

Construction Management Plan (CMP)

Project:

ERECTION OF 122 DWELLINGS, LANDSCAPING AND ASSOCIATED INFRASTRUCTURE

At: LAND AT, WHITECHAPEL ROAD, CLECKHEATON

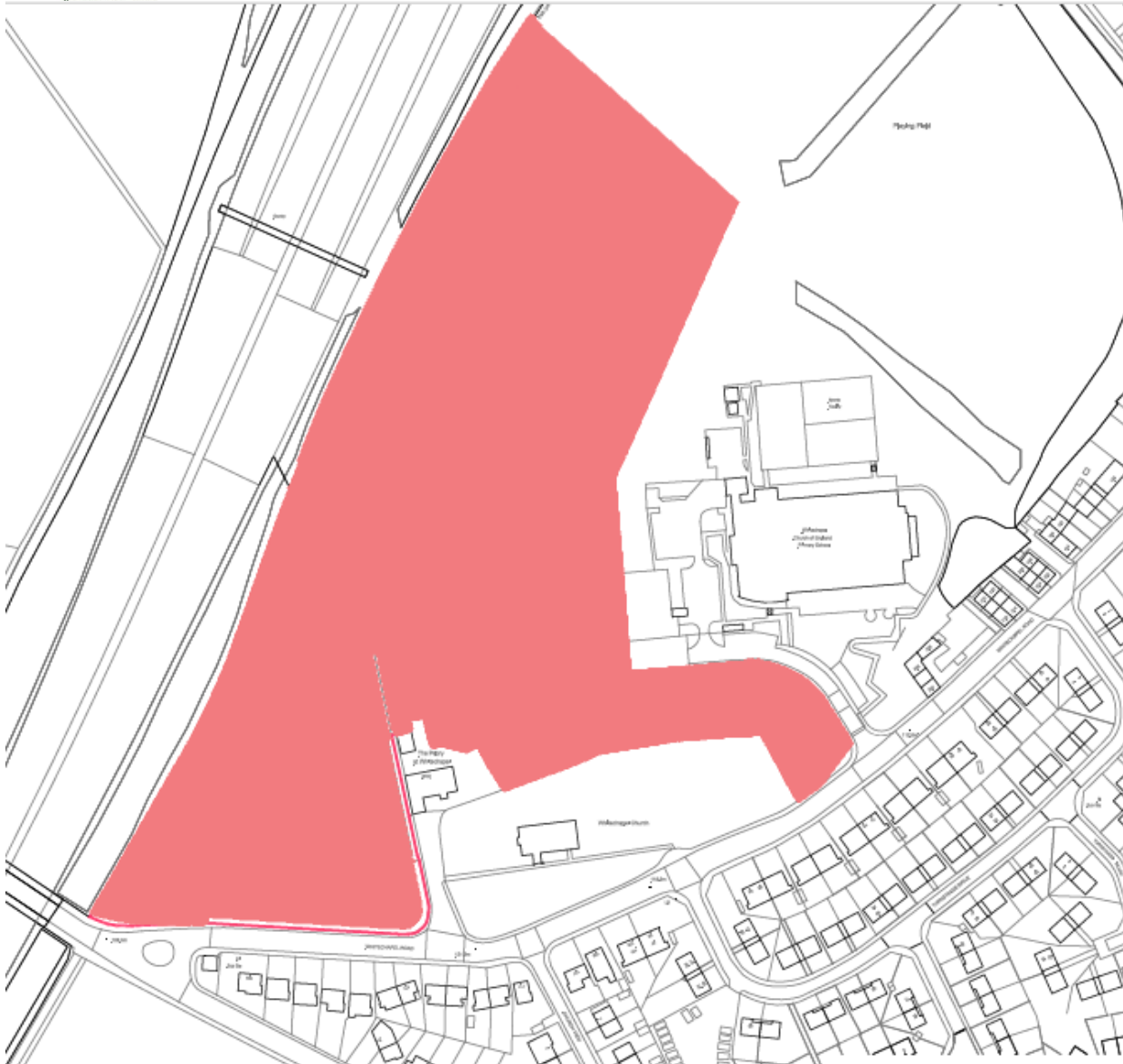
Report ref: H8040/CEMP/RP1

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Location Plan

Important Dates

Tree clearance works started 11/10/20

Road & Drainage start 15/11/21

First Superstructure Start 06/12/ 21

**Refer to BDW House Build Program dated 25/10/2021 in Appendix J. (This appendix will be updated as the build phase progresses)



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Relevant Planning Condition no 3: Application Number: 2019/62/93658/E

No development shall take place, until a Construction Management Plan (CMP) has been submitted to, and approved in writing by, the Local Planning Authority. The Construction Management Plan shall provide details of:

- timetable of all works; [See Appendix J](#)
- the construction access(es) and confirmation that adequate visibility splays shall be provided prior to the commencement of development including groundworks; [See Page 4 Appendix K](#)
- pre-development road condition survey of Whitechapel Road;
- vehicle sizes and routes, times of vehicle movements, identify the location of any HGV waiting areas and include details of the management of said areas; [See page 11](#)
- the parking of vehicles of site operatives and visitors; [See Appendix A & Page 6](#)
- signage;
- loading and unloading of plant and materials; [See Appendix A](#)
- storage of plant and materials used in constructing the development; [See Appendix A](#)
- the erection and maintenance of security hoarding including decorative displays and facilities for public viewing; [See Appendix A](#)
- measures to be taken to minimise the deposit of mud, grit and dirt on public highways by vehicles travelling to and from the site, including the provision of adequate wheel washing facilities within the site; [See Page 7](#)
- measures to control and monitor the emission of dust and dirt during construction; [see Page 11-15](#)
- a Site Waste Management Plan, detailing recycling/disposing of waste resulting from demolition and construction works; [See Page 7](#)
- mitigation of noise and vibration arising from all construction related activities to (these details should also include suitable restrictions on the hours of working on the site including times of deliveries); [See Page 8-10](#)
- artificial lighting used in connection with all construction related activities and security of the construction site; [See Page 16-19](#)
- site manager and resident liaison officer contact details (including their remit and responsibilities); and [See Page 22-26](#)
- details of engagement with local residents and occupants or their representatives. [See Page 22-26](#)

The development shall be carried out strictly in accordance with the approved CMP and no change there from shall take place without the prior written consent of the Local Planning Authority. Upon completion of the development, post-development road condition surveys and a schedule and timetable of remedial works shall be submitted to and approved in writing by the Local Planning Authority, and the approved remedial works shall be carried out following the completion of all construction works related to the development.

