



Proposed Lidl Store, St Luke's Hospital Site,
Crosland Moor, Huddersfield

Design and Access Statement – Update

December 2023





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1 Introduction

1.1 This Design and Access Statement (DAS) has been prepared by Lichfields on behalf of Lidl GB Ltd (“Lidl”). It accompanies a full planning application for the development of part of the former St Luke’s Hospital site, Crosland Moor, Huddersfield, to provide a Lidl foodstore.

1.2 The document considers the overall design and layout of the proposed development and how this has evolved from concept through to the final proposals. The document also considers wider accessibility and sustainability matters.

Scope of the Report

1.3 This Statement responds to the Town and Country Planning (Development Management Procedure) (England) Order 2015 requirement that certain planning applications be accompanied by a Design and Access Statement.

Structure of the Report

1.4 The report structure is as follows:

- **Section 2.0:** summarises the context of the application proposals, including a description of the site and its surroundings, its accessibility and relevant planning policy context;
- **Section 3.0:** sets out the constraints and opportunities;
- **Section 4.0:** describes how the scheme has evolved following pre-application discussions with the local planning authority and consultation with the local community;
- **Section 5.0:** describes the design of the proposed development;
- **Section 6.0:** sets out the issues relating to access;
- **Section 7.0:** considers the sustainability of the proposed development; and
- **Section 8.0:** assesses the proposed development against relevant design and access related planning policy.



LIDL

2 Context

2.1 This section demonstrates the applicant’s understanding of the physical characteristics of the site and its context. ‘Context’ relates to the physical, social and economic characteristics of the area, together with the planning history and planning policy context. Advice received during the pre-application process are of particular relevance to this understanding of the wider context, along with the adopted planning policies and guidance that affect and shape the development of the site.

Physical Context

Site Description

2.2 The application site is located to the south of Blackmoorfoot Road, in the Crosland Moor area on the south-western side of Huddersfield and covers around 0.72 ha. It falls within the wider former St Luke’s Hospital site, which is subject to residential-led development by Avant Homes, and which is being marketed as Fitzwilliam Grange. The site directly fronts onto Blackmoorfoot Road.

Surrounding Area

2.3 The site is bound:

- to the north by Blackmoorfoot Road;
- to the east by Turnstone Way (the access road to the Fitzwilliam Grange development) and existing properties on Chapel Terrace;

- to the south and west by residential properties within the Fitzwilliam Grange development.

2.4 The surrounding area is predominantly in residential use however there are a number of small commercial and retail units less than 100m to the east of the site on Blackmoorfoot Road. These commercial and retail units are designated as forming part of a Local Centre in the Kirklees Local Plan (Crosland Moor Local Centre). There is another cluster of commercial uses located around 400m to the west of the site on Blackmoorfoot Road, which is also designated as a Local Centre in the Local Plan (‘Blackmoorfoot Road, Crosland Moor’ Local Centre).



Figure 2.1 Site Location Plan

(Note: the red line is an approximate boundary. See the Site Location Plan which accompanies the application for an exact red line boundary)

2 Context

Accessibility

- 2.5 The site occupies an accessible location within a predominantly residential area. As noted above, the site lies less than 100m walking distance from the boundary of Crosland Moor Local Centre and is therefore an edge-of-centre site in policy terms. The site is conveniently accessible on foot from the surrounding residential areas, via a network of existing footways, and the proposed development also incorporates cycle parking spaces.
- 2.6 Bus stops are located close to the site on Blackmoorfoot Road, providing services to and from surrounding areas, such as Crosland Hill and Thornton Lodge. The nearest railway station is Lockwood,

which is located around 800m to the south-east (approximately 10 minutes' walking distance). Lockwood station is on the Penistone line, with services running between Huddersfield and Sheffield. The site is also easily accessible by private car, with Blackmoorfoot Road joining the A62/Manchester Road, which connects Huddersfield with locations to the south-west, such as Linthwaite, Slaithwaite and Marsden.

- 2.7 The application site is therefore easily accessible via a range of transport modes, including via public transport and on foot or by bicycle.

Planning Policy Context

- 2.8 A brief summary of the policies and guidance which have affected and shaped the form of development on the site is provided below.

The Development Plan

- 2.9 The current development plan for Kirklees comprises:
- 1 Kirklees Local Plan (“KLP”): Strategy and Policies (adopted February 2019);
 - 2 Kirklees Local Plan: Allocations and Designations (adopted February 2019); and
 - 3 Interactive Policies Map associated with the above
- 2.10 The KLP sets the development requirements for the District over the plan period from 2013 - 2031, allocates land for development and identifies sites subject to relevant policies.
- 2.11 The KLP Policies Map identifies the site as part of a wider Mixed-Use allocation (site ref. MXS1 – ‘Land south of Blackmoorfoot Road, Crosland Moor, Huddersfield’). For further details, please refer to the Planning & Retail Statement.

- 2.12 Criteria for how new development should achieve good design are set out at KLP Policy LP24. This policy advises that design should be at the core of all proposals in the district and should be considered at the outset of the development process, ensuring that design forms part of pre-application consultation of a proposal. Proposals should promote good design by ensuring:
- the form, scale, layout and details of all development respects and enhances the character of the townscape, heritage assets and landscape;
 - they provide a high standard of amenity for future and neighbouring occupiers, incorporating means of screening where necessary;
 - high levels of sustainability, to a degree proportionate to the proposal through:
 - i considering the use of innovative construction materials and techniques, including reclaimed and recycled materials

- ii providing charging points to encourage the use of electric and low emission vehicles
- the needs of a range of different users are met, including disabled people, older people and families with small children to create accessible and inclusive places; and
- the retention of valuable or important trees and where appropriate the planting of new trees and other landscaping to maximise visual amenity and environmental benefits

All major planning applications are expected to be accompanied by Travel Plans and encourage sustainable modes of travel (KLP Policy LP20), and proposals are required to demonstrate adequate information and mitigation measures to avoid a detrimental impact on highway safety and the local highway network (KLP Policy LP21).

National Planning Policy Framework

- 2.13 The National Planning Policy Framework (NPPF) (July 2021) provides guidance on design as part of its broader presumption in favour of sustainable development.
- 2.14 The Government advises that good design is a key aspect of sustainable development, creates better places in which to live and work and helps make development acceptable to communities (paragraph 126).
- 2.15 Paragraph 130 of the NPPF sets out that planning decisions should aim to ensure that developments:
- will function well and add to the overall quality of the area, not just for the short term but over the lifetime of the development;
 - are visually attractive as a result of good architecture, layout and appropriate and effective landscaping;

