

JNB Project Code: LX26
Project Title: March Haigh Reservoir
Client: Canal & River Trust



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1 Introduction

The purpose of this document is to outline site access arrangements at March Haigh reservoir. The Construction Traffic Management Plan (CTMP) aims to:

- i) support any planned future maintenance and repair works to be completed, as well as any emergency works, on behalf of Canal & River Trust.
- ii) help stakeholders understand how JN Bentley will minimise the impact of their works on the surrounding community and local road network, both for activities on site, and transport arrangements for servicing the site.
- iii) enable third parties to understand the nature of the works, and the various activities associated with the project.

This plan is a dynamic document that details how JN Bentley will plan the works which impact the surrounding highway network only, with due regard to safety, health and environment matters. It is not a repository for generic risk assessments, records of how decisions were reached, or detailed method statements.

The CTMP will be reviewed monthly, to ensure it is still appropriate and being worked to.

1.1 Site Contacts

Site contacts are, including but not limited to:

- Contracts Manager- Adam Longrigg – 07733 125613
- Site Manager-Richard Timson – 07814 660901
- Safety, Health, and Environment Advisor Derick Kent – 07890 561914

1.2 Site Location

The site is located at March Haigh reservoir which is located approximately 3.5 km northwest of Marsden in the Metropolitan District of Kirklees, West Yorkshire. It was constructed in 1838 to provide improved water supplies to the heavily locked Huddersfield Narrow Canal, by impounding the Clough Haigh stream.

March Haigh reservoir has a maximum capacity of 240,070 m³ of water and is formed behind a 300m long, 21m high dam.

The A62 Manchester Road is the nearest main road to the site.

Grid Reference: SE 01731 13067

Nearest Postcode: HD7 6NR

What3Words: ruffling.victory.disengage

1.3 Scheme Overview

The scheme consists of the construction of a permanent access track, new drainage at the right hand mitre and replacement of the left mitre drain. The proposed new track has been designed by Arcadis and assumes the route of an old stone access track which has become overgrown. Passing places have been allowed for in the design, to allow for 6 tonne dumpers to pass one another. The track has been designed for 7.5 tonne vehicles during construction, but will need the installation of road plates in certain areas which will be confirmed prior to the beginning of construction works. Road plates will be installed to increase protection of the ground and the peat beneath the track during construction. Should

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roadplates be required they will be delivered on a flat bed vehicle & lifted into position using the 8T excavator using 4 leg chains for lifting purposes.

The access track route will only be accessible via Blake Lea Lane. The route of the old track is visible from aerial photographs.

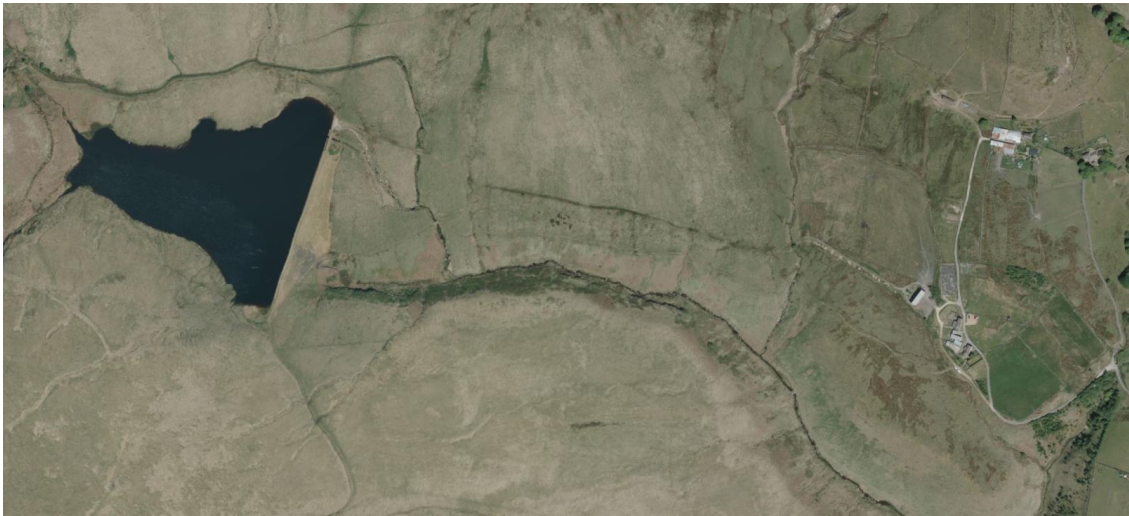


Figure 1 - Aerial View of Reservoir

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2 Delivery Programme and Traffic Risks

Due to the access restriction for large vehicular movements on Blake Lea Lane, an 'off-site' transfer compound outside of Kirklees at Brun Clough is proposed, which is detailed in Figure 2 below.