This pre-commencement condition is necessary to ensure measures to avoid obstruction to the wider highway network, to avoid increased risks to highway safety, and to prevent or minimize amenity impacts are devised and agreed at an appropriate stage of the development process.



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Construction Traffic Management Statement

The registered post code for the site is **BD19 6HR**

The main access of the site connects directly onto White Chapel Road, from the development.

The site entrance is wide and clearly visible on its approach. The entrance will have restricted parking within the turning head and fronting the development off Whitechapel Road.

The entrance of the development will be constructed with all tree works, retaining walls removed to establish unrestricted visibility splays for the main entrance road into the development. Appendix K shows the scheme to be proposed to restrict parking along the frontage with Whitechapel Road (WSP Drawing number 70055404 P05). The visibility splays and bell mouth have been constructed in accordance with this layout keeping the visibility splay free open and required for construction vehicle movements to safely manoeuvre off site

The site road network will be delivered in four phases shown on Phasing Plan in **Appendix L**

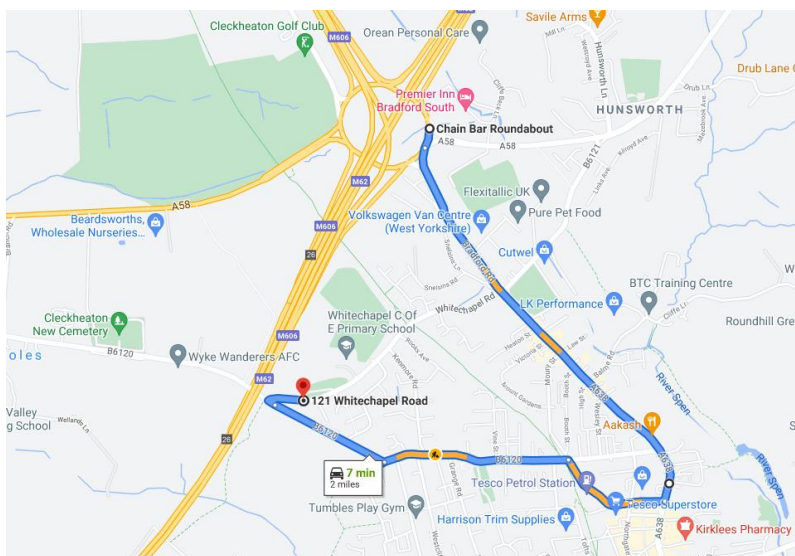
Construction traffic will be discouraged from using Whitechapel Road from the East (Bradford Road) due to the school location near and the restrictive nature during school pick up times with parked cars

The use of Banksmen may be required for safe access and egress from site where it is a requirement. Highway Code will apply to all traffic movements with strictly no reserving in or out of site. Construction traffic will be restricted to the approved routes below only:-

Preferred Construction Route from the M62 EAST & WEST ONLY

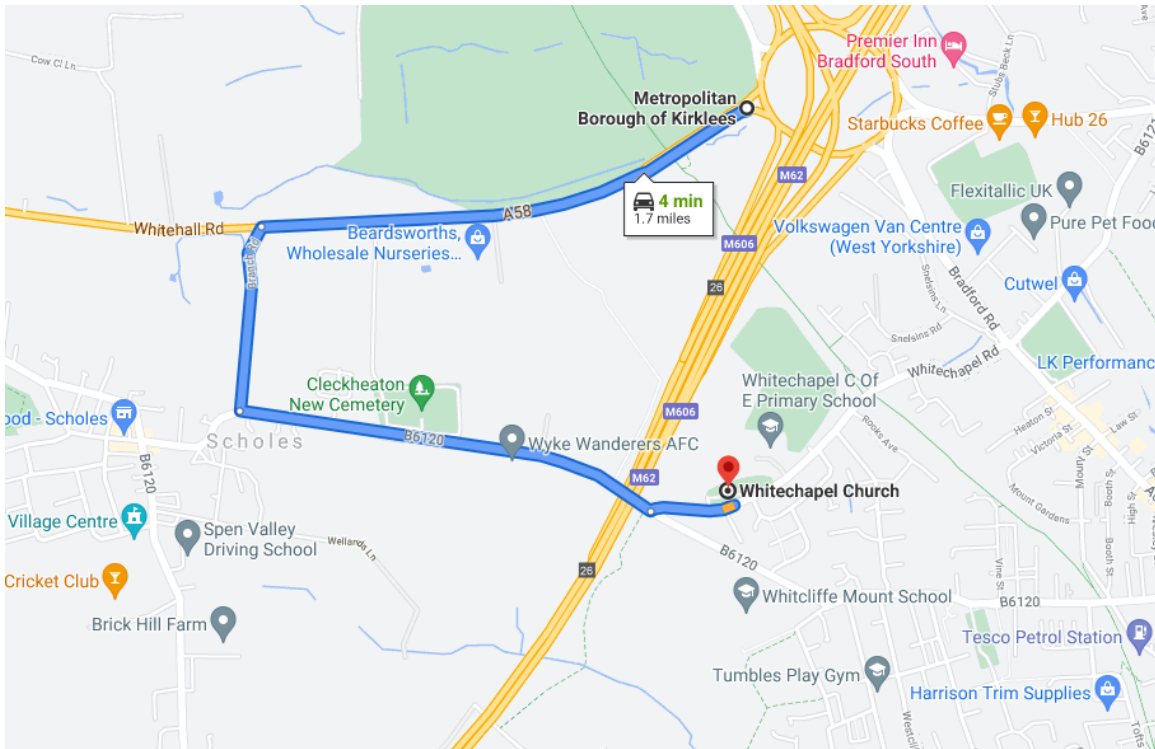
Two suitable routes have been identified

Preferred Construction traffic will be instructed from M62 Chain Bar round-a-bout onto Bradford Road (A638) until Horncastle Street. Take Serpentine Road then left on Whitcliffe road, right onto Turnsteads Ave then right into Whitechapel Road then arrival into the designated site entrance. Refer to blue line below