2 Context

- are sympathetic to local character and history, including the surrounding built environment and landscape setting, while not preventing or discouraging appropriate innovation or change (such as increased densities);
 - establish or maintain a strong sense of place, using the arrangement of streets, spaces, building types and materials to create attractive, welcoming and distinctive places to live, work and visit;
 - optimise the potential of the site to accommodate and sustain an appropriate amount and mix of development (including green and other public space) and support local facilities and transport networks; and
 - create places that are safe, inclusive and accessible, and which promote health and well-being, with a high standard of amenity for existing and future users; and where crime and disorder, and the fear of crime, do not undermine the quality of life or community cohesion and resilience.
- 2.16 This section of the NPPF also adds that applicants should work closely with those affected by their proposals to evolve designs that take account of the views of the community. Applications that can demonstrate early, proactive and effective engagement with the community should be looked on more favourably than those that cannot (paragraph 132).
- 2.17 With regards to promoting sustainable transport, paragraph 112 states that developments should be located and designed to give priority to pedestrian and cycle movements, and facilitate access to high quality public transport facilities; address the needs of people with disabilities; create safe and secure layouts which minimise conflicts between traffic and cyclists, pedestrians and vehicles, avoid street clutter and respond to local character and design standards; allow for the efficient delivery of goods and services by service and emergency vehicles; and
- enable charging of plug-in and other ultra-low emission vehicles.
- 2.18 Furthermore, all developments that generate significant amounts of movement should be supported by a Transport Statement (or Assessment) and Travel Plan (para 113).
- 2.19 Importantly, paragraph 111 establishes that development should only be prevented or refused on transport grounds where there would be an unacceptable impact upon highway safety, or the residual cumulative impacts of development are severe.
- ### Planning Practice Guidance
- 2.20 The National Design Guide is part of national Planning Practice Guidance (PPG). PPG accompanies the NPPF and is regularly updated online.

2.21 The PPG states that good design should be fit for purpose, durable and bring delight. A well-designed place is achieved through making the right choices on multiple components including the layout, the form and scale of buildings, the appearance, the landscape, the materials and the detailing of buildings.

2.22 Well-designed new development responds positively to the features of the site itself and the surrounding context, enhancing positive qualities and improving negative ones.

2.23 The PPG also notes that well-designed places contribute to local distinctiveness. Materials are selected with care for their context, they are attractive but also practical, durable and affordable. They contribute to visual appeal and local distinctiveness.

2.24 It also states that well-designed spaces feel safe for people who occupy the buildings, visitors and passers-by. Careful planning and design create the right conditions for people to feel safe and secure, without the need for additional security measures.

Planning History

2.25 Outline planning permission (with all matters reserved except for access) was granted in 2015 for a residential-led mixed use development of the site (ref. 2014/60/93099/W). An indicative masterplan submitted as part of this application showed a residential layout of c. 200 dwellings, two retail units (comprising 30,000 sqft and 12,000 sqft respectively) and a public house/ restaurant.

2.26 Since the grant of outline permission, a number of applications for amendments to this permission (under Section 73) and

for the approval of reserved matters have been approved, as detailed in Table 2.1.

The residential element of the scheme, known as Fitzwilliam Grange, has now been built out by Avant Homes. Whilst this application seeks a ‘fresh’ planning permission for the development of a Lidl foodstore on part of the wider site, as set out above, the principle of retail development on the site was established by the original outline planning permission.

2.27 For further details of the planning history of the site, please refer to the Planning & Retail Statement.

3 Constraints and Opportunities

3.1 This section of the statement identifies potential opportunities and constraints in respect of the site and considers how these have influenced the development.

Constraints

3.2 The site's constraints include:

- The proximity of residential properties to the east, south and west of the site;
- The two trees within the site which are protected by Tree Preservation Order;
- The setting of the listed buildings on Blackmoorfoot Road;
- The wider setting of the Castle Hill Scheduled Monument; and
- The potential for congestion at the proposed site access on Blackmoorfoot Road/Turnstone Way.

Opportunities

3.3 The opportunities presented by the site include:

- Enhancement of the range of retail provision in the local area, increasing local choice and competition, clawing back spending and reducing unsustainable shopping patterns;
- The opportunity to develop a vacant site in a sustainable location, as well as bringing forward a commercial use that will serve the local community;
- The provision of a landscaping scheme including the planting of new trees, as well as a landscape buffer to the southern and western boundaries to help the development assimilate into its surroundings and screen the development from neighbouring residential properties;
- Potential to develop a new foodstore with a contemporary design, which preserves and enhances the setting of nearby heritage assets;
- Delivery of a foodstore which benefits from good accessibility to sustainable modes of transport and movement (i.e. walking, cycling and public transport); and
- The opportunity to provide rapid electric vehicle charging spaces.

