000	1 days
100,001 to 125,000	1 days
125,001 to 250,000	2 days
250,001 to 500,000	2 days
500,001 or More	1 days

This data displays the number of selected surveys within stated 5-mile radii of population.

Car ownership within 5 miles:

0.5 or Less	2 days
0.6 to 1.0	1 days
1.1 to 1.5	4 days
1.6 to 2.0	1 days

This data displays the number of selected surveys within stated ranges of average cars owned per residential dwelling, within a radius of 5-miles of selected survey sites.

Travel Plan:

Yes	1 days
No	7 days

This data displays the number of surveys within the selected set that were undertaken at sites with Travel Plans in place, and the number of surveys that were undertaken at sites without Travel Plans.

PTAL Rating:

No PTAL Present	7 days
0 None	1 days

This data displays the number of selected surveys with PTAL Ratings.

LIST OF SITES relevant to selection parameters

1	CA-03-N-02 DOGSTHORPE ROAD PETERBOROUGH	RETIREMENT FLATS	CAMBRI DGESHI RE
	Suburban Area (PPS6 Out of Centre) Residential Zone Total No of Dwellings: 32 <i>Survey date: MONDAY 17/10/16</i>		
2	CH-03-N-01 HOBSON STREET MACCLESFIELD	RETIREMENT FLATS	CHESHIRE
	Edge of Town Centre Residential Zone Total No of Dwellings: 33 <i>Survey date: FRIDAY 16/09/16</i>		
3	DS-03-N-01 LEAPER STREET DERBY	RETIREMENT FLATS	DERBYSHIRE
	Edge of Town Centre Residential Zone Total No of Dwellings: 36 <i>Survey date: WEDNESDAY 25/09/19</i>		
4	LN-03-N-01 NEWPORT ROAD LINCOLN ERMINE	RETIREMENT FLATS	LINCOLNSHIRE
	Suburban Area (PPS6 Out of Centre) Residential Zone Total No of Dwellings: 39 <i>Survey date: FRIDAY 28/06/19</i>		
5	NY-03-N-01 EASTGATE PICKERING	RETIREMENT FLATS	NORTH YORKSHIRE
	Edge of Town Residential Zone Total No of Dwellings: 30 <i>Survey date: MONDAY 26/09/16</i>		
6	WF-03-N-01 SEWARDSTONE ROAD WALTHAMSTOW	RETIREMENT FLATS	WALTHAM FOREST
	Edge of Town Residential Zone Total No of Dwellings: 40 <i>Survey date: TUESDAY 05/11/19</i>		
7	WM-03-N-01 SHORT STREET STOURBRIDGE	RETIREMENT BUNGALOWS	WEST MIDLANDS
	Edge of Town Centre Residential Zone Total No of Dwellings: 25 <i>Survey date: TUESDAY 21/11/17</i>		
8	WY-03-N-01 GROVE AVENUE HALIFAX WHEATLEY	RETIREMENT BUNGALOWS	WEST YORKSHIRE
	Suburban Area (PPS6 Out of Centre) Residential Zone Total No of Dwellings: 34 <i>Survey date: TUESDAY 23/10/18</i>		

This section provides a list of all survey sites and days in the selected set. For each individual survey site, it displays a unique site reference code and site address, the selected trip rate calculation parameter and its value, the day of the week and date of each survey, and whether the survey was a manual classified count or an ATC count.

TRIP RATE for Land Use 03 - RESIDENTIAL/N - RETIREMENT FLATS

TOTAL VEHICLES

Calculation factor: 1 DWELLS

BOLD print indicates peak (busiest) period

Time Range	ARRIVALS			DEPARTURES			TOTALS		
	No. Days	Ave. DWELLS	Trip Rate	No. Days	Ave. DWELLS	Trip Rate	No. Days	Ave. DWELLS	Trip Rate
00:00 - 01:00									
01:00 - 02:00									
02:00 - 03:00									
03:00 - 04:00									
04:00 - 05:00									
05:00 - 06:00									
06:00 - 07:00									
07:00 - 08:00	8	34	0.022	8	34	0.030	8	34	0.052
08:00 - 09:00	8	34	0.063	8	34	0.059	8	34	0.122
09:00 - 10:00	8	34	0.093	8	34	0.123	8	34	0.216
10:00 - 11:00	8	34	0.078	8	34	0.104	8	34	0.182
11:00 - 12:00	8	34	0.093	8	34	0.086	8	34	0.179
12:00 - 13:00	8	34	0.093	8	34	0.078	8	34	0.171
13:00 - 14:00	8	34	0.074	8	34	0.089	8	34	0.163
14:00 - 15:00	8	34	0.100	8	34	0.089	8	34	0.189
15:00 - 16:00	8	34	0.052	8	34	0.067	8	34	0.119
16:00 - 17:00	8	34	0.074	8	34	0.041	8	34	0.115
17:00 - 18:00	8	34	0.059	8	34	0.048	8	34	0.107
18:00 - 19:00	8	34	0.048	8	34	0.045	8	34	0.093
19:00 - 20:00	1	40	0.000	1	40	0.000	1	40	0.000
20:00 - 21:00	1	40	0.000	1	40	0.000	1	40	0.000
21:00 - 22:00									
22:00 - 23:00									
23:00 - 24:00									
Total Rates:			0.849			0.859			1.708