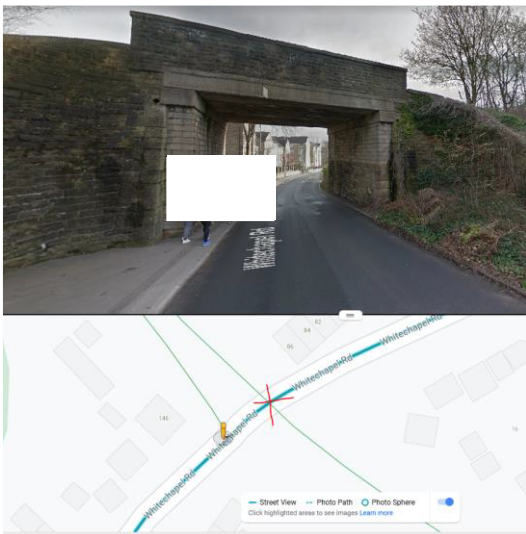


Alternative Route to avoid Cleakheaton

M62 Chain Bar round-a-bout onto A58 , turning Right onto Branch Road towards Scholes , then right onto Whitechapel Road over the motorway bridge then then again into site – see blue route below :-



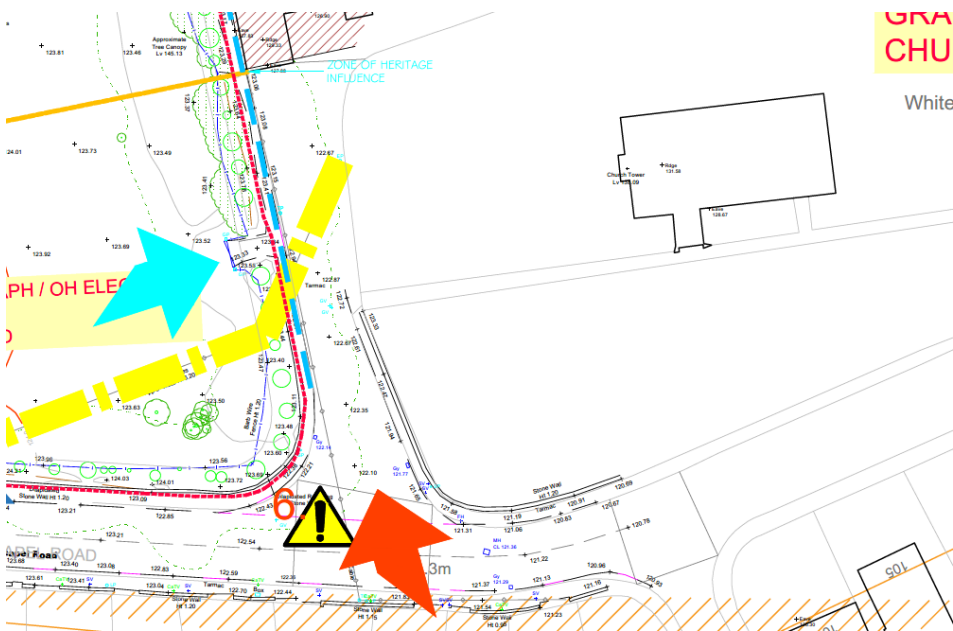
Plases note the Old Railway Bridge mid-way along Whitechapel Road does not have a head restriction plaque on it so its height is unknown to high sided vehicles, Whitechapel Road is therefore deemed unsuitable as a route for Construction Traffic – image below of Railways Bridge and its location.



It has been necessary to erect additional signs on site to ensure and remind that all construction vehicles leaving site will need to turn right and not left onto White chapel Road. These will be placed at the entrance of the specific construction phases being worked on and at the entrance with White Chapel Road

Contractor & Visitor Parking

There will be **STRICTLY NO PERMANENT** contractor parking on any part of White Chapel Road, the layby entrance to the Priory & Whitechapel C OF E Church shown below, Turnstead Drive and Church View.



There will be strictly no parking in the vicinity of White Chapel Road Primary School or any areas fronting the school or within the school grounds.

All contractor and visitors vehicles to site must be contained on site in the allocated purpose made car park at the approved compound location on the development side. Prospective customers to park in the Sales Office designated visitors Car Park.

Delivery times will be to between 8.00am to 6pm Monday to Friday and 9.00am to 1pm on Saturdays. Wherever possible, deliveries will try to avoid school pick up times between 8.30-9.30am & 3.10 - 4.30 pm

The setting down of all materials and deliveries will take place at the compound locations shown blue on the Site execution plan number labelled Compound as construction progresses. Storage areas may be required within site where site condition and constraints dictate unforeseen circumstances.

Barratt David Wilson Homes have secured an overflow car park for contractor vehicles at the Priory pub. This is a contractual agreement based on a monthly rental figure until we finish the development. This is to ensure that during busy or large infrastructure works e.g. Sewer Works especially that all additional traffic can be accommodate on or near site without pressure to the local area networks.

Waste on Site statement

All waste must be segregated. Each site must have a specific waste area to locate site skips and must be signed appropriately using signs

Any inert waste (bricks, blocks, concrete, roof tiles, etc.) must be either re-used on site as fill, or removed from site using large tipper Lorries. In order to achieve maximum efficiency, stockpiling of inert waste on site may be necessary. Inert waste should not be removed from site in skips. Every effort should be made to minimize waste generated on site. Contracts Managers will review site waste cost performance with the Site Manager each month.

For more detailed procedures refer to BSG 29 Documents in Appendix D

Managing Mud and Debris Statement

The access of the site connects directly from White Chapel Road. Therefore cleanliness of vehicles and plant travelling between the site and the main road is an important consideration. This means that action needs to be taken to prevent mud or other debris from leaving the construction site and getting onto the highway.

In this instance several measures are being implemented by the developer.

Firstly road signage erected for the duration of the construction period will be placed either side of the site entrance as shown on the Construction Plan. This signage is to remind operatives that vehicle must be washed down prior to leaving site.

Power washing areas will be installed behind the main compound, alongside the delivery access road so that any vehicles leaving the site that require it can clean their tires and undercarriage to wash off mud and debris before they exit. Similarly a power washing area will be installed on the main site road near the site entrance so that other construction vehicles exiting can also be cleaned down before entering the public highway. The locations for both facilities will be shown on the traffic management plan. All vehicles will be inspected prior to leaving site to ensure **NO** mud will be placed on the carriage way. The power washing areas are shown as “W” on the extract plan. (Section 3a on Page 14 below)

Lastly a road sweeper vehicle will be employed to visit site and clean the site roads each day to prevent a build-up of mud, grit and dirt. This will take place every afternoon prior to the close of site and will also be on call as necessary through each day should there be a need for its services.