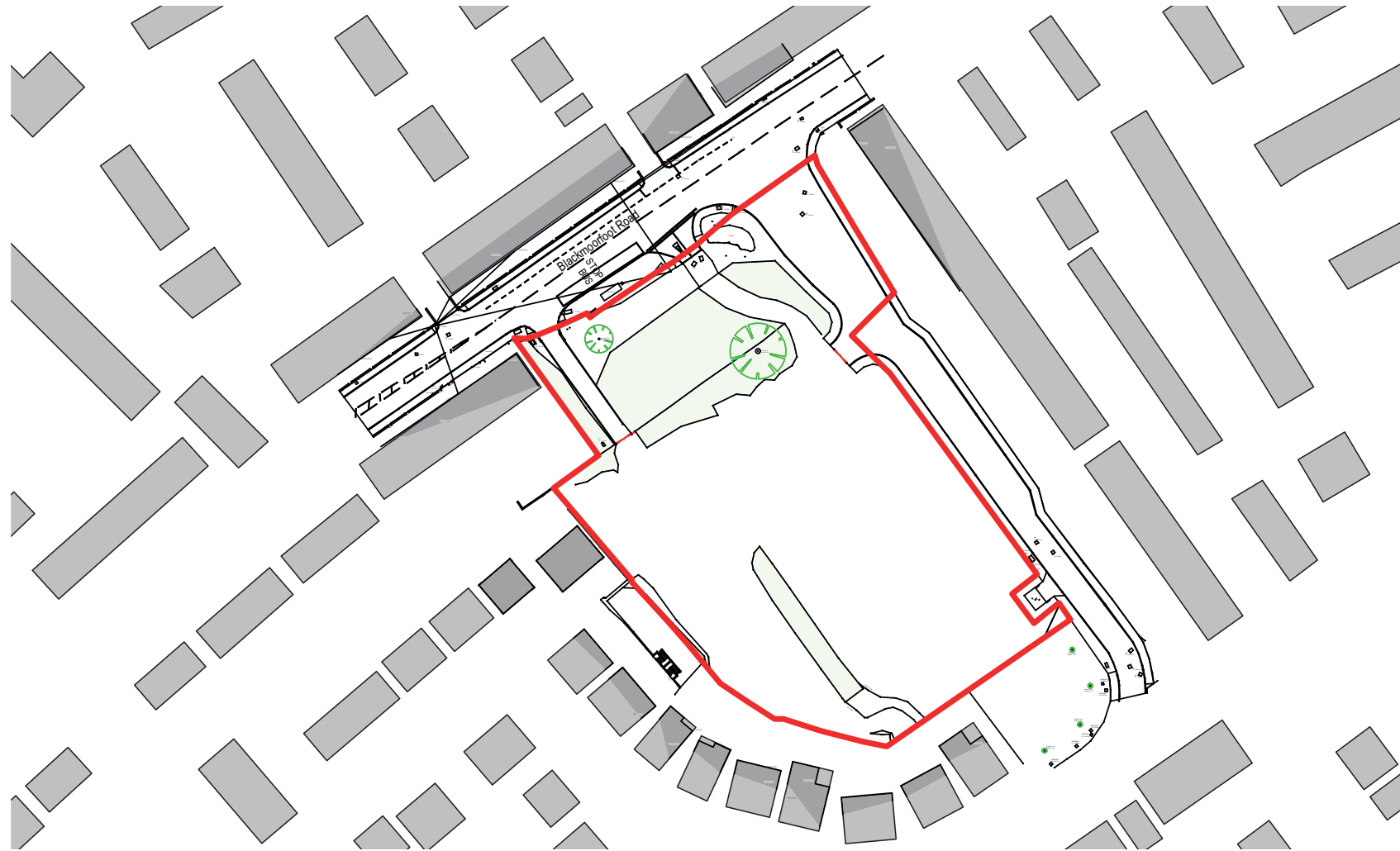


Figure 3.1 Location Plan

4 Scheme Evolution

- 4.1 The design and layout of the proposals has evolved over time. This has been in response to discussions with the local planning authority (both pre and post-application submission) and the Designing Out Crime Officer, Lidl's operational requirements, and the community consultation exercise. The scheme has also evolved to address site constraints, matters that have arisen as technical assessments have been undertaken and to deliver scheme improvements.
- 4.2 This section explains how the site context and planning policies outlined in the earlier sections have influenced the design approach to the proposed development. It also outlines how the pre-application engagement with the local planning authority and local community has helped to inform the final design of the scheme.

Pre-Application Engagement

- 4.3 The application proposals have been subject to a range of pre-application consultation methods. Full details of the consultation process and its outcomes are provided in the accompanying Community Consultation Statement and Planning Statement. In summary, pre-application engagement comprised:
- Discussions with officers at Kirklees Council in September 2021 as part of a pre-application enquiry. This included consideration of the principle of development, the scope of retail impact assessment, amenity impacts, design and layout concepts, and highways considerations.
 - Engagement with the local community in March and April 2023. This involved:
 - a The distribution of a consultation leaflet to approximately 10,000 homes and businesses in Crosland Moor from 31 March 2023 onwards, informing residents and stakeholders of the application proposals. This leaflet also provided details of a community consultation website where further information could be found and comments submitted.
 - b Lidl wrote directly to local Councillors during the public consultation period to offer further information about the scheme and also to provide an opportunity to discuss the proposals in detail. A meeting was held with a Councillor for the Crosland Moor and Netherton ward with a representative from Lidl and members of the design and consultant team.
 - c Hosting a community consultation website which provided information about the scheme and contained an online feedback form, allowing residents and stakeholders to submit their comments regarding the proposals.

- d An email address was also provided on the website for residents and stakeholders to ask any questions or make additional comments on the scheme.

4.4 As set out in the Community Consultation Statement, there was a significant level of local interest in relation to the proposals. 968 responses were received from members of the public via completed feedback forms and the consultation website, with the vast majority of respondents (85.6%) confirming their support for the scheme.

Community Consultation Feedback

4.5 A majority of respondents support the principle of improving food shopping provision in Crosland Moor and welcomed the proposals. Some concerns were raised in terms of the proposed development's design, layout and accessibility which are summarised below:

- Concerns relating to increased traffic as a result of the proposals;
- Amenity impacts upon neighbouring residents; and
- The exacerbation of existing pedestrian safety issues on the road network surrounding the site.

4.6 These comments are considered in greater detail within the accompanying Community Consultation Statement.

Responding to Feedback

4.7 The scheme's design has evolved to consider comments received at the pre-application stage in the following ways:

- The planning application is accompanied by a comprehensive Transport Assessment and Travel Plan that demonstrates that the application site is in a sustainable location in respect of travel by a range of modes of transports. The site can be accessed safely and there is adequate capacity in

the local highways network with which to accommodate any uplift in vehicular trips associated with the proposals.

- The site layout and levels have been carefully designed so as to minimise potential amenity impacts on neighbouring residential properties.
- The recommendations of the Noise Impact Assessment have been incorporated including a 2.4 m timber fence is provided along the southern and south-western boundary of the loading bay. Lidl is also willing to accept a restriction on delivery hours, in accordance with the previously granted planning permission for the site. For further details, please refer to the Noise Impact Assessment which accompanies the application.
- The landscaping scheme includes the creation of landscape buffers to the site boundaries to ensure that any potential visual impacts from the foodstore are adequately mitigated and the privacy of neighbouring properties preserved.

4 Scheme Evolution

- The proposed parking spaces meet the Council’s requirements in terms of level of provision and size. Secure cycle storage provision will also be available for customers and employees. Details of the proposed Electric Vehicle Charging Points (EVCPs) are included in the application.
- The design, construction and operation of the store will prioritise environmental sustainability measures to ensure high standards of energy efficiency and significant reductions in carbon emissions. Section 7.0 of this DAS covers the sustainability of the proposed Lidl store in greater detail.

Post-submission updated scheme layout

- 4.8 Since submission of the planning application, Lidl has reviewed the layout of the proposed store. The store format has been amended slightly to meet Lidl’s latest operational requirements and is slightly larger than the previously proposed layout. The sales area has increased from 1,241

sqm to 1,331 sqm (an increase of 7.3%) and the overall floorspace has increased from 1,902 sqm GIA to 2,000 sqm GIA (an increase of 5.2%). The change in store format is addressed in the Retail Impact Assessment note.

- 4.9 In addition to the change to the store format, the foodstore has been relocated to the eastern corner of the site in response to comments from planning officers. The number of proposed car parking spaces has increased from 90 to 93. This includes six disabled spaces, nine parent and child spaces, and two electric vehicle charging points.

- 4.10 The repositioning of the store to the eastern side of the site will mean the store has a better and more active relationship with Blackmoorfoot Road and would line and define the estate road. The foodstore remains set back within the site (i.e. rather than being up to the Blackmoorfoot Road), which means the building would not be overbearing and will also not give rise to

any harm to the group of listed houses opposite the site on Blackmoorfoot Road.

- 4.11 The applicant’s design team has met with Richard Thornton, Designing Out Crime Officer at West Yorkshire Police. Following this discussion, the following elements have been incorporated into the design of the scheme:

- Updated boundary treatment drawing to have vertical timber fence with 20 mm gap between timbers for natural surveillance.
- Bollards have been added at the site entrance, with 1.2 m spacing.

- 4.12 Cycle parking and motorcycle parking both shown, including a ground anchor for the latter. An indicative ANPR system is also shown on the first lighting pole from the vehicular entrance. The natural surveillance afforded by the proposed site layout is also an important Secured by Design principle.