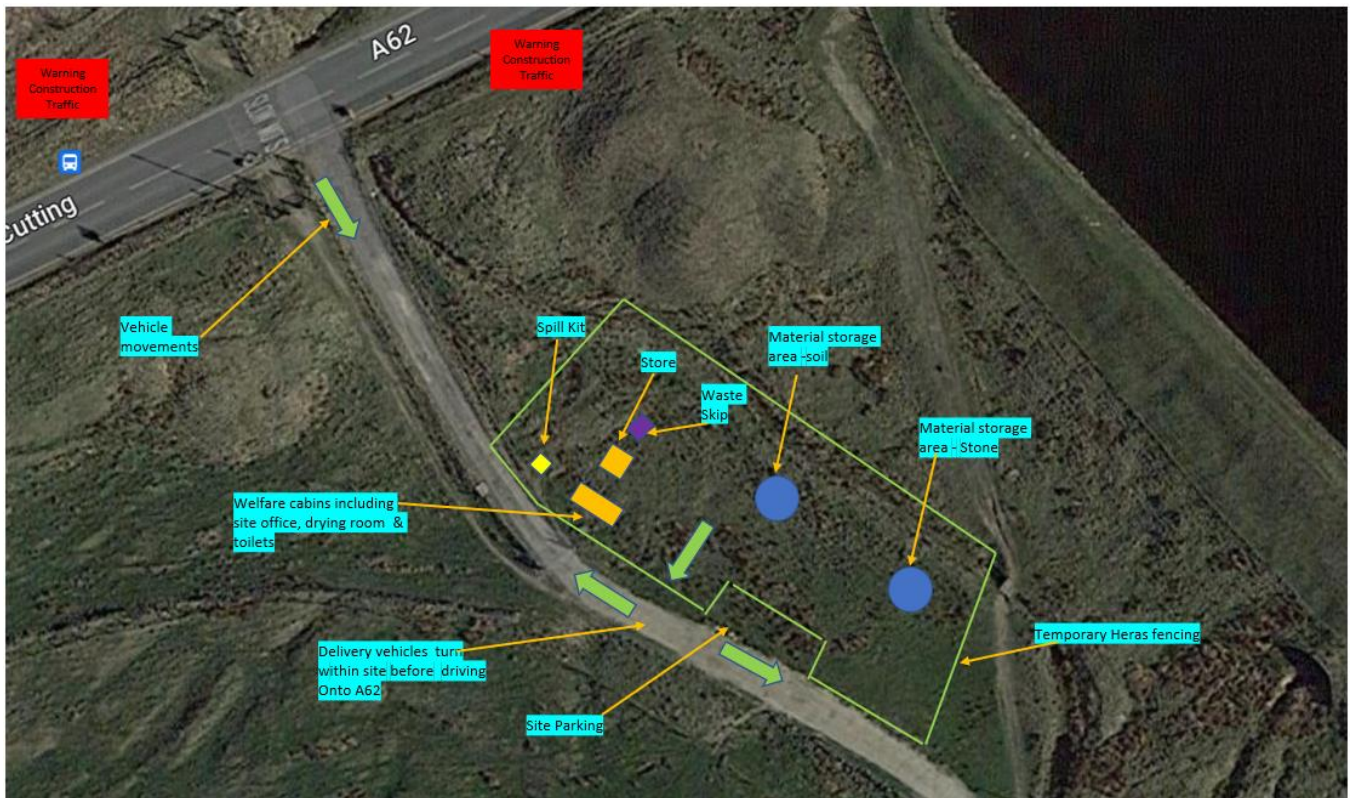


Figure 2 – Transfer compound at Brun Clough

It is proposed that stone will be delivered to the off-site compound using 20t road wagon loads where it will be transferred to tractors & trailers as this is deemed to be the most efficient & safest option to transport the material directly up Blake Lea Lane.

A second compound will be located at the top of Blake Lea Lane as shown in Figure 3. This will be used for material storage as well as housing welfare facilities for the site team. This will typically be in the form of welfare vans due to the restricted access to this compound location.

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Figure 3 – Aerial View of Location of Site Compound for Storage Top of Blake Lea Lane adjacent to new access track

It is estimated that there will be a requirement of 2380 tonnes of stone to be imported. Due to the steep and winding nature of Blake Lea Lane the trailers will be loaded with 14 tonnes max. of stone (22 tonnes is the maximum load of the trailer). The total transportation of stone to the site will equate to approximately 340 vehicles movements. It is anticipated that each trailer (carrying 14 tonnes of stone) will make 5 journeys each day transporting 70 tonnes of stone. There will approximately 15 deliveries each day, 8 of which will be in the morning and 7 in the afternoon equating to 210 tonnes of stone per day, using 3no tractors and trailers. All deliveries will take place during normal working hours using 3no. tractors and trailers.