Reducing Noise and Vibration during the construction phase.

Noisy work is one of the main sources of nuisance from a construction site. The control of noise from this Development will be carried out to mitigate the effects to employees and contractors working onsite and the local residents.

The method of undertaking the work will be completed using the quietest method possible. Wherever practicable all activities will be restricted to below 85dBls

This will be achieved on site by following the HSE guidance L108 on controlling noise at work and by complying with the Control of Noise at Work Regulations 2005 (S.I.2005/1643). This will be to protect the work force on site whilst working on this site

Barratt David Homes will assess noise to the local areas through the recommendations set down in BS5228-1:2009+A1:2014 Code of practice for noise and open sites. Where requested by the EHO we will (given achievable timescales) undertake instrumental" single sample "measuring in accordance with Appendix G of this code of practice to ensure that the noise emitted from site is controlled

We will also employ SLR consultants to advice and report on the position of the compounds and assess the noise from our activities and calculate the likely recommendations to reduce the noise from source. This report will be commissioned once this principle is agreed with KCC

The noisy element of most construction projects are substructure and superstructure. The methods of Construction will consider the creation of noise and, where reasonable practicable, the method used will be the quietest solutions possible taking into account economic and environmental constraints.

BDW Site Manager will monitor noise and vibration in accordance with BSG 05 & 06.

Regular on site monitoring to be conducted and reviewed to the individual tasks where noise is likely to be envisaged or where more detailed noise monitoring is requested by the LA.

With regards to Hand arm vibration – BDW have a robust hand arm monitoring procedure in place as detailed in BGS13 detailed in Appendix G.

The following actions will be taken to reduce the noise produced on site but it is important to note that noise will managed, with tasks reviewed, prevented where ever possible but mitigated at source first off.

- BDW will insist that all vehicles used on site are not less than 5 years old with all machines will be regularly maintained in accordance with manufacturer's instructions to ensure they operate at their lowest noise levels. Vehicles to be fitted with appropriate silencers or noise baffles as approved by the machinery manufacturers. Vehicle Maintenance logs are to be provided for inspection.



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- All generators to be turned off outside of the approved working hours. Wherever practicable generators, large drills and breakers will be situated facing away from noise sensitive areas and for generators preferably within the earmarked fenced construction compound area.
- All vehicles and mechanical plant used for the purpose of the works shall be fitted with effective exhaust silencers and shall be maintained in good and efficient working order.
- All compressors shall be “sound reduced” models fitted with properly lined and sealed acoustic covers which shall be kept closed whenever the machines are in use and all ancillary pneumatic percussive tools shall be fitted with mufflers or silencers of the type recommended by the
- Manufacturers. Special attention should be given to the use and maintenance of sound reduction equipment fitted to power tools and machines.
- Machines in intermittent use shall be shut down in the intervening period between works
- The unloading and loading of equipment and materials shall be limited to the hours set out in the Hours of Work section of this CEMP on page 20
- Preferably Materials to be placed on the ground rather than dropped. If needed, drop heights to be minimised. Ovoid double handling or multiple movement of materials around the site to help reduce heavy plant vehicle movements
- Electrically powered equipment should be used in preference to that powered by other sources where reasonably possible.
- The use of any air compressor, pneumatic drill or percussive equipment shall be limited to the hours set out in the Hours of Work section of this CEMP on Page 20
- Noise Sensitive areas e.g. Public areas or areas where wildlife is present & occupied plots will be respected and evaluated in accordance with the construction phasing of the development and assessed to ensure noise is kept to below the required levels.
- The location of the main compound is located 165M away from the nearest properties off Whitechapel Road and 170m from White Chapel Road Primary School. Both distances giving significant protection against noise Machines starting up behind protective compound fencing
- Use enclosed rubble chutes with linings to use for wastes off and cover skips to prevent dust and material being thrown in air
- Surface water management bunds can also be constructed as noise barriers to be planted with vegetation to reduce noise against sensitive boundaries.



- Install noise protection zones within construction areas where necessary. Ear protection use as mandatory with appropriate signage placed to instruct this throughout site
- All visitors to site to be provided with Ear Protection and signage erected to indicate relevant noise protection zones
- Machinery should be started within the compound fenced area and should all be ideally started at the same time. The compound is to be sited away from noise sensitive area. The Compound is set central to the development footprint giving a good distance from any habitable dwelling house.
- Revving of engines will be prohibited anywhere on site. Alternative to reversing beacons to be provided by contractors
- Vehicle movement are to be routed away from noise sensitive area on haul roads which are to be well maintained and material used to soften reverberation from construction vehicles. All vehicles speeds to be set at 10mph or below
- No Workmen radios or music allowed on the development.
- There are certain properties on site which will be vibro-piled foundations shown as blue dots below, these are located in the area against a boundary which does not have any residential properties abutting it but Whitechapel Primary School is against the Eastern Boundary where these plots will be piled . The type of piling will be chosen to minimise noise pollution. The Location of these plots are over 165m away from the nearest properties off Whitechapel Road & 165m from the school building & 180m away from the school playing fields so “close vibration and noise problems are not expected to be a significant problem.



- The piling operative will inform the Local Authority and Local residents affected within 48 hours in advance of this activity commencing. It will be recorded on the site information boards when the date piling is to



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commence, total duration of the piling operation. Where possible works will be undertaken under one rig visit to minimise piling rigs visits to site. Close consultation is to be undertaken with the engineers to review type of piles being driven and with the LA to set an acceptable noise target level and agreed the period of piling activity e.g. times during the day. We will review the piling engineering solution prior to any plots being piled including rig selection, First Preference is for a hydraulic rig to be used. Noise levels will be monitored during all piling operations and an Acoustic shroud will be placed around the rig in line with contractor's recommendations to mitigate any noise from the pile head striking. It is anticipated that any piling will take 1 week in duration to execute.

- Coordination of works to be encouraged between subcontractors as not to prolong similar noisy activities on site to go on at different times.