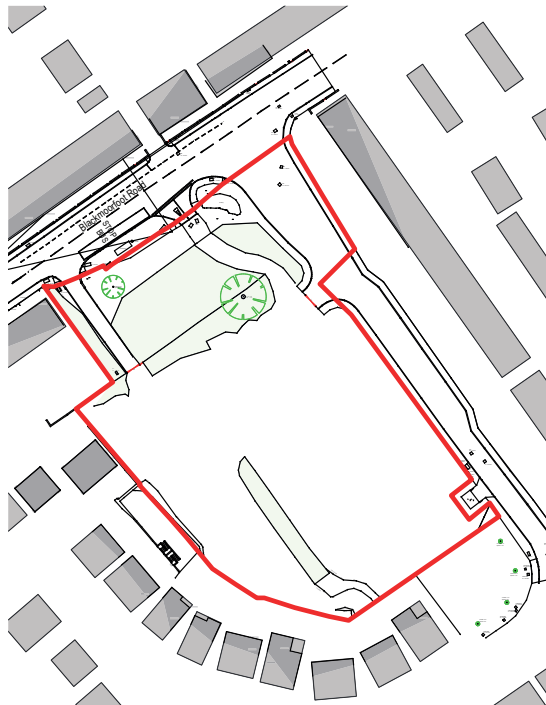


Figure 4.1 Existing Site Plan

Key Design Principles

4.13 The following overarching design principles have been established based upon the analysis of the site and its context and the pre-application feedback from officers and members of the public as set out above. These principles have guided the project team in the formulation of the scheme:

- 1 The need to provide a modern new Lidl foodstore, of a size and appearance consistent with Lidl's branding and business model;
- 2 The need for a design quality which is appropriate to the site's setting, the surrounding residential development, and the nearby heritage assets (listed buildings and Scheduled Monument);
- 3 The need to protect the amenity of existing and future occupiers of adjacent residential properties;
- 4 The need to provide additional tree/hedgerow planting and screening;
- 5 The opportunity to capitalise on the application site's existing accessibility by travel modes such as walking, cycling and by bus;
- 6 The shape, size and topography of the site;
- 7 The need to maximise natural surveillance and reduce the potential for crime and antisocial behaviour; and
- 8 The need to promote high levels of sustainability to minimise the carbon footprint of the development and utilise energy conservation and efficiency methods where feasible and viable.

5 The Proposed Development

5.1 Having considered the constraints and opportunities of the site, and discussed the background of the application, this section sets out details of the scheme’s final proposed design.

The Application Scheme

5.2 The scheme proposes a new Lidl foodstore, providing a spacious and attractive shopping environment for local residents and visitors in a convenient and accessible location.

5.3 The proposals development will include:

- a 2,000 sqm gross internal area / 1,331 sqm net (sales) Lidl foodstore;
- 93 car parking spaces, including six disabled spaces, nine parent and child spaces, and two electric vehicle charging points;
- vehicular and pedestrian access from Turnstone Way;

- a servicing area to the eastern side of the store; and
- a co-ordinated scheme of landscaping across the site.

Use

5.4 The proposed new Lidl foodstore will comprise Class E floorspace. Within the store itself, there will be a sales area, along with warehouse and delivery facilities, a freezer room and chilled room, bakery, an IT room, manager’s office, welfare area and staff cloakroom.

5.5 The wider site will comprise car parking areas, along with a servicing area, pedestrian and vehicular accesses and associated landscaping.



Figure 5.1 CGI of Site Layout

5 The Proposed Development

Amount

- 5.6 The proposed Lidl store will have a gross internal area of 2,000 sqm and a net sales area of 1,331 sqm. The majority of the sales area (around 80%) will be used for convenience goods sales (i.e. predominantly food) and the remainder will be occupied by comparison goods (i.e. non-food).
- 5.7 93 car parking spaces will be provided across the application site, including six accessible spaces, nine parent child spaces and two electric vehicle charging points. Lidl will also provide cabling for further electric vehicle charging points to be added in the future, depending on demand.

Layout

- 5.8 The development proposal comprises a new Lidl foodstore, together with a dedicated delivery area, associated car and cycle parking.
- 5.9 The layout of the proposal has been influenced by an iterative design process where a number of options and suggestions put forward during the preapplication engagement have been tested.
- 5.10 The new Lidl store would be accommodated within the eastern part of the site. The entrance to the store would be located on the western corner of the building and would be linked to parking spaces in the northern and western parts of the site by dedicated pedestrian routes. Internally, the store would be modern in design, including a glazed shopfront, consistent with Lidl's corporate style.

The elevations will include natural stone to match the materiality of the surrounding townscape.

- 5.11 Vehicular access to the site is proposed to be taken via Turnstone Way, on the eastern boundary of the site. The existing vehicular access would be slightly modified to allow for HGV/vehicle access. A dedicated pedestrian route will also be provided.
- 5.12 Pedestrian routes and crossings will provide a coherent route through the site. Cycle parking and rapid electric vehicle charging points will be provided within the site's car park. A service yard will be located to the southern of the store.
- 5.13 Additional planting, including a landscape buffer to the southern and western boundaries to screen the development from neighbouring residential properties, would also be provided.