This section displays the trip rate results based on the selected set of surveys and the selected count type (shown just above the table). It is split by three main columns, representing arrivals trips, departures trips, and total trips (arrivals plus departures). Within each of these main columns are three sub-columns. These display the number of survey days where count data is included (per time period), the average value of the selected trip rate calculation parameter (per time period), and the trip rate result (per time period). Total trip rates (the sum of the column) are also displayed at the foot of the table.

To obtain a trip rate, the average (mean) trip rate parameter value (TRP) is first calculated for all selected survey days that have count data available for the stated time period. The average (mean) number of arrivals, departures or totals (whichever applies) is also calculated (COUNT) for all selected survey days that have count data available for the stated time period. Then, the average count is divided by the average trip rate parameter value, and multiplied by the stated calculation factor (shown just above the table and abbreviated here as FACT). So, the method is: $COUNT/TRP*FACT$. Trip rates are then rounded to 3 decimal places.

The survey data, graphs and all associated supporting information, contained within the TRICS Database are published by TRICS Consortium Limited ("the Company") and the Company claims copyright and database rights in this published work. The Company authorises those who possess a current TRICS licence to access the TRICS Database and copy the data contained within the TRICS Database for the licence holders' use only. Any resulting copy must retain all copyrights and other proprietary notices, and any disclaimer contained thereon.

The Company accepts no responsibility for loss which may arise from reliance on data contained in the TRICS Database. [No warranty of any kind, express or implied, is made as to the data contained in the TRICS Database.]

Parameter summary

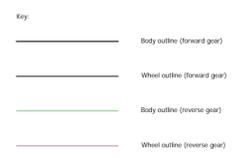
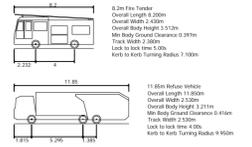
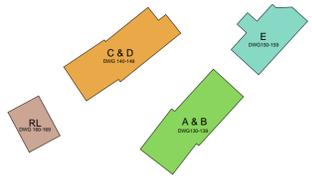
Trip rate parameter range selected: 25 - 40 (units:)
 Survey date range: 01/01/13 - 20/11/19
 Number of weekdays (Monday-Friday): 8
 Number of Saturdays: 0
 Number of Sundays: 0
 Surveys automatically removed from selection: 0
 Surveys manually removed from selection: 0

This section displays a quick summary of some of the data filtering selections made by the TRICS® user. The trip rate calculation parameter range of all selected surveys is displayed first, followed by the range of minimum and maximum survey dates selected by the user. Then, the total number of selected weekdays and weekend days in the selected set of surveys are shown. Finally, the number of survey days that have been manually removed from the selected set outside of the standard filtering procedure are displayed.

Appendix D

Proposed Layout

Only figured dimensions should be used.
 Scaled dimensions should be checked with the Architect.
 This drawing together with the design, is the property and copyright of the Architect and must not be reproduced without written permission Aerial photography copyright Google.



D	Service vehicle tracking shown.	JF	JC	12.05.2023
C	Amended block C & D road plan to show the modified staged windows on the ground floor and first floor plan.	AD	JC	10.05.2023
B	Site plan amended to show the reduced blocks roof plans and site context and levels changed to suit & 4 new parking places added.	AD	JC	21.09.2022
A	Blocks amended to show reduced pitch and updated layouts. Undercroft parking FF1, raised to 147.30. Road bridge deck lowered. New raised bund around lake formed at 150.25. Emergency access road off Woodhead road added. Pedestrian escape route shown to north. Residents lounge raised to 150.53. Mill pond outlet dropped by 400mm to 149.00. Solar PV shown to south roofs of all blocks. aerial context added.	JF	JC	25.11.2021