Typical Heavy Plant and Vehicles Movements

Plant on site;

- 2 excavators
- Hydromek 310
- Kobelco 210
- CATD6 XE
- Volvo Truck A30F
- CAT truck 730
- CAT 213 roller
- 9 tonne dumper
- 3 x small water bowser

Typical Movements per day:

- **Estimated between 7 to 16 heavy wagons per day.**
- **Estimated the total number of wagon loads to be taken off site when removing soils only is approx. 50 to 60 loads**
- **Estimated 8 or 9 movements per day for staff and auxiliary vehicles**
- **Time of vehicles movement during site operation times only as detailed in "Hours of Work " Page 18**
- **7 vehicles max will be operated for remediation**

Managing Dust during the Construction Phase

The site management team will be responsible for dust suppression on the site.

Roles and responsibilities are clearly defined within the Barratt Group Occupational Safety, Health & Environmental Management System. A copy of the roles and responsibilities of the Site Manager accompany this statement. Also accompanying this report is a brief Environmental Policy Statement outlining the group's vision on environmental issues, found at Appendix H.



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All our Site Managers attend the SMSTS training course on site safety. This course concentrates on Health and Safety which includes the health and safety of the workforce. Part of this is protecting the workforce against environmental impacts, one of which is fugitive dust.

Dust emissions during the construction of buildings or roads are associated with land clearing, drilling and blasting, ground excavation, and cut and fill operations (i.e., earth moving). Dust emissions can vary substantially from day to day, depending on the level of activity, the specific operations, and the prevailing meteorological conditions. A significant amount of the dust emissions result from construction vehicle traffic over temporary roads at construction sites. Dust emissions from residential construction are a function of the total acres of land disturbed and the volume of soil excavated. The volume of soil excavated also varies by type of structure under construction.

Monitoring of dust will be in accordance with guideline's (e.g. Institute of Air Quality Management dated January 2014). If dust appears from unidentified sources, the site team will suppress the dust where possible. Safety, Health & Environmental Briefings (SHEB's) will be provided to site operatives at least monthly and should reflect the actual work being undertaken on site. Records must be maintained of the briefings. All operatives/visitors on site receive a Barratt Group Induction prior to commencing work on site. Environmental Incidents and complaints should be recorded in the incident book on site and records forwarded to the divisional office and Group SHE department as required.

The Barratt Group Standards 05 & 06 (appendix E & F) explain what Monitoring, Reporting and Auditing of Environmental matters and the standards that are required.

The project is located to the north east of Dewsbury town centre on former green field land. The site is surrounded by the following features/land uses –

- North – Open Space / Amenity space
- East – White Chapel Primary School
- South – Residential White Chapel Road
- South – East – Church & Grounds
- West – M62 Motor Way

These main receptors lie within approximately 50-250m of the site boundary. From a south westerly prevailing wind directions the site boundary links with the Church and residential housing on White Chapel Road.

We have identified that generation of dust will be as a result of the following main construction activities and from tracking in and out of the construction entry point off Owl Lane.

These activities has been assessed as mostly **Medium to periods of High Risk in** accordance with the size, activities and anticipated vehicle movements during the construction phase for this development. The reason for this classification is the location of the M62 Motorway in the West and White Chapel Primary School in the East

- Vegetation Clearance;
- Topsoil removal including contaminated topsoil



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- Cut and fill to required levels
- Stockpile of surplus topsoil (in phase 2 area)
- Trim and compact ground formation
- Formation of haul road
- Foundations excavation and drainage trenches
- Top soil grading
- Cutting Kerbs cutting materials , small duration for power tools use for coring ducting
- General site traffic movements (tracking)
- Deep Sewer construction Diversion works over 10m deep closet to Motorway
- Service disconnections
- Significant Retaining wall construction , Eastern Boundary

Dust Management Mitigation (In line with section 8.2 Air quality Management)

1. Communication

BDW Site Manager and Site Team contacts with will be displayed on the display board found in the Local Engagement Section . These are placed in strategic locations around the development boundary and will be communicative to the local community and will include HQ contact numbers and points of contact.

2. Site Management & Monitoring

Barratt Homes Standards BSG06 details that a log book will be maintained by the Site Manager. The site manager will Record all dust and air quality complaints, identify causes ad take appropriate measures to reduce emissions in a timely manner, and record the measures taken. The log book will be made available to EHO at any time giving 24 hour notice.

Daily visual monitoring will take place, this will include haul road surfaces and stockpile conditions.

The daily inspection are to note weather and wind direction to assess types of activity undertaking are appropriate or can be re-planned to reduce the risk from dust accumulation.

Weekly inspection of areas outside of the site boundary to ensure dust mitigation measures are working and not impacting on local houses, cars and amenity areas.

3. Preparing , maintaining the site

- a) Areas have been identified for stockpiling of any stripped usable soils.
These are located behind the Tree belt on site to the Motorway embankment and School Boundary. The stock pile are to be sited with care so that there is no dust migration onto the motorway, school fields or affecting the frontage properties of White Chapel Road.

This is shown on the layout extract below in yellow shading:



Stockpiles to be managed so that they are not overburdened to encourage reduced drop zones from tipping buckets, therefore reducing the whipping up of particles. If stockpiles are to be stood for a while without being reused then they will be seeded or suitably vegetated to stabilise them. Temporary stockpiling is to be avoided or materials removed from site.

- b) Site mixing or mortar is not allowed. All mixed mortars will be delivered in silos and or tubs. This is to stop on site mixing and bagged mortar, waste and large sand stock piles.
- c) Any sand required to be stored on site will be in covered bunkers covered to prevent drying out.
- d) The attenuation fence will be constructed early in the construction phase. This will be placed as shown on the Planning Layout and will ask the primary dust protection fence.to prevent “dust blooming over the motorway, causing significant hazards for drivers.
- e) Fence – wind breakers will be formed around earth stock piles are to be located against Western and Eastern Boundary. These wind breakers to be netted to protect the hedgerow, on the Eastern Boundary and school playing fields
- f) No over stripping of the site to be undertaken. Only areas to be stripped in correlation with the build phase
- g) Use enclosed rubble chutes and cover skips to prevent dust and material being thrown in air



h) Inspect scaffolding fencing and barriers , wash down with wet methods to remove any dust accumulations