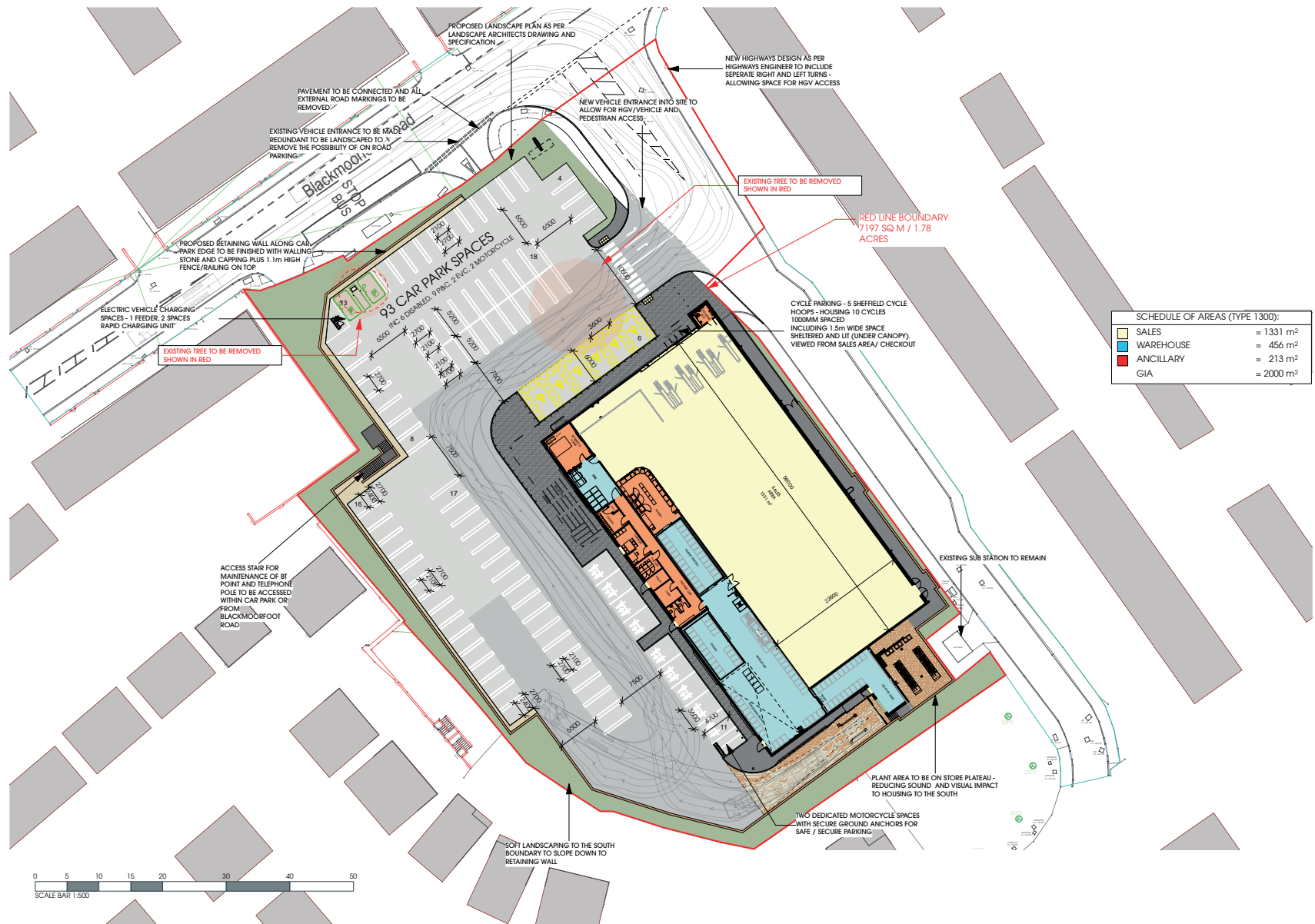


Figure 5.2 Proposed Site Plan

5 The Proposed Development

5.14 Further factors which have influenced the layout of the proposal include accessibility; natural surveillance and security; existing trees and hedgerows; the amenity of adjoining residents; and the operational needs of Lidl, which have to take into account deliveries, warehousing and staff welfare.

5.15 The application scheme represents the optimum layout for the site and responds to the different factors mentioned above and discussions between the project team and local authority / local community. Overall, the proposals provide a development scheme with good accessibility and clear wayfinding for visitors.

Scale

5.16 The application site extends to approximately 0.7 hectares. The proposed Lidl foodstore has a rectangular shaped footprint and will extend to an area of 2,000 sqm (GIA) with an approximate length of 70 metres. The proposed store will have a mono-pitched roof.

5.17 Plant equipment for the Lidl store will be located to the south of the store, in the southern corner of the site. The plant store will be partially covered by a short section of retaining wall. The accompanying Noise Impact Assessment recommends provision of a timber fence around the plant area and this has been incorporated within the proposals.

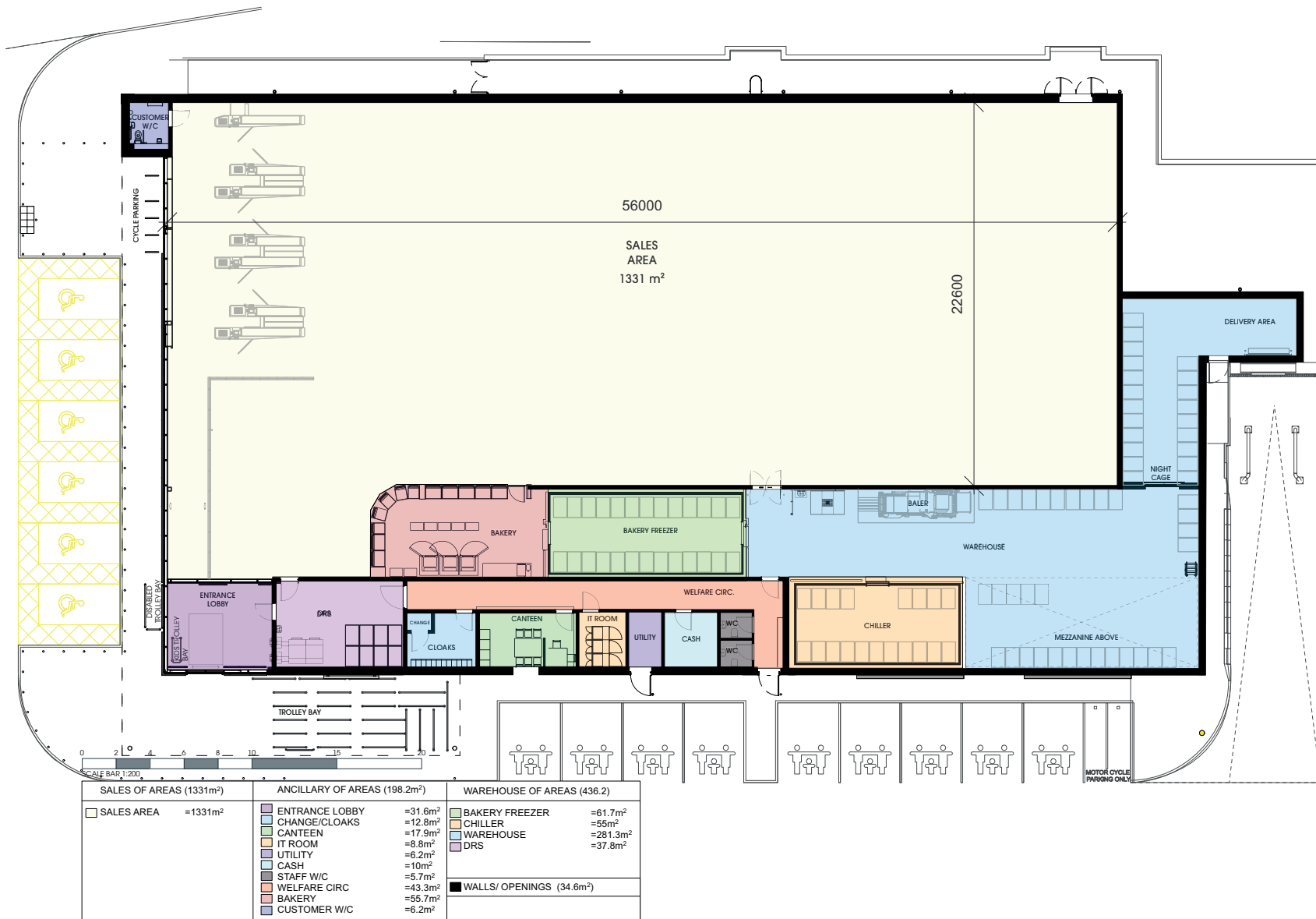
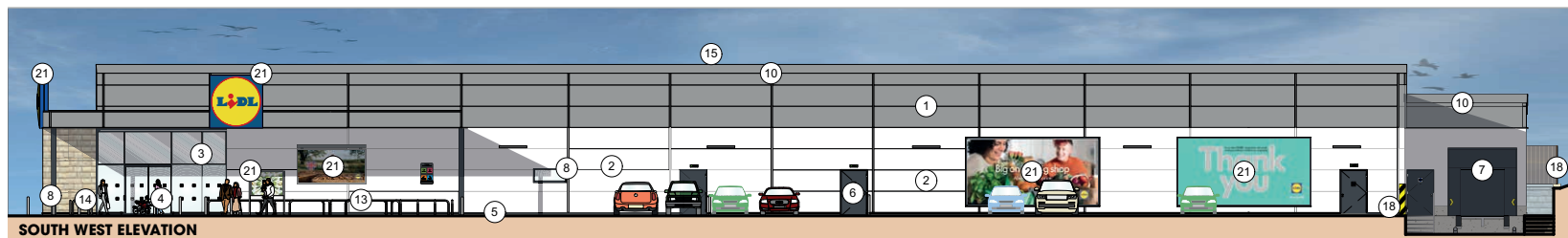
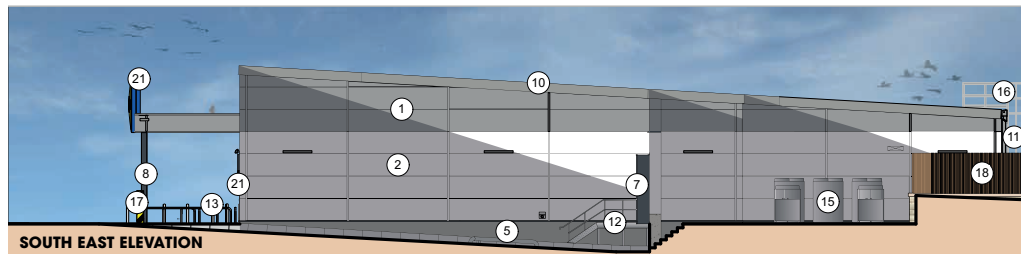
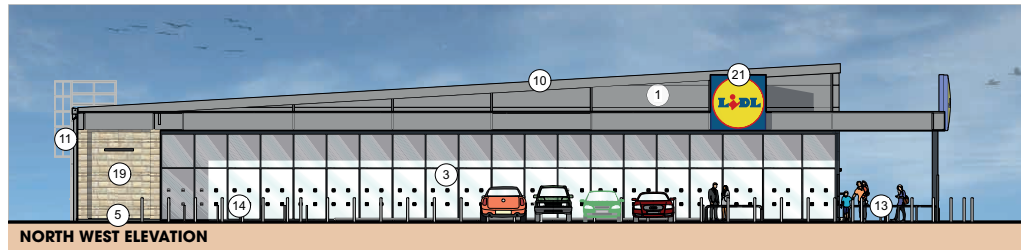