DO NOT SCALE OFF THIS DRAWING

rev	description	drawn	auth	date
-----	-------------	-------	------	------

ACUMEN
 DESIGNERS & ARCHITECTS

acumenarchitects.co.uk 01484 546 000
 Headrow House, Old Leeds Road, Huddersfield, HD1 1SG

Client
ELISTON HOMES

Project
PRICKLEDEN MILLS HOLMFIRTH

Project No Drawing No Rev
2659 (100)10 D

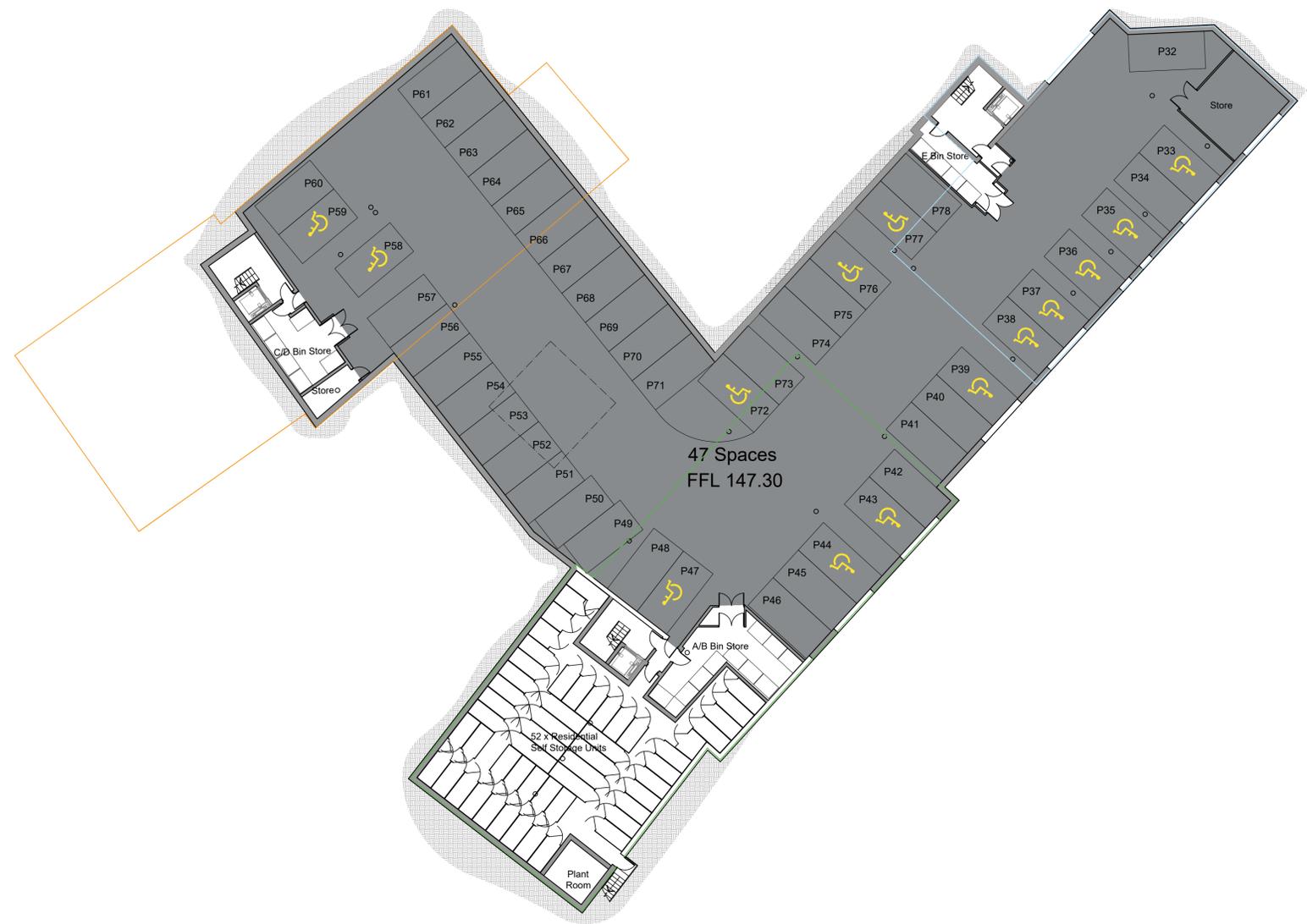
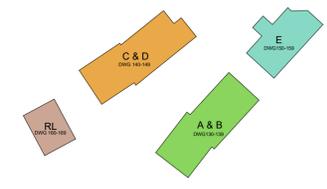
PROPOSED SITE PLAN