4. **Operating vehicle/machinery and sustainable travel**

- a) Haul roads are to be constructed suitable for the types of traffic using them. These should be kept in a well compacted & good condition with materials used to prevent the kicking up of dust e.g. crushed limestone on ground mats or ground stabilizing membranes They should be of suitable depth to ensure no tracks are stripped back to bare earth. Repairs to all worn out tracks is to be undertaken as soon as practically possible, certainly within a 24 hours timescale.
- b) Haul Roads length are to be kept to short travel distances and until the main road infrastructure is in place , then all construction traffic to be transferred to surfaced roads.
- c) All on site plant vehicles are to be regularly washed down to remove accumulation of mud and dust debris
- d) Washing down facilities are to be provided via hose at both the compound location and site entrance. These are shown with a red W on the extract plan above at 3a.. The washing down lay-by near to site entrance is to have a rumple grid to dislodge accumulated debris.
- e) Signage to be provided to remind operates that vehicles are to be washed down on exiting the site. A washing down layby off the main haul road is to be located min 10m from the site exit onto White Chapel Road
- f) 10mph on site speed restrictions will apply on all unsurfaced haul roads policed by site team and signage. 15mph speed limit applied on all surfaced roads.
- g) Tipper trucks to be covered when entering to and from site and whilst driving on site
- h) Dry sweeping of large sections of road will be prohibited instead a water-assisted dust sweeper will operate on the access and local roads, to remove, as necessary material tracked out of the site. This may require the sweeper being in use during wet and dry periods continually. Water to be used from a bowser filled from collected rainwater from compound cabins to protect local water supplies
- i) Vehicle operators to be encouraged to switch engines off when waiting.
- j) All waste management skips to be covered when not in use to prevent material escapes across site

5. **Site specific Operations**

This is not an exhaustive list but identified tasks undertaken on a typical BDW development which may generate the most dust episodes

Task	Eliminate or limit the dust by:	Control the dust by using:
Cutting concrete kerbs, blocks and paving with a cut-off saw	Limiting the number of cuts during design/layout Using lower energy equipment like block splitters Getting material cut off site and delivered	Water suppression and RPE* with an APF of 20



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Chasing concrete and raking mortar	Limiting the need for chasing at the design/layout stage Using a work method that limits/does not need chasing, like over-covering cables	On-tool extraction using an H or M Class extraction unit and RPE* with an APF of 20 – consider powered RPE for longer duration work
Cutting roofing tiles with a cut-off saw	Hand cutting natural/fibre cement slates and other tiles where possible .. Using 1/2 and 1 1/2 tiles .. Correct setting out/design .. Minimising valleys/using dry valleys	Water suppression and A dedicated cutting area with scaffold board protection and RPE* with an APF of 20
Short-duration drilling totalling 15–30 minutes with hand-held rotary power tools	Limiting the number of holes during design/planning Using direct fastening or screws	Where possible use equipment that stops dust getting into the air. The larger the holes the better this needs to be. Options range from: – drilling through a dust ‘collector’ or using cordless extraction attached to the drill (for smaller drill bits) or – on-tool extraction using an H or M Class extraction unit .. Otherwise use RPE* with an APF of 20
Using a hand-held breaker in enclosed spaces with limited ventilation	Limiting the amount of breaking during design/planning stage ..Bursting, crushing, cutting, sawing or other techniques ..	On-tool extraction using an H or M Class extraction unit and RPE* with an APF of 20
Wet coring	Limiting the number of holes during design/planning	Water suppression Long periods of wet coring in enclosed spaces will also need RPE.* Use an APF of 20
Dry coring	Limiting the number of holes during design/planning	On-tool extraction using an H or M Class extraction unit Longer duration work (ie over 15–30 minutes accumulated time over the day) will also need RPE.* Use an APF of 20

This information will be made available to the public upon request to the Barratt homes head office and also within the site manager’s compound on site throughout the duration of the development

Artificial Lighting

The primary objective of lighting during the construction phase is to provide illumination for construction activities to provide

- A safe working environment, meeting statutory requirements and standards;
- site security lighting; and
- Mitigate the impact of artificial lighting on the surrounding environment as far as reasonably practicable.
- Provide safe assistance to emergency vehicles

Due to the dynamic nature of a construction site, there will be the need for different levels of illumination needed for certain tasks or stages in the construction process in order to provide a safe working environment. Some areas will require suitable task lighting while other areas will require a level of ambient lighting.

HSG38 Guidance will be used to plan, review and inspect lighting is fit for purpose and does not cause concern. All lighting installed by a qualified electrician on the advice of a lighting specialist



Task lighting – Task lighting will typically be provided for construction Activities and the required levels will vary depending upon the type of activity being undertaken. For example, clearance, excavation and loading typically requires an average of 20 lux, appropriate standard shall be used. Task lighting will also be required at the main entry points to allow the inspection of vehicles entering and exiting the site.

Task lighting – *In general, task lighting will only be used during specific times at specific locations and will have manual switching.*

Ambient lighting – Ambient lighting will be constant and typically be Provided to aid the safe navigation for areas such as access roads, footpaths, car parks contractors' compounds and accommodation areas. Typical levels will be an average of 5 to 30 lux, depending on the area to be lit.

Ambient lighting will be permanent for the during of the construction phase and will be required to operate dusk to dawn, so the most suitable method of control will be via a photo electric control cell which will automatically turn the lighting on at dusk and off again at dawn, when natural lighting levels have fallen or reached pre-determined levels.

The following mitigation measures will be adopted for both fixed and temporary lighting.

- Adopt the lowest safe lighting levels possible for task being undertaken.
- Limit the hours of lighting where practicable.
- Use a high quality luminaire with good optical control.
- Use the lowest possible mounting for the luminaire based on the required level of illumination needed for the task being undertaken.
- Direct luminaires into the area to be lit (light from the boundary inwards).
- Ensure the luminaire is mounted at zero degrees to the horizontal and avoid any tilt.
- If required make use of manufacture supplied custom shields.
- Provide local control for the lighting so it may be switched off when not required.
- Keep mounting heights lower than fences and bunding, where possible.
- Position equipment so it is not visible to sensitive receptors by using natural *screening trees and hedgerows.*

Appendix B show the compound site set up is a combination of one and two stores temporary building of varying heights and materials. To help mitigate the impact that these may have on the night sky the following measures will be put in place.