Figure 5.3 Proposed Internal Area Breakdown

5 The Proposed Development

Appearance

- 5.18 The store will have a contemporary design that is in-keeping with Lidl's brand identity, yet also responds appropriately to the local context.
- 5.19 The proposed materials palette for the Lidl foodstore includes a grey colour metal composite roof, while the external walls have a mix of stone-coloured render and will feature buff facing locally sourced stone. A glazed shopfront is proposed on the store's south west elevation.
- 5.20 Boundary treatment includes handrails, the retention of existing fencing, and retaining structures alongside substantial landscaping buffers. A timber rail will be provided at the boundaries around the site entrance.
- 5.21 In respect of heritage considerations, the proposed development has been carefully designed to ensure that the significance and special architectural and historic interest of listed buildings on Blackmoorfoot Road and the Castle Hill Scheduled Monument is preserved and that any visual effects on the setting of these heritage assets are minimised. The proposed development would not adversely affect the historic and architectural significance of the listed buildings and would have a neutral effect upon their setting. For further details, please refer to the Heritage Impact Assessment which accompanies the planning application.
- 5.22 Illustrative advertisement signage is indicated on the application drawings. All signage will be subject to an application for advertisement consent which will be submitted at a later date.



MATERIALS KEY

- ① INSULATED METAL CLADDING PANELS - COLOUR: GREY (RAL 9006)
- ② INSULATED METAL CLADDING PANELS - COLOUR: WHITE (RAL 9010)
- ③ CURTAIN WALLING - DOUBLE GLAZED UNITS - COLOUR: GREY (RAL 7024)
- ④ ENTRANCE DOORS - DOUBLE GLAZED UNITS - COLOUR: GREY (RAL 7024)
- ⑤ LOW LEVEL RENDER - COLOUR: GREY (RAL 7038)
- ⑥ POWDER COATED STEEL DOORS - COLOUR: GREY (RAL 7024)
- ⑦ SECTIONAL DOOR - COLOUR: GREY (RAL 7024)
- ⑧ NON OPENABLE FIXED SAFETY GLASS WINDOWS - COLOUR: GREY (RAL 7024)
- ⑨ METAL COMPOSITE INSULATED ROOF PANELS
- ⑩ STEEL COPING FLASHING TO ROOF PERIMETER - COLOUR: GREY (RAL 9006)
- ⑪ ALUMINIUM GUTTERS AND RAIN WATER PIPES - COLOUR: GREY (RAL 9006)
- ⑫ GALVANISED METAL DRIVERS STEPS, STAIRCASE AND BALLUSTERS WITH HANDRAILS. ALL STEPS TO BE CONSTRUCTED IN ACCORDANCE WITH BUILDING REGS PART K
- ⑬ STAINLESS STEEL TROLLEY GUIDES (MARINE GRADE 316) SPACES FOR WHEELCHAIR SHOPPING TROLLEYS AND TROLLEYS WITH BABY SEATS.
- ⑭ STAINLESS STEEL CYCLE STANDS, 750mm HIGH, ROOT FIXED TO CONCRETE FOUNDATION.
- ⑮ M&E ITEMS, SIZE AND LOCATIONS ARE INDICATIVE ONLY
- ⑯ ROOF ACCESS LADDER
- ⑰ CONCRETE FILLED GALVANISED STEEL HGV BOLLARD PAINTED BLACK & YELLOW AT 45DEG.
- ⑱ LOCALLY SOURCED STONE FACED RETAINING WALL FINISHED WITH CLEAN CUT ASHLAR AND 1.8m VERT. HIT AND MISS TIMBER FENCE
- ⑲ BUFF FACING NATURAL LOCALLY SOURCED STONE. FINISHED WITH CLEAN CUT ASHLAR BANDING.
- ⑳ STONE COLOURED RENDER, COLOUR TO MATCH (20) NATURAL LOCAL SOURCED STONE.
- ㉑ ADVERTISING SIGNAGE INDICATIVE ONLY

Figure 5.4 Proposed Elevations

5 The Proposed Development

Landscaping

- 5.23 A landscaping scheme will be implemented across the site in order to enhance the development's appearance, as well as its assimilation into the streetscene and to reduce the visual impact of the scheme and protect the amenity of existing residents. Additional tree planting will also be provided.
- 5.24 As shown on the Landscape Masterplan (drawing no. R/2682/1C), a co-ordinated scheme of landscaping around the site is proposed with planting of new trees and shrubs, including a substantial landscape buffer to the south and west which will help to screen the development from neighbouring residential properties.
- 5.25 Hard landscaping will also be used to differentiate the different parts of the site. Tarmac surfacing will be used across the vehicular access points, pedestrian links and car park areas with block paving around the entrance to the store.