Scale	Date Drawn	Drawn By	Authorised By
1:500 @ A1	APR'21	JF	JC

Purpose of Issue:
 Planning Building Regs Tender Construction Comment Info

PROPOSED SITE PLAN

Only figured dimensions should be used.
 Scaled dimensions should be checked with the Architect.
 This drawing together with the design, is the property and copyright of the Architect and must not be reproduced without written permission



UNDERCROFT CAR PARK

D	Structural columns omitted following frame rationalisation.	JF	JC	15.05.2023
C	Amended the main parking layout to match the above blocks. Added a plant room within the storage unit block - removing some of the storage units. Shown indicative hydroelectric pipe.	AO	JC	21.09.2022
B	Block footprints overlaid for clarity.	JF	JC	30.06.2021
A	Bin stores enlarged and double bobbed following LPA comment. Self storage reduced.	JF	JC	21.06.2021

DO NOT SCALE OFF THIS DRAWING

rev	description	drawn	auth	date

ACUMEN
 DESIGNERS & ARCHITECTS

acumenarchitects.co.uk 01484 546 000
 Headrow House, Old Leeds Road, Huddersfield, HD1 1SG

Client ELISTON HOMES		
Project PRICKLEDEN MILLS HOLMFIRTH		
Project No 2659	Drawing No 20	Rev D

Description
**PROPOSED UNDERCROFT
 GENERAL ARRANGEMENT
 PLAN**

Scale 1:200@ A1	Date Drawn APR'21	Drawn By JF	Authorised By JC
---------------------------	-----------------------------	-----------------------	----------------------------

Purpose of Issue:
 Planning Building Regs Tender Construction Comment Info

Appendix E

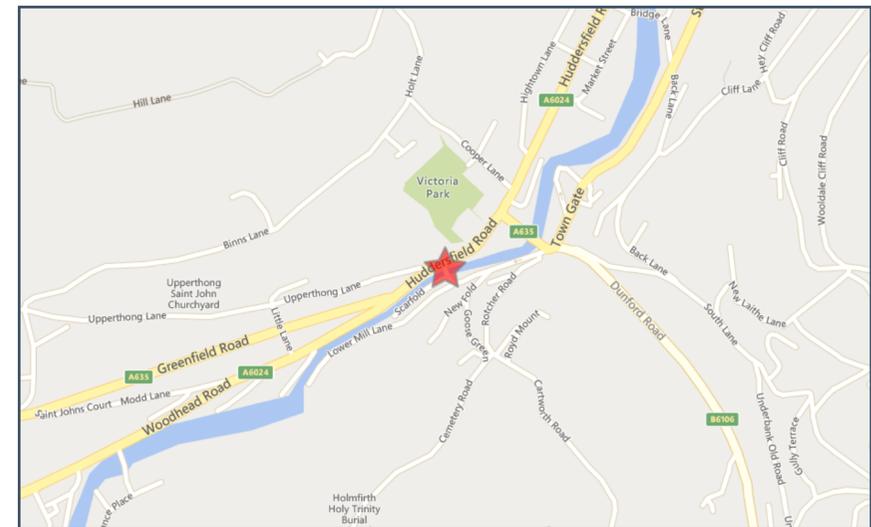
Accident Data



Validated Data

Crash Date: Monday, September 19, 2016 **Time of Crash:** 11:10:00 AM **Crash Reference:** 20161339J0530

Highest Injury Severity:	Slight	Road Number:	A6024	Number of Casualties:	1
Highway Authority:	Kirklees	Number of Vehicles:	2	OS Grid Reference:	414073 408120
Local Authority:	Kirklees				
Weather Description:	Fine without high winds				
Road Surface Description:	Dry				
Speed Limit:	30				
Light Conditions:	Daylight: regardless of presence of streetlights				
Carriageway Hazards:	None				
Junction Detail:	T or staggered junction				
Junction Pedestrian Crossing:	No physical crossing facility within 50 metres				
Road Type:	Single carriageway				
Junction Control:	Give way or uncontrolled				



For more information about the data please visit: www.crashmap.co.uk/home/Faq
To subscribe to unlimited reports using CrashMap Pro visit www.crashmap.co.uk/Home/Premium_Services