- i. Site buildings – Any lighting attached to site buildings should be fully cut off (emitting no light above the horizontal).
- ii. Site buildings – Where reasonably practicable avoid light coloured paint finish to reduce reflectivity



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Appendix B shows the location of the lighting fixed within the compound areas. Shown as symbols



On drawing numbers DB-SD13-008 & 009



Car park.	To compound areas	Permanent ambient lighting will be required in this area.
Fences to compound area and protective walkways	Illumination of permanent security fences, allowing detection of perimeter activity.	Permanent ambient lighting will be required in these areas to specific security levels with a high uniformity.

The following types of lighting are example which will be fixed within the compound building and covid safety hub all of which will be of the ambient type as explained above

	LED flat glass full cut-off area luminaire. For column or building / surface mounting.
	LED flat glass full cut-off area luminaire with hood. For column or building / surface mounting.
	LED flat glass full cut-off luminaire with good optical control. For building / surface mounting.

Permanent lighting will ONLY be placed on compound fencing and within contractor’s car park areas and along temporary vehicle routing. These will be placed with shields at centres to provide the safe vehicle movement

Example of the lighting stations to car parks, walkway routes etc.

<p>Permanent Lighting Columns</p>	<p>A temporary-only lighting design was ruled out as it is recognised that certain equipment will be permanently stored in the compound and car park areas and therefore the design needed to deliver sufficient lighting for safe and secure operations on an ongoing basis.</p> <p>As such, a laydown design using a combination of temporary and permanent columns was considered. The proposed concept was to create a basic arrangement of permanent columns delivering a minimum required lighting level for safety and security.</p> <p>Supplementary, temporary columns would then be erected when additional task lighting was required. This solution could be delivered using temporary luminaires supplied with dedicated generators or temporary columns contacted to a permanent infrastructure.</p>	 
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These are examples only and may change subject to the selection of the Electrical subcontractor but will be provided to avoid unnecessary light spill to the environment

Task Lighting will be in main provided within houses being worked on and primarily on the main sewer diversion.

This will involve deep excavations so it is planned that permanent lighting will be needed in this area should works over run after sun set. This route is well away from the residential properties ON the boundary of the site.

It has been programmed that majority of the works required are undertaken within daylight hours in line with the agreed construction timescales, so that artificial lighting is not the first choice of lighting

Hours of work



Note regarding construction activities:

Noisy construction related activities should not take place outside the hours of:

07.30 to 18.30 hours Mondays to Fridays

08.00 to 13.00hours , Saturdays

With no noisy activities on Sundays or Public Holidays

DELIVERIES

Delivery times will be to between 8.00am to 6.00pm Monday to Friday and 9.00am to 1.00pm on Saturdays. Wherever possible, deliveries will try to avoid school pick up times between 8.30-9.30am & 3.10 - 4.30 pm

THIS TIMES WILL BE STRICTLY ENFORCED:-

- A. NO HEAVY PLANT OR GENERATORS TO BE STARTED ON SITE BY ANY CONTRACTOR STRICTLY BEFORE 7.30AM ON A WEEKDAY AND 8.00AM ON A SATURDAY.
- B. NO PLANT TO BE LEFT RUNNING STATIONARY
- C. NO HEAVY PLANT WORK ON SUNDAY OR ON BANK HOLIDAYS

Note: Consent for work outside these hours may be given after any necessary consultation, giving 14 days' notice is required from the contractor to the L.A

The noise levels scheduled below for periods outside the normal working hours will only be permitted when consent has been given to exceptional working by the local authority.



Road Condition Surveys

Barratt Homes will commission a competent transport and road consultants to undertake a road dilapidation / condition surveys at the following stages

- ❖ Pre-commencement of works to roads , superstructure and tank construction
- ❖ Post construction, after the completion and occupation of last plot and on completion of all public open spaces.



The conditions survey will take place along the red routes highlighted on the plan above as sent by Optima ,



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Local Engagement

Prior to any superstructure start a mail drop will be sent to the following properties facing the Development on Whitechapel Road, Numbers 111 -133 Whitechapel Road , The Priory , Whitechapel C of E Church and White Chapel Primary School

Our ref: H8040/6.1



12th November 2021

Dear Resident,

Re: NEW RESIDENTIAL DEVELOPMENT AT LAND WHITECHAPEL ROAD, CLECKHEATON – CONSTRUCTION ENVIRONMENTAL MANAGEMENT PLAN

As you will be aware land at Whitechapel Road recently obtained full planning permission for residential development.

We are writing to you to provide you with some information associated with the construction of the development. Full details of which are provided within the Construction Environmental Management Plan (CEMP) Report ref H6590 / CEMP/RP1 which has been submitted to Kirklees Council.

From Monday 15th November 2021, we will be using Whitechapel Road to access the site.

Construction vehicles will be requested to access Whitechapel Road from the West at the Junction with Turnsteads Avenue, turning left into the development at the main access point. A map of the proposed routes and the access point to this development is shown with this letter.

We would very much appreciate if you could keep the roads to the proposed entrance free of vehicles. We will ensure that the road is swept daily, and we will set up a wheel wash facility at the construction entrance.

Noisy construction related activities should not take place outside of the hours of 7.30am and 6pm on Mondays to Friday and 8am to 1pm on Saturdays. No noisy activities should take place on Sundays and Public Holidays. Wherever possible deliveries will try to avoid school pick up times.

We will place two information boards outside our Sales Offices for your information.

These boards will show the following information relevant to the operations being carried out across that particular week or month: -

- Current Phase of Work – Detailing the current work being undertaken e.g.. Ground working, Roads and Sewers, or Superstructure works.
- Site Management Team contact numbers
- Out Of Hours Contact numbers