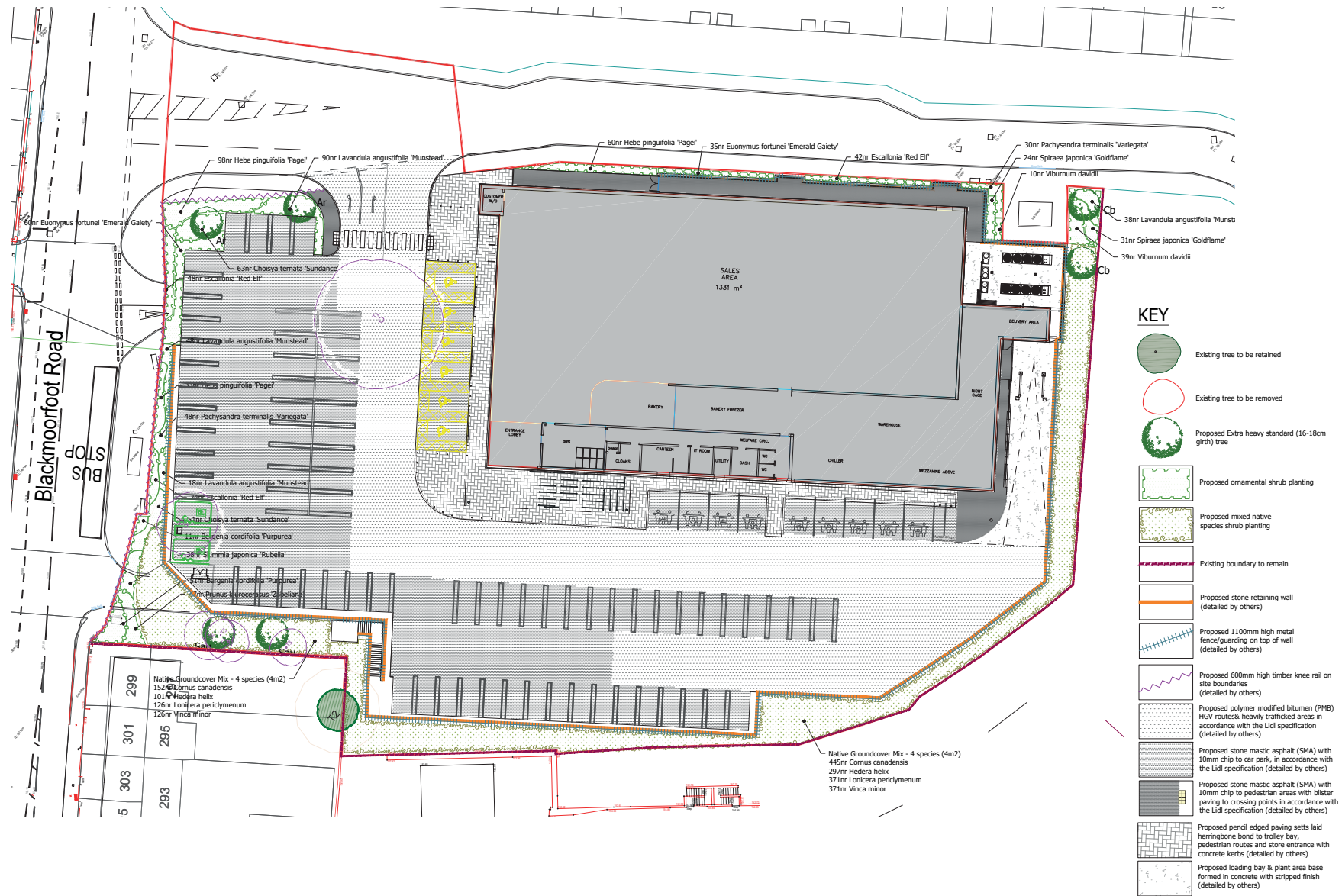


Figure 5.5 Proposed Landscape Plan

6 Access

6.1 The application is situated in a sustainable location and is easily accessible via a range of transport modes including by foot, cycle, bus and private car.

Pedestrian, Cycle and Bus Access

6.2 The site occupies an accessible location within 100m walking distance of Crosland Moor Local Centre. The site is conveniently accessible on foot from the surrounding residential areas, via a network of existing footways.

6.3 Five Sheffield cycle stands will be provided along the northern elevation of the building, close to the stepped pedestrian access. These stands will provide space for 10 bicycles for use by customers. Lidl staff will also be able to store bicycles in the back of house area of the store.

6.4 Bus stops are located close to the site on Blackmoorfoot Road, providing services to and from surrounding areas, such as Crosland Hill and Thornton Lodge.

6.5 The nearest railway station is Lockwood, which is located around 800m to the south-east (approximately 10 minutes' walking distance). Lockwood station is on the Penistone line, with services running between Huddersfield and Sheffield.

6.6 It is therefore considered that the site is accessible via a range of sustainable transport modes.



Vehicular Access

- 6.7 Access to the proposed Lidl foodstore will be provided from an existing priority controlled T-Junction with Turnstone Way along the north-eastern boundary of the site. A T-junction in this location has been constructed as part of the construction of Turnstone Way, with a short stub leading into the site which will form the access to the site. The site is easily accessible by private car, with Blackmoorfoot Road joining the A62/Manchester Road, which connects Huddersfield with locations to the south-west, such as Linthwaite, Slaithwaite and Marsden.
- 6.8 With regard to servicing, the largest type of vehicle expected to access the site will be for delivery and servicing requirements, which will be a 16.5-metre-long maximum legal length articulated HGV. These types of vehicles will be used to provide deliveries to the store and will use the servicing bay on the southern side of the Lidl store. Vehicle swept path analysis has been undertaken which has demonstrated that the proposed site access arrangements and internal site layout can safely accommodate movements associated with the delivery vehicle. For further details in relation servicing, access and highways matters, please refer to the Transport Assessment.
- 6.9 The car park will have a total capacity for 93 spaces, including six accessible spaces, two EVCPs and nine parent & child spaces.
- 6.10 Full details of the proposed vehicular access, associated highways works and parking spaces are provided within the accompanying Transport Assessment and Travel Plans.

7 Sustainability

- 7.1 The Lidl foodstore will contribute to meeting sustainable development targets, helping to limit carbon dioxide emissions and thereby providing resilience to climate change.
- 7.2 Lidl UK GB has a corporate environmental policy which ensures that all new stores achieve high standards of energy efficiency. This environmental policy represents a conscious effort to reduce carbon emissions through responsible and considerate operating procedures. A company-wide programme aligned to ISO:50001 closely monitors the energy used and helps to identify efficiency improvements in how Lidl source their energy requirements, what technologies they use and what changes can be made to ensure maximum efficiency and sustainability.
- 7.3 Key features of the company's environmental policy, which will be implemented in respect of the proposed new store are as follows:

Solar Photovoltaic

- Since 2020, Lidl has been aiming to install solar photovoltaic ("PV") systems on all appropriate stores. A 180kWp solar PV system will be installed onto the roof of the new Lidl foodstore and will generate approximately 25% of the store's electricity requirements per year, reducing carbon emissions by at least 44 tonnes per year. The Solar PV panels will be installed flush onto the store's roof and are also non-reflective to avoid glare.

Energy and resources

- The store will be heated using a refrigeration waste heat recovery system. This form of renewable energy recovers waste heat from the food chilling equipment and is used to provide heating to the main sales area, warehouses and offices area. The use of this system will reduce carbon dioxide emissions by 26%.
- Temperature levels within the store will be controlled by a Building Management

System (BMS) and will vary throughout the store dependent on the use of each area.