As well as stone deliveries there will be a requirement to remove spoil from the site. As per the Arcadis technical note 10058105_ARC_GEN_ZZ_TN_CE_0001, this will be approximately 1400 cubic metres which equates to approximately 2520 tonnes. This will be back loaded onto the same vehicles to minimise vehicle movements.

Stone delivery to the site location and spoil removal is expected to take 26 working days, inclusive of a 4 days risk allowance, as set out below.

$$4900 \text{ tonnes (approx)} / (14 \text{ tonnes} \times 15 \text{ loads}) = \text{approx. } 24 \text{ days} + 4 \text{ days risk allowance} = \underline{28 \text{ days}}$$

The site location will require a degree of traffic management. We will require site operatives working as a 'banksperson' to manage the stone delivery wagons along & at the top & bottom of Blake Lea Lane.

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Each banksperson will be equipped with a radio for continuous communication so that there is no conflict between vehicles using Hey Green, Waters Road & Blake Lea Lane.

There will also be the requirement of plant and machinery to be delivered to the start of the access track to allow for its construction. This machinery will include excavators, rollers, dumpers, welfare units, fuel and water bowzers – delivery vehicles will be requested during plant hire to include rigid 20 tonne lorries, ensuring no low loaders or articulated vehicles.

- 8 tonne excavators are proposed for carrying out the stone track installation works at the reservoir.

Details of typical excavators and dumpers to be used can be found in **Appendix A**.

It is anticipated that the site team will comprise of the following:

- 5 x Operatives/General Foreman/Plant Operators, who will travel to and from site in a works van each.
- 1 x Site Supervisor, who will travel to and from site in a car.
- 1 x Site Manager, will travel to and from site in a car.

The site team will each complete one trip to and from site each working day; entering site at the start of the shift, and leaving site at the end of their shift.

- Until the site compound has been constructed, the site team will initially park at the top of Blake Lea Lane.
- Thereafter, the site team will park near the main track construction works.
- To keep daily vehicle numbers down, a minibus or welfare van may be used as an alternative method to travel to and from site.

3 Site Access Arrangements

Access to the site will be from the A62 Manchester Road and along Towngate to Church Lane. Bear left onto Station Road & turn left onto Reddisher Road joining Waters Road & finally joining the accent up Blake Lea Lane

3.1 Access Route Approach

The image below shows an example of the above access route which avoids using the majority of the A62 from the West with the tractors and trailers and uses the public highway network to approach Town Gate avoiding Marsden Town Centre. Clearly, vehicles will travel the same route in the opposite direction when existing the works.

CONSTRUCTION TRAFFIC MANAGEMENT PLAN

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Figure 4 – Route from transfer compound at Brun Clough

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3.2 Blake Lea Lane

Turning into Blake Lea Lane from Waters Road, the road narrows considerably; the entrance to Blake Lea Lane is approximately 3m wide.



Figure 5 - Entrance to Blake Lea Lane

Blake Lea Lane raises steeply and becomes narrow. There is only one location which is suitable for the passing of large vehicles along the 1300m road, at approximately 865m from Water Roads. The narrowest section of the road is 2.5m with stone walling on either side.



Figure 6 - Narrowest Section of Blake Lea Lane

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The proposed site route plan from Blake Lea Lake onwards is shown below:

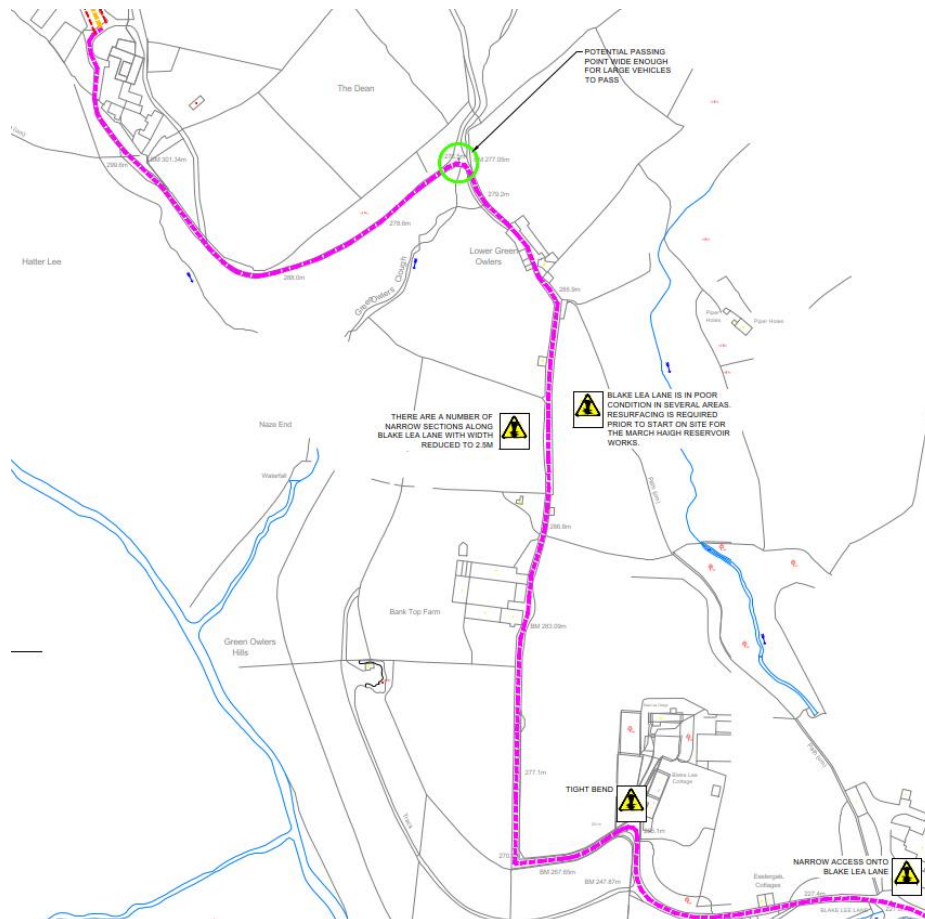


Figure7 - Overview of Proposed Site Route Plan

Planning of deliveries will be carried out to prevent large vehicles meeting each other along this road. All deliveries to site, including materials, equipment and plant will be directed to the site compound using a banksman, where required.

Due to the narrowness of Blake Lea Lane, it is proposed to use the junction at Waters Road and Blake Lea Lane for the potential passing of vehicles in opposite directions.

3.3 Site Working and Delivery Hours

Deliveries to site will occur between 07:30 and 17:00. However, to minimise congestion on the local highways network, JN Bentley will look to plan deliveries outside of peak hours - between 09:30 and 15:00, where possible.

Normal site operating hours will be Monday to Friday from 07:30 to 18:00. Although, in some circumstances, construction activity may stop earlier.

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4 Construction Traffic Generation

4.1 Construction Staff

To reduce travelling and exposure to the road network, JN Bentley will look to utilise local staff for the March Haigh Reservoir project, where possible.

Parking Arrangements

Parking arrangements will be made at the off-site and on-site compounds.

5 Control Measures

5.1 Construction Environmental Management Plan

A Construction Environmental Management Plan (CEMP) has been prepared, submitted and agreed with Kirklees Council. The CEMP includes measures that will maintain highway safety during the construction period. For example, surfacing all compounds with stone hardcore to ensure that there is little or no transfer of mud onto the highway network.

If the Site Manager deems necessary, a wheel wash system or road sweeping will be used during the construction of the new access track.

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5.2 Community Engagement

JN Bentley will communicate with the local community, residents, and businesses in advance of the works, to ensure minimal disruption.

A WhatsApp group will be set-up to give local residents any relevant information.

Additional letter drops will also be undertaken as necessary to keep the local community informed.

5.3 Monitoring

“The Construction Traffic Management Plan” will be reviewed and updated periodically during the delivery of the scheme.

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6 Risks and Constraints

The following risks and constraints have been identified:





- Due to the sharp inclines along the route, the trailers will not be loaded to the full 22 tonne capacity. This will increase the number of vehicle movements to and from the site compound but create a safer journey.
- A risk allowance of 4 days has been included. However, there could be potential for this to increase.
- It is anticipated that localised vegetation clearance will be required from the bottom of Blake Lea Lane, up to the proposed site compound area. There is the risk of the vegetation being privately owned, and the owners not supporting this decision.
- Risk of public road usage including vehicles, pedestrians, farmers, and the local community. All delivery vehicle movements will be banked accordingly.

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Appendices

Appendix A – Plant and Machinery

9 tonne Dumper	 Stage 5 Ops manual Dv60,90,100.pdf
6 tonne Dumper	 6t Swivel Dumper.pdf
8 tonne Tracked Excavator	 8t Tracked Excavator.pdf
Tractor & Trailer	 Lynch_Tractors-John Deere-6155M.pdf