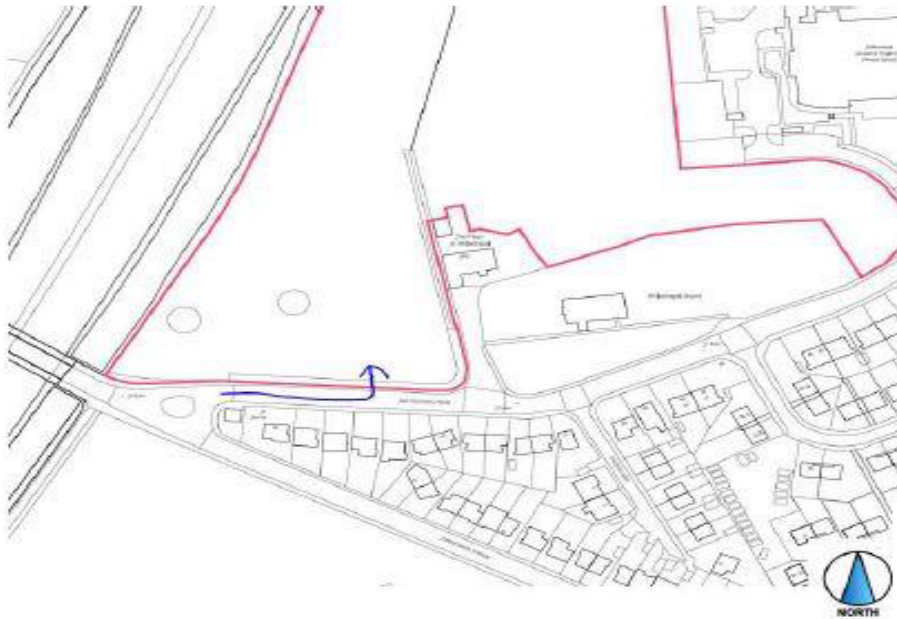
We apologise in advance for any inconvenience caused whilst these works are undertaken.

Barratt Homes Yorkshire West, Raynham House, 2 Capital Close, Morley, Leeds, LS27 0WH
T: 0113 307 4850 ■ F: 0113 307 6929 ■ W: barratthomes.co.uk

Barratt Homes is a trading name of BSW Trading Limited. Registered in England and Wales. Registered Number 31018173.
Registered Office: Barratt Homes, Guttergate Way, Forest Business Park, Brackley Hill, CookDA, Leicester, LE12 7UC.



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▲ Construction access location and routes into site

please do not hesitate to contact me.

ian kuthven
MANAGING DIRECTOR
Barratt & David Wilson Homes Yorkshire West

Barratt Homes Yorkshire West, Raynham House, 2 Capital Close, Morley, Leeds, LS27 6WH
T: 0113 307 6850 ■ F: 0113 307 6929 ■ W: barratthomes.co.uk

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Registered Office: Barratt House, Cartwright Way, Forest Business Park, Forest Hill, Coombes, Leicester, LE67 3JY.

This mail drop will also be sent to local MP, Important Ward Member and the acting Planning Officers at Kirklees Council

The letter was hand delivered to residents listed above on Friday 12th November 2021 P.M



As Part of the local resident's engagement it was requested by the Ward Members and the Planning authority & agreed by Barratt David Wilson Homes that any engagement will be proactive.

Therefore letters will be sent out at several stages throughout the development life time as listed below, informing the local residents when the main infrastructure and works will be commencing. The timing of these will be subject to detailed programming and may change during the construction lifecycle. We will also issue several updates during the development lifecycle

- Letter 1: Site start, letter 12th November 2022
- Letter 2: Update Feb 2022, includes Temporary Diversion of Plot 1-9
- Letter 3: Ground Modelling works and main ground works
- Letter 4: Commencement of Yorkshire Water sewer diversion works
- Letter 5: Installation of Surface Water Tanks and Main retaining wall Eastern Boundary
- Letter 6: Update August 2022, Dec 2022, April 2023, August 2023
- Letter 7: Installation of Acoustic Fence and Footpath Links
- Letter 8 : Installation of POS and Play area
- Letter 9 : Completion , removal to vacate

Typical Information Boards as an example shown below

Information Boards will be placed at strategic location shown below:

1. Sales Arena
2. Area of Land close by to Entrance of school to White Chapel Road

These Boards will give the details of any special operations eg sewer works, foundation operations, infrastructure, location of footpath diversions & progress being made





Typical Information Boards at strategic locations for public information

1265mm x 1300mm



ACM

Two number Information Boards will be placed at the sales arena. This would be most recognisable and safest points for any members of the public visits during hours of operation and out of hours.

At the request of the Local Community meetings can be arranged with the following, groups giving BDWH 5 days' notice of the setting up of such engagement meetings

- A. Appointed representatives from the local community
- B. Local Ward Members or Local Councillors



C. Planning OR statutory officers (if specifically requested for)

Complaints will be addressed to BDW HQ Yorkshire West in writing for the attention of the Construction Director and will be responded within 48 hours subject to the nature of the complaint

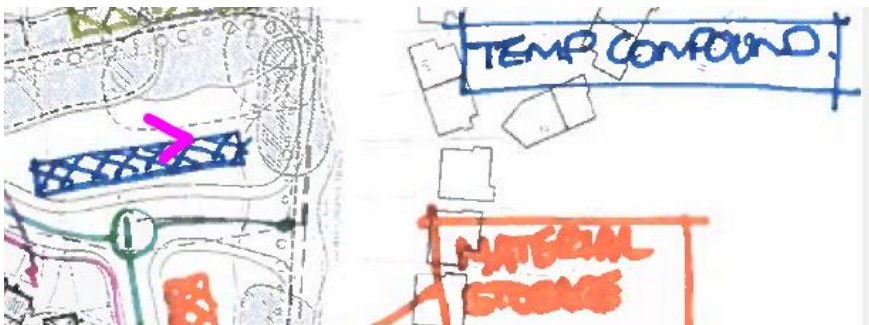
School Engagement

In agreement with the School Management Team, at an agreeable to=time a talk will be held by Barratt David Wilson Homes Site Manager Team to the Primary school to reinforce Site Safety Messages to the children that Building Sites and Traffic associated with construction are dangerous environments .

Compound Locations Identified

Temporary Compound Location

This location is of a temporary nature until the main compound infrastructure and roads to the bottom field is built and operational, then it will be closed down and returned to open space. The temporary compound is necessary until the Sales arena and Yorkshire water diversion works are completed as these works divide the whole site construction phase into construction phases and restrict the movement of materials from one part of site until the main road works are constructed to the other.



Main Compound

Located over the attenuation tank once built. This is shown in the further most north eastern point of the site. This will be the main construction compound and storage area throughout the development like cycle.





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REVISION TO NOTES:-

REVISION #: 08.10.21 First Issue

REVISION A: 01.11.21: Report amended to comments received from LPA depts.

REVISION B: 17.11.21: Report amended

REVISION C: 04.03.22: Report updated, amendments shown in green

REVISION D: 14.04.22: delivery times amended following EHO comments dated 05th April 2022

Revision E: 07.02.23: updated to include revisions requested in the LPA Planning Officers letter 03/02/23 reference 2021/93584