Validated Data

Vehicles involved

Vehicle Ref	Vehicle Type	Vehicle Age	Driver Gender	Driver Age Band	Vehicle Manoeuvre	First Point of Impact	Journey Purpose	Hit Object - On Carriageway	Hit Object - Off Carriageway
1	Car (excluding private hire)		2 Male	56 - 65	Vehicle is in the act of turning right	Offside	Other	None	None
2	Van or goods vehicle 3.5 tonnes mgw and under		5 Male	46 - 55	Vehicle proceeding normally along the carriageway, not on a bend	Front	Other	None	None

Casualties

Vehicle Ref	Casualty Ref	Injury Severity	Casualty Class	Gender	Age Band	Pedestrian Location	Pedestrian Movement
1	1	Slight	Vehicle or pillion passenger	Female	Over 75	Unknown or other	Unknown or other

For more information about the data please visit: www.crashmap.co.uk/home/Faq

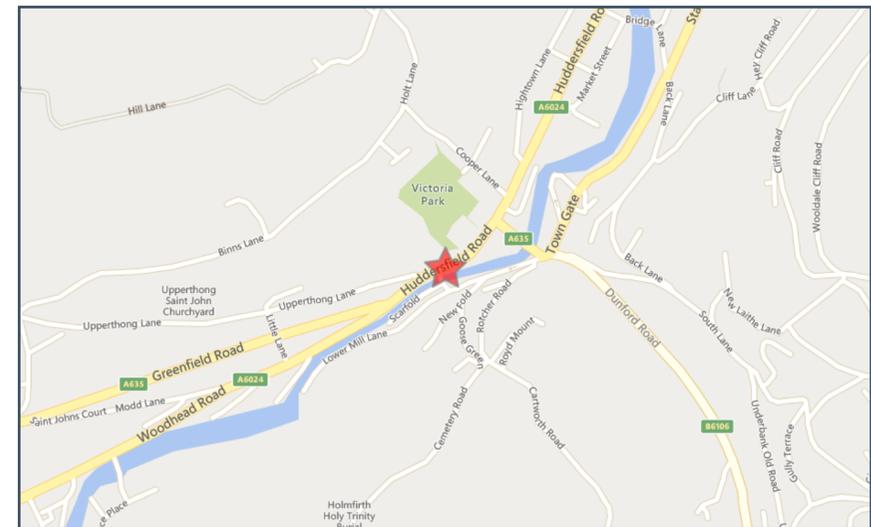
To subscribe to unlimited reports using CrashMap Pro visit www.crashmap.co.uk/Home/Premium_Services



Validated Data

Crash Date: Sunday, February 04, 2018 **Time of Crash:** 10:00:00 PM **Crash Reference:** 2018135241856

Highest Injury Severity:	Slight	Road Number:	A6024	Number of Casualties:	3
Highway Authority:	Kirklees			Number of Vehicles:	2
Local Authority:	Kirklees			OS Grid Reference:	414082 408124
Weather Description:	Fine without high winds				
Road Surface Description:	Wet or Damp				
Speed Limit:	30				
Light Conditions:	Darkness: street lights present and lit				
Carriageway Hazards:	None				
Junction Detail:	Not at or within 20 metres of junction				
Junction Pedestrian Crossing:	No physical crossing facility within 50 metres				
Road Type:	Single carriageway				
Junction Control:	Not Applicable				



For more information about the data please visit: www.crashmap.co.uk/home/Faq
To subscribe to unlimited reports using CrashMap Pro visit www.crashmap.co.uk/Home/Premium_Services



Validated Data

Vehicles involved

Vehicle Ref	Vehicle Type	Vehicle Age	Driver Gender	Driver Age Band	Vehicle Maneouvre	First Point of Impact	Journey Purpose	Hit Object - On Carriageway	Hit Object - Off Carriageway
1	Car (excluding private hire)	10	Male	26 - 35	Vehicle is performing a U turn	Back	Journey as part of work	None	None
2	Car (excluding private hire)	3	Female	16 - 20	Vehicle proceeding normally along the carriageway, not on a bend	Front	Other	None	Other permanent object

Casualties

Vehicle Ref	Casualty Ref	Injury Severity	Casualty Class	Gender	Age Band	Pedestrian Location	Pedestrian Movement
2	1	Slight	Driver or rider	Female	16 - 20	Unknown or other	Unknown or other
2	2	Slight	Vehicle or pillion passenger	Female	16 - 20	Unknown or other	Unknown or other
2	3	Slight	Vehicle or pillion passenger	Male	16 - 20	Unknown or other	Unknown or other

For more information about the data please visit: www.crashmap.co.uk/home/Faq

To subscribe to unlimited reports using CrashMap Pro visit www.crashmap.co.uk/Home/Premium_Services