- Low energy lighting design, using highly efficient LED luminaries, will be used throughout the store.
- All lighting within the store will be connected to the BMS. The lighting within the store's sales area will be controlled by timers which are set to the store's opening times, meaning that lighting is not left on unnecessarily. The sales area uses full lighting during trading hours and cuts back to one third lighting before and after trading hours to allow for the stocking of the store. Occupancy lighting controls will be used on non-retail areas.
- BMS and LUX sensors also power the external lighting. This means that lights are only on during dark hours and ensures that they do not remain on later than one hour after the store closes.
- Water consumption is carefully monitored and there are sensors on the taps in the toilets in order to minimise usage and prevent wastage.
- Lidl use '4 fan' chiller condenser units,

which are externally mounted. These units are designed to omit very little noise and to be as environmentally friendly as possible.

- The chilled food cabinets incorporate night blinds which trap the chilled air and reduce overnight energy consumption and Lidl refrigeration plant has low carbon emissions ratings and operates without the need for chlorofluorocarbons.
- Lidl undertake careful planning of store stock levels, stock availability and deliveries to the foodstore to ensure unnecessary journeys are avoided and the maximum amount of products possible are delivered per litre of fuel used. In addition, Lidl will optimise each delivery by returning all waste to the distribution depot on the lorry's return journey, and there are no separate waste collections made to their stores.
- Lidl install manually operated 'dock levelling' plates within new stores which are used in conjunction with a sloped loading ramp. These 'dock levelling' plates replace hydraulic scissor lifts, which were previously used by Lidl, and have significantly lower energy consumption.



7 Sustainability

- As noted in section 6.0, the proposals include two EVCPs. Passive infrastructure is also to be installed within the car park to allow increased EV provision to be provided in order to meet future demand.

Waste

- An Integrated Waste Management Policy - which complies with relevant legislation and minimises the impact of the foodstore on the environment – will be used with high packaging recoverability as a result of Lidl's efficient delivery and stocking system. Lidl will install waste handling equipment, such as plastic/cardboard baler, into the store.
- The store has been designed to accommodate a reverse vending scheme in line with and to comply with future Government legislation, whereby customers will be able to recycle waste packaging – including polyethylene terephthalate (PET) and High-Density polyethylene (HDPE) plastic bottles, steel and aluminium cans and glass bottles - in exchange for

rewards. This will most likely be in the form of vouchers that can be spent on products within the store.

7.4 As a result of all of the above, the building is likely to achieve a high standard of energy efficiency and perform well in excess of Part L minimum requirements.

7.5 Lidl seeks to be a zero waste to landfill business, which means that it reduces, reuses and recycles as much as possible. As part of this, all of Lidl's cardboard and clear plastic wrap is recycled, and this equates to approximately 115,000 tonnes every year. Additionally, Lidl now purchases 100% of its electricity from renewable sources.



Figure 7.1 CGI of Site

8 Assessment Against Policy

8.1 Based on national and local policy and guidance – as summarised in section 2.0 of this statement, the table below demonstrates how the proposal satisfies relevant design policy objectives.

Design Policy Objective	Response
<p>Development should be of a high standard of design and integrate with / enhance the character of the site and its wider surroundings.</p> <p>(NPPF para 130, National Design Guide 2021, KLP Policy LP24)</p>	<p>The proposed development scheme has been designed to respect its context both in terms of design and materiality. It is designed to a high standard, both in terms of aesthetic value and environmental sustainability. The architectural design is modern but utilises an appropriate colour palette and mix of materials that reflect the appearance of the traditional materials found in the wider area.</p> <p>The overall design has, as far as possible, been informed by and relates positively to the surrounding historic environment and is broadly in accordance with NPPF paragraphs 130 and 134, and Policy LP24. The development would have a neutral effect on the setting of the nearby listed buildings and Scheduled Monument.</p> <p>The provision of a landscaping scheme including new planting will help the development assimilate into its surroundings and screen the development from neighbouring residential properties.</p> <p>The proposed development’s height, scale, massing, layout, design and materials are all appropriate and the scheme will result in a high-quality development.</p> <p>In addition, the proposed scheme meets the requirements of Lidl in terms of operating and servicing arrangements, whilst maintaining efficient and safe vehicular and pedestrian access.</p>

<p>Provide appropriate and effective landscaping schemes as an integral part of new developments. Integrate proposals sensitively into the existing landscape and achieve a net gain in biodiversity where possible.</p> <p>(NPPF paras 130, 131 and 174, National Design Guide 2021, KLP Policy LP24)</p>	<p>As set out in this Statement, a landscaping scheme is proposed on-site which includes tree planting along the site boundaries.</p> <p>The proposed landscaping will, therefore, soften the impact of development and ensure it sits sensitively within its setting. The landscaped areas will be maintained and managed for the lifetime of the development.</p> <p>Hard landscaping (i.e. tarmac surfacing for vehicular access points and block paving at store entrances) will be used to differentiate between different parts of the site.</p> <p>The accompanying Ecological Impact Assessment demonstrates that the proposals have the potential to achieve a net gain in biodiversity, through providing enhanced opportunities for nesting birds and roosting bats, and landscape planting.</p>
<p>Maximise opportunities for walking, cycling and use of public transport through the location and design of development to improve environmental quality and encourage more active and healthy lifestyles. Ensure developments are accessible to all.</p> <p>(NPPF para 112 and para 130, National Design Guide 2021, KLP Policies LP20 and LP21)</p>	<p>The foodstore is situated in a sustainable location and will be accessible by a choice of means of transport. It will provide a car park – including disabled, parent & child spaces and electrical charging points, as well as Sheffield cycle stands. The development, both internally and externally, has been designed to aid access for all and promotes legibility in movement.</p> <p>The proposals are supported by a Transport Assessment and Travel Plan which demonstrate that the development can be safely accommodated within the highway network and is accessible by a range of modes of transport including public transport, walking and cycling.</p>
<p>Creation of a safe and attractive environment, at all times of the day and night, ensuring that streets, spaces and pedestrian routes are overlooked, with natural surveillance in order to minimise crime and the perception of crime.</p> <p>(NPPF para 130, National Design Guide 2021, KLP Policy LP24)</p>	<p>The layout of the proposed development scheme will create a safe and attractive environment. The store is orientated to allow natural surveillance of the proposed car and cycle parking areas. Clearly defined pedestrian and vehicular routes into, and around, the site are proposed, as well as CCTV and ANPR cameras in tandem with adequate external lighting.</p>
<p>Development proposals will need to demonstrate that careful consideration has been given to minimising CO2 emissions and the potential impacts of climate change. Energy efficient design and arrangements to minimise waste should be incorporated.</p> <p>(NPPF para 154, National Design Guide 2021, KLP Policies LP24, LP26 and LP52)</p>	<p>The proposed development has been designed to reduce associated impacts upon the environment through intelligent design, sustainable sourcing of materials, efficient use of energy resources and sustainable construction methods. Provision for cycle storage and rapid electric vehicle charging is also made.</p> <p>The scheme will also comply with the corporate environmental policies for Lidl, which ensure that all new stores achieve high standards of energy efficiency. This represents a conscious effort to reduce carbon emissions through responsible and considerate operating procedures.</p